



City of Clayton, Missouri
Sidewalk and
Curb Ramp

ADA SELF-EVALUATION & TRANSITION PLAN



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City of Clayton, Missouri

Sidewalk and Curb Ramp ADA SELF-EVALUATION & TRANSITION PLAN



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01/ INTRODUCTION



INTRODUCTION

The Americans with Disabilities Act (ADA) is a comprehensive civil rights law for persons with disabilities in both employment and the provision of goods and services. The ADA states that its purpose is to provide a “clear and comprehensive national mandate for the elimination of discrimination against individuals with disabilities.”

The ADA seeks to dispel assumptions about disabilities and to assure equality of opportunity. The ADA prohibits discrimination in access to jobs, public accommodations, government services, public transportation and telecommunications.

This ADA Self Evaluation and Transition Plan, specific to pedestrian access within the public rights of way, is being prepared in response to expectations set forth in Title II of the Americans with Disabilities Act (ADA). Access to civic life by people with disabilities is a fundamental goal of the Americans with Disabilities Act (ADA). To ensure that this goal is met, Title II of the ADA requires state and local governments to make

their programs and services accessible to persons with disabilities ([28 CFR 35.149-35.151](#)). This requirement extends not only to physical access at government facilities, programs, and events -- but also to pedestrian facilities in public rights-of-way.

The self-evaluation process creates the opportunity for public entities to identify barriers to accessibility and develop action plans to remove existing barriers and mitigate future barriers stated within the transition plan.

This process has assisted the City of Clayton, its City Board of Alderman and staff in identifying policy, practices and physical barriers to accessibility and in developing barrier removal solutions that will facilitate the opportunity of improved access to all individuals within the city's public rights-of-way.

This report describes the overall process and findings of a self-evaluation for programs, policies and practices in the Public Works Department and a comprehensive review of the city's public rights of way infrastructure for sidewalks and curb ramps. An overview of the requirements for developing a Transition Plan per federal regulations is provided.

Subsequent sections describe the self-evaluation process, reveal the findings, provide recommendations and identify plans to remove barriers over time.



LEGISLATIVE MANDATE

The development of a Transition Plan is a requirement of the federal regulations implementing the Rehabilitation Act of 1973,


which require that all organizations receiving federal funds make their programs available without discrimination toward people with disabilities.

The Act, which has become known as the “civil rights act” of persons with disabilities, states that:

“ No otherwise qualified handicapped individual in the United States shall, solely by reason of handicap, be excluded from the participation in, be denied the benefits of, or be subjected to discrimination under any program or activity receiving federal financial assistance. (Section 504)



Subsequent to the enactment of the Rehabilitation Act, Congress passed the Americans with Disabilities Act (ADA) on July 26, 1990. Title II of the ADA covers programs, activities, and services of public entities. The Department of Justice’s Title II regulation adopts the general prohibitions of discrimination established under Section 504 and incorporates specific prohibitions of discrimination for the ADA. Title II provides protections to individuals with disabilities that are at least equal to those provided by the nondiscrimination provisions of Title V of the Rehabilitation Act.

Title II of the ADA provides that public entities must identify and evaluate all programs, activities, and services and review all policies, practices, and procedures that govern administration of the entity’s programs, activities, and services. This report establishes an ADA Self Evaluation and Transition Plan, focused specifically upon pedestrian access and public rights of way. 

02/ SELF-EVALUATION OF POLICIES, PRACTICES & PROCEDURES





Mr. Steve Meyer, P.E. served as the primary contact for the Americans with Disabilities Act self-evaluation report on the policies, practices, and procedures of the City of Clayton Public Works Department.

INTRODUCTION

1.0 AMERICANS WITH DISABILITIES ACT

The passage of the Americans with Disabilities Act (ADA) of 1990 produced a comprehensive package of civil rights protections for persons with disabilities. The key goals of the ADA are to ensure that all people with disabilities have equality of opportunity, economic self-sufficiency, full participation in American life, and independent living. To ensure that these goals are met, Title II of the ADA requires state and local governments to make their programs and services accessible to persons with disabilities. [\[28 CFR § 35.149\]](#)

The City of Clayton is classified as a “public entity” pursuant to Title II of the Americans with Disabilities Act (ADA). The City of Clayton Public Works Department (PW) is responsible for the operation and maintenance of approximately 99 lane miles and 34 centerline miles of public streets. Additionally, within the public right-of-way that the city maintains are 55 miles of sidewalk, and 22 traffic signals along with other pedestrian facilities.

PW is also responsible for administering the ADA compliance program for accessible pedestrian facilities within the public right-of-way. Barriers that deny or limit access to these programs, services or activities may be structural or nonstructural. Nonstructural barriers are defined as policies, practices or procedures that

inadvertently limit, segregate or discriminate against individuals with disabilities.

Under the ADA, PW is required to “make reasonable modifications in policies, practices, or procedures when the modifications are necessary to avoid discrimination on the basis of disability.” PW is required to do so in every situation, unless it can demonstrate “that making the modifications would fundamentally alter the nature of the service, program, or activity.” [\[28 CFR § 35.130 \(7\)\]](#)

The “reasonable modification requirement” is perhaps the most important element of a programmatic access infrastructure. This is because the Reasonable Modification Policy is the policy that says every policy, every procedure, and every practice is modifiable. The policy states that if you are qualified to participate in this program or receive this service, whatever is reasonable will be done to ensure that you receive as effective a service as any other qualified individual. It is the requirement of the ADA that demands a combination of flexibility, patience and creativity, without which a program cannot be fully accessible.



Figure 1: City of Clayton Mayor Harold Sanger City receives the 2013 Accessible STL Shine the Light Award recognizing organizations and individuals whose actions and policies exemplify the goals of promoting a fully accessible community for people with disabilities (see [notice](#) and [video](#)).

As evidenced by this self-evaluation report, the City of Clayton actively strives to eliminate barriers that may prevent persons with disabilities from enjoying access to the transportation network and independent mobility. The City of Clayton recognizes that people with disabilities are better able to participate in the community if the public rights-of-way are accessible because it is easier for them to reach their desired destinations. (see **Figure 1**)

1.1 SCOPE OF WORK

This self-evaluation report of City of Clayton PW policies, practices, and procedures is based on guidance found in the US Department of Justice [Title II Technical Assistance Manual](#) (Sections 8100 and 8200). Given that this is a study of one department within the City of Clayton, only the elements pertaining to the programs, services, and activities of PW were considered.

On May 28, 2013 an Americans with Disabilities Act (ADA) Self Evaluation Survey was sent to City of Clayton staff to document PW's formal written policies and the actual operating practices of each program, service, or activity in relation to the general prohibitions against discrimination contained in Title II of the ADA. Survey questions addressed a broad range of issues (see Appendix ___). Responses provided the consultant team with insights into PW internal program operations.

This report contains findings based on a review of responses to the ADA Self Evaluation Survey and provides recommendations to ensure compliance with the ADA. To assist the City of Clayton in its movement toward an organizational culture of compliance, strategies implemented in jurisdictions elsewhere in the nation are

presented that could assist PW improve the accessibility of its programs, services, and facilities for people with disabilities. Not all the strategies identified in this report will be appropriate for the City of Clayton. Rather, these strategies are intended to initiate a discussion about how to enhance PW's policies, practices, and procedures to improve access, mobility, and quality of life for people with disabilities while simultaneously helping the Department meet the city's overall transportation goals.

2.0 GENERAL & ADMINISTRATIVE REQUIREMENTS

Title II of the Americans with Disabilities Act (ADA) identifies specific steps that state and local governments must follow to comply with the ADA. These include:

1. *Prepare a Self-Evaluation of programs, services and activities that may not be accessible to persons with disabilities;*
2. *Develop a Transition Plan to provide for the elimination of barriers for disabled persons to access these programs, services and activities;*
3. *Designate at least one employee as the ADA Coordinator to be responsible for the ADA compliance program;*
4. *Establish a Grievance Procedure to respond to complaints regarding accessibility;*
5. *Provide notice to the public of the City's obligations under Title II to prohibit discrimination on the basis of disability; and*



6. Provide an opportunity for interested persons, including individuals with disabilities, or organizations representing individuals with disabilities, to participate in the development of the Transition Plan by submitting comments and making specific recommendations.

In accordance with the requirements set forth by the ADA, this document represents the ADA self-evaluation report on the policies, practices, and procedures of the City of Clayton Public Works Department.

2.1 ADA COORDINATOR

As required by the ADA, public agencies with 50 or more employees must designate at least one responsible employee to coordinate ADA compliance and investigate complaints. The public entity must provide the ADA coordinator's name, office address, and telephone number to all interested individuals [28 CFR § 35.107(a)].

The City of Clayton has a designated ADA Coordinator and Inclusion Officer that provides

City Department ADA Liaison Directory

The city of Phoenix has a network of ADA liaisons to serve you. For disability concerns and specific questions on disability access regarding city activities, events, services or facilities, consult the following liaisons. If you have ADA questions that have diverse implications, contact the Equal Opportunity Department at 602-502-7448, fax 602-495-8017 or TTY 602-834-1847.

All phone numbers have a 602 area code.

Department	Liaison	Phone Number
Human	Ronald Russell	312-0624
Budget & Finance	Genevieve Zili	488-7226
City Auditor	Diana Skoric	514-0871
City Clerk	Yung Peam	362-4460
City Council	Debra Mann	262-7390
City Manager	Cory Williams	261-8879
Community & Economic Development	Isabel Washchuck	495-0848
Equal Opportunity	Ruby Dinto	200-4127
Finance	Dady Vinola	514-0820
Fire	Jim Deary	338-9876
Flowing	Michelle Ayres	534-7355
Human Resources	Janelle Hancock	261-8808
Human Services	Dora Whitson	262-4824
Information Technology	Juliana Carver	261-8887
Law	Ellie Baker	262-7728
Library	Sandra Beard	514-0801
Mayor's Office	Adara Lewis	514-1275

Figure 2: City of Phoenix, AZ [ADA Liaison Directory](#)

centralized oversight and coordination of ADA compliance efforts with City departments. The identity and contact information of the City's ADA Coordinator should continue to be provided to staff and incorporated into new employee orientation packets, placed in frequently used publications and included on the City of Clayton's website and in staff and public directories.

It is recommended that the City of Clayton consider designating at least one responsible employee from each Department to build a strong network for disability awareness and compliance. The benefits of having an ADA Liasons from each Department are that: (i) It makes it easier for members of the public to identify someone to help them with questions and concerns about disability discrimination; (ii) it designates a subject matter expert so Department specific questions can be answered quickly and consistently; and (iii) it creates a team effort that can be instrumental in moving compliance plans forward. It would then be appropriate to publish the names and contact information of these Departmental representatives on the City of Clayton homepage. (see **Figure 2**)

The person appointed to the Public Works ADA Liason position should be familiar with the Department's operation, trained in the requirements of the ADA, and able to deal effectively with advocacy groups, and the public. The PW ADA Liason should bring all department programs, policies/procedures, maintenance practices, and infrastructure into compliance with ADA Standards. The individual should also develop and oversee ongoing public outreach programs with the goal to disseminate information about the Department's efforts to eliminate barriers and make the City's pedestrian rights-of-way accessible by persons with disabilities. Finally, the PW ADA Liason

is responsible for ensuring that staff members receive annual training on their responsibilities under the ADA.

2.2 GRIEVANCE PROCEDURE

The City of Clayton is required to adopt and publish procedures for resolving grievances arising under Title II of the ADA [[28 CFR § 35.107 \(b\)](#)]. This requirement provides for a timely resolution of all problems or conflicts related to ADA compliance before they escalate to litigation and/or the federal complaint process.

Neither Title II nor its implementing regulations describe what ADA grievance procedures must include. However, the Department of Justice has developed a [model grievance procedure](#) that includes: (i) a description of how and where a complaint under Title II may be filed with the government entity; (ii) if a written complaint is required, a statement notifying potential complainants that alternative means of filing will be available to people with disabilities who require such an alternative; (iii) a description of the time frames and processes to be followed by the complainant and the government entity; (iv) information on how to appeal an adverse decision; and, (v) a statement of how long complaint files will be retained.

Once the City of Clayton updates its grievance procedure under the ADA, it should be distributed to all Department heads (see Appendix "A" for the updated ADA Grievance Procedure Form). Information regarding complaint procedures should be readily available to members of the public in addition to employees. Copies should be posted in public spaces of City buildings and on the City's website (see **Figure 3**)

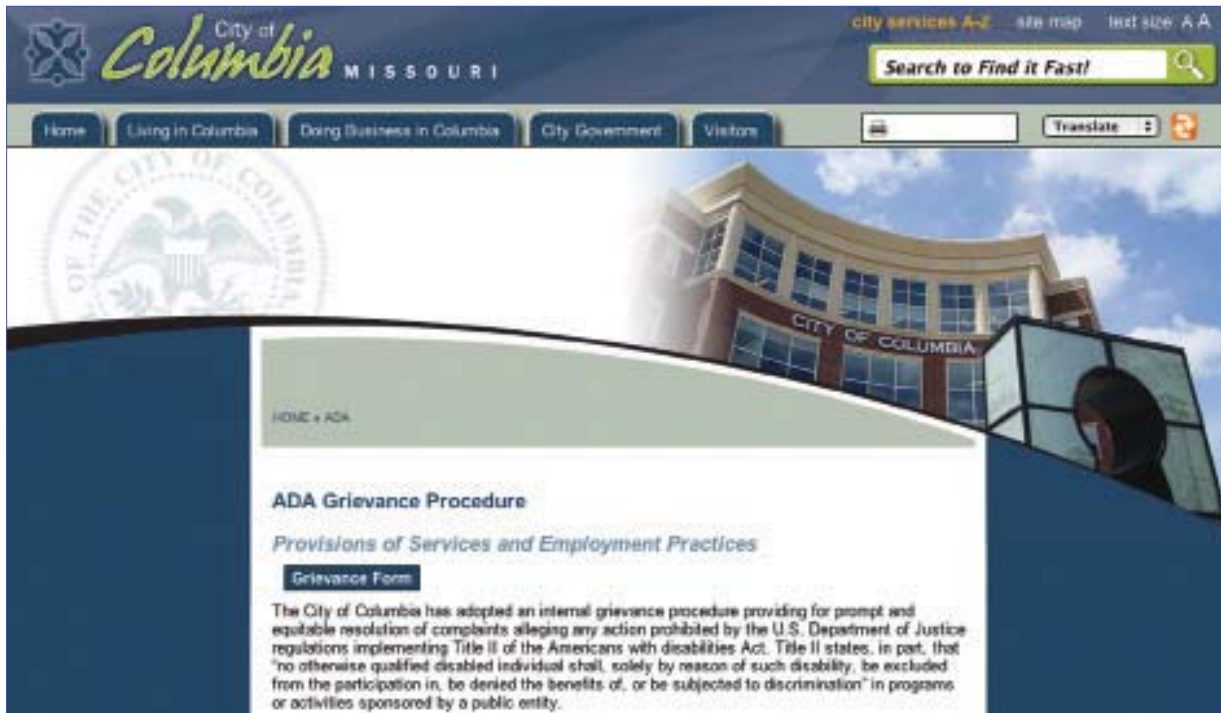


Figure 3: City of Columbia, Missouri [ADA Grievance Procedure and Form](#)

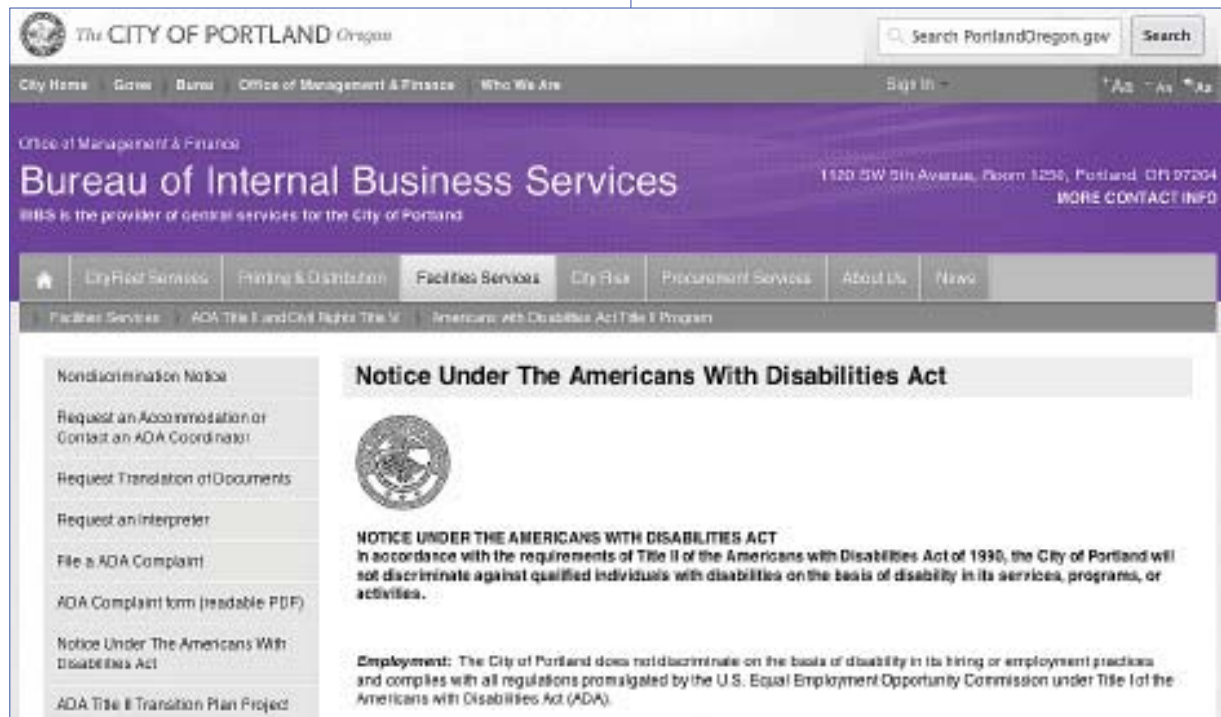


Figure 4: City of Portland (OR) [Notice of Under the Americans with Disabilities Act](#)

The City of Clayton might consider leveraging its centralized complaint handling system (known as ACR) to assist with the tracking of ADA complaint resolution. Centralized record keeping of such information will help the City and PW to regularly update its compliance efforts, and plan for additional compliance implementation for training and budget considerations.

Although individual departments within a municipal government are not required to develop and administer their own separate grievance procedure – distinct from the overarching citywide framework – the City of Clayton PW may elect to establish procedures to address complaints regarding the accessibility of pedestrian facilities in the public right-of-way (see City of Riverside, CA [ADA Complaint Resolution Procedure for Pedestrian Facilities in the Public Road Right-of-Way](#) and [ADA Complaint/Service Request Form](#)). Tracking resolution of these complaints/requests could assist PW in preparing an annual ADA compliance progress report (see Section 2.4 Plan Outreach).

It should be noted that if a complainant is dissatisfied with City's handling of the grievance at any stage of the process or does not wish to file a grievance by utilizing the City's ADA Grievance Procedure, the complainant may file a grievance directly with the [Missouri Commission on Human Rights](#) or the [United States Department of Justice](#). Use of the City's grievance procedure is not a prerequisite to the pursuit of other remedies.

2.3 NOTICE OF ADA COMPLIANCE

All public entities, regardless of size, must provide information about the rights and protections of Title II to applicants, participants, beneficiaries, employees, and other interested persons [[28 CFR § 35.106](#)]. The City of Clayton's "Disability Access and Nondiscrimination" notice is in keeping with the letter and spirit of the ADA legislation (see Appendix). The activities of the City of Clayton's PW Department are covered under this notice. Consistent with best practices elsewhere (see **Figure 4**), the "Disability Access and Nondiscrimination" notice should be posted on the city webpage and at the City of Clayton front desk counter, located at the main entrance to City Hall, including PW.

A nondiscrimination notice, such as the following sample, should be utilized more broadly in other City of Clayton publications:

“The City of Clayton does not discriminate on the basis of disability in admission to, access to, or operations of its programs, services or activities. The City does not discriminate on the basis of disability in its hiring or employment practices. Questions, concerns, complaints or requests for additional information regarding the Americans with Disabilities Act may be forwarded to the City’s ADA Coordinator (provide contact information).”

This statement should be incorporated into new employee orientation packets, and placed in frequently used publications and brochures describing City of Clayton programs, services, and activities.



2.4 PLAN OUTREACH

The ADA states that a public entity is required to enable interested persons to participate in the development of the transition plan by submitting comments and making specific recommendations. As part of the Self Evaluation process, the City of Clayton initiated an outreach program to draw out community concerns and thoughts on ADA accessibility issues in the public right-of-way. This process began with a notice on the Department website communicating the intent and current status of the initiative.

Public noticing was followed by an outreach effort that provided stakeholders from the disability community with improved access to the decision-making process. Recognizing that Clayton has limited funds and cannot

immediately make all sidewalks, curb ramp, and other pedestrian facilities fully accessible, PW solicited community input on where to prioritize its access modifications. Paraquod, a nonprofit organization whose mission is to empower people with disabilities to increase their independence through choice and opportunity, participated in this public consultation process that provided input to Clayton's ranking of sidewalks, curb ramp, and other pedestrian facilities improvements. The City reached out Paraquod and other agencies that represent the disability community, such as Society for the Blind, requesting their participation at outreach events. An example of meeting minutes from one of these events can be found in the Appendix.

After the ADA Transition Plan is adopted, a copy of this Plan should remain available for public



Figure 5: It is suggested PW consider formalizing its procedures with a protocol similar to the one adopted in the [City of Chicago's ADA Sidewalk Ramp Program progress reports](#).

reference through the Department website. As part of the project's overall public participation activities, it is recommended that PW maintain a continuing outreach component to inform the public of the progress made under the ADA Transition Plan. As part of the project's overall public participation activities, it is recommended that PW maintain a continuing outreach component to inform the public of the progress made under the ADA Transition Plan (see **Figure 5**).

The ADA compliance progress reports could identify information such as: the number of newly constructed curb ramps; number of modified curb ramps; other barrier removals and upgrades to accessibility; number of design exceptions filed that identify technical infeasibility; dollars spent by funding source for the accessibility program; updates on funding availability and the efforts taken by PW to secure extra funding; efforts toward community outreach; summary of requests/complaints received and how requests/complaints were resolved; evaluation of the targeted goals of the previous year; establishment of targets for the upcoming year; and other information as to the long term progress of the ADA program. Reports would be retained by the Department for at least three years and made available to the public in alternate formats upon request.

2.5 DEPARTMENT WEBSITE

As people turn to the internet as their primary source of information regarding services, programs, activities, and facilities, the City of Clayton's website assumes an increased importance as a communications tool. PW should strive to make its website as accessible as it can be for all visitors. Every effort should be made to encourage individuals to offer feedback and

suggestions to improve the website's architecture and organization to make it easier to navigate and more intuitive. An example of this approach is found in the City of Bloomington, IN [Website Accessibility Statement](#).

At present the City of Clayton PW website has limited ADA compliance information. PW should also work with the city-wide ADA Coordinator to document all of its ADA related policies and reports on one centralized webpage. At present the City of Clayton PW website has limited ADA compliance information. PW should also work with the city-wide ADA Coordinator to document all of its ADA related policies and reports on one centralized webpage. Documenting ADA compliance information represents a best practice (see **Figure 6**).

Utilizing the Department's web page can also provide a one-stop portal for issues related to the ADA, including pedestrian accessibility on Department rights-of-way, Transition Plan status and methodologies for filing complaints. PW should continue to welcome ongoing feedback from the public regarding the accessibility of its pedestrian facilities on its website. The website should be set up to receive requests from the public for sidewalk maintenance, curb ramps, accessible pedestrian signals, and other accessibility-related improvements; the City of San Francisco's [ADA Complaint/Assistance Form](#) is a template to consider as a model.

Finally, PW should consider providing links to ADA laws and regulations and related national websites. Compiling this information allows for education of the general public and facilitates the exchange of information with the disabled community (see **Figure 7**).



Office of
ADA Compliance and Accessibility

OFFICE OF ADA COMPLIANCE AND ACCESSIBILITY HOME ABOUT THE DEPARTMENT AMERICANS WITH DISABILITIES ACT (ADA) CITY DISABILITY PROGRAMS **POLICIES AND REGULATIONS** RELATED LINKS MAKE A COMPLAINT

Office of ADA Compliance and Accessibility Home - Policies and Regulations

Policies and Regulations

Council Policies

- [Policy No. 000-03 - People with Disabilities \(PDF\)](#) - Accessible Programs, Services, Activities and Facilities and Employment
- [Policy No. 100-04 - ADA Compliance City Contractors \(PDF\)](#) - Contracts and Contracting Compliance with the ADA and EOP
- [Policy No. 200-07 - Pedestrian Crossing Signals \(PDF\)](#) - Guideline for Pedestrian Crossings and Safety
- [Policy No. 200-10 - Audible Pedestrian Signals \(PDF\)](#) - Modernization of Traffic Control Devices including Accessible Pedestrian Signals
- [Policy No. 300-10 - Equal Opportunity \(PDF\)](#) - Employment and EEO
- [Policy No. 500-08 - Disabled Parking on City Streets \(PDF\)](#) - Blue Curb on Street Parking
- [Policy No. 600-24 - Procedures of Recognized Community Planning Groups \(PDF\)](#) - Community Planning Groups Procedures and Responsibilities
- [Policy No. 600-27 - Affordable Housing \(PDF\)](#) - Permit Expedite Program for Affordable Housing
- [Policy No. 600-32 - Centre City Streets Enhanced Pedestrian Access \(PDF\)](#) - Pedestrian and Mass Transit Circulation in Downtown
- [Policy No. 800-07 - Public Restrooms \(PDF\)](#) - Public Restroom Facilities and City Facilities including Redevelopment Areas

Figure 6: City of San Diego (CA) ADA Policies and Regulations website.

THE CITY OF
ST. LOUIS
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Search the Site

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AAA [How fonts and colors](#)

Services Government Business Live & Work Visit & Play News & Media Investor Relations

Office on the Disabled by Kim Koeltner

Resources

Helpful St. Louis resources for the community regarding disabilities.

Select Language

Helpful Community Links

City of Saint Louis Department of Personnel Administration Regulation No. 103

Home - Government - Departments and Services - Human Services - Office on the Disabled - Resources

Office on the Disabled

- Accessible Environment
- ADA Non Discrimination Policy
- Advisory Council Open Meeting Notice
- Coming to The Office on the Disabled
- Communicating with People with Disabilities

Figure 7: City of St. Louis, Missouri [Helpful Community Links](#)

3.0 PROGRAMS FOR PHYSICAL ACCESS BARRIERS

Where sidewalks and curb ramps are provided, they must be constructed to be accessible to all potential users, including those with disabilities. Accepted design standards to achieve the intended level of access for people with disabilities can be found in the following reference documents: [2010 Department of Justice ADA Standards for Accessible Design](#); [2004 ADA Accessibility Guidelines \(ADAAG\)](#); [2006 Department of Transportation ADA Standards for Transportation Facilities](#); [2009 Federal Manual on Uniform Traffic Control Devices](#); and, [2011 Proposed Accessibility Guidelines for Pedestrian Facilities in the Public Right of Way \(PROWAG\)](#).

Due to the widespread acceptance of the Proposed Accessibility Guidelines for Pedestrian Facilities in the Public Right-of-Way and their pending adoption in the future, it is appropriate that the City of Clayton PW has been referencing PROWAG for the last five years when considering new construction and alterations in the public right-of-way. Compliance with these guidelines may help limit facility items that could or would be added to the Transition Plan when these Guidelines, in their final form, are adopted by the Department of Justice into the ADA Regulations and Standards.

4.0 PRACTICES THAT LIMIT PARTICIPATION

Consistent with the assessment protocol set forth in [Title II Technical Assistant's Manual](#) - II-8.2000, this self-evaluation reviews PW's policies and practices to determine whether any exclude or limit the participation of individuals with

disabilities in its programs, activities, or services. Such policies or practices must be modified, unless they are necessary for the operation or provision of the program, service, or activity. In relation to PW's practices in the public rights-of-way, there are almost always unique circumstances where complete compliance with all of the applicable Title II ADA Regulations and Standards may not be possible.

The 2010 ADA Title II Regulations, in [[28 CFR § 35.151\(q\)\(2\)](#)], define this circumstance as being "structural impracticability". The [Proposed Accessibility Guidelines for Pedestrian Facilities in the Public Right-of-Way](#) published by the U.S. Access Board, defines this circumstance as being "existing physical restraint". The [2004 ADAAG](#), published by the U.S. Access Board, defines this circumstance as being "technically infeasible". All of these standards acknowledge, with very similar language, that if complete compliance with the ADA Regulations and Standards is determined to not be possible due to technical infeasibility the site condition(s) and reason(s) leading to this determination must be documented and receive approval before proceeding with the project.

Before reaching a conclusion about technical infeasibility, City of Clayton PW considers the extent to which physical or site constraints can be addressed by alternative designs. PW makes every effort to provide modifications that would improve the existing conditions to facilitate access to the maximum extent feasible. For example, if a curb ramp is required along a street with curb-adjacent sidewalk where the street grade is approximately 8%, then installing an 8.33% curb ramp would be impractical. In such a case, PW would still install a curb ramp, albeit steeper, because a steeper ramp would be preferred over no ramp at all. In these instances, PW makes notes on its plan documenting technical infeasibility.



The following are several approaches to formalizing current practices in the application of the technical infeasibility exception:

1. *Reference recurring conditions leading to technical infeasibility directly in PW's standard drawings. Consistent with the approach taken in the [Washington State Department of Transportation Standard Plans for sidewalks and curb ramps](#), the City of Clayton PW may want to amend the "general notes" in design drawing C-7 to include the following consideration: "The curb ramp maximum running slope shall not require the ramp length to exceed 15-feet to avoid chasing the slope indefinitely when connecting to steep grades. When applying the 15-foot maximum length, the running slope of the curb ramp shall be as flat as feasible."*
2. *Develop an ADA Statement of Technical Infeasibility Form similar to the ones produced by other state (e.g., Michigan Department of Transportation) and local governments (e.g., Metropolitan Government of Nashville and Davidson County) to document the reason full compliance is infeasible (see **Figure 8**). This approach is consistent with the following guidance in the Missouri Department of Transportation ADA Checklist: "All exceptions and technically infeasible locations should be discussed with the project manager and/or area engineer prior to acceptance of the work. All exceptions and technically infeasible locations will need to be thoroughly documented by the PW, and that documentation will be attached to this form and retained as part of the final acceptance records."*

Metropolitan Government of Nashville and Davidson County
"Statement of Technical Infeasibility"
Provisions of the Americans with Disability Act Accessibility Guidelines (ADAAG)

ADAAG contains a provision relating to "technical infeasibility", applicable only in alterations. This exception does not apply to new construction. The provision is as follows:

Technical Infeasibility means, with respect to an alteration of an existing building or a facility, that it has little likelihood of being accomplished because existing structural conditions would require removing or altering a load-bearing member which is an essential part of the structural frame; or because other existing physical or site constraints prohibit modification or addition of elements, spaces, or features which are in full and strict compliance with the minimum requirements for new construction and which are necessary to provide accessibility.

Exception: In alteration work, if compliance with the ADA is technically infeasible, the alteration shall provide accessibility to the maximum extent feasible. Any elements or features of the building or facility that are being altered and can be made accessible shall be made accessible within the scope of the alteration.

"TECHNICAL INFEASIBILITY" STATEMENT
Relating to a project under review

Project Name: _____
Project Number: _____

The following item in the planned alteration project is not in full compliance with the respective ADA guidelines as noted in the review by the ADA Compliance Division of the Metropolitan Government of Nashville and Davidson County.
As Determined by the Department overseeing the project and/or the project Architect or Engineer, this item does provide a level of accessibility to the maximum extent feasible in compliance with the above exception.

Reference to Document Review Dated: _____ Item Number: _____

Explanation of why item is "Technically Infeasible" (attach additional sheets as necessary):

Steps taken to ensure that the project was constructed to the maximum extent feasible?

I/We acknowledge that responsibility for determining "technical infeasibility" rests with the Department overseeing the project and/or the Architect or Engineer.

Department _____	Name (print) and title _____	Signature _____	Date _____
Architect / Engineer (print name) _____	License Number _____	Signature _____	Date _____

ADA Compliance Division Adopted 05/18/04

Figure 8: Metropolitan Government of Nashville and Davidson County [Statement of Technical Infeasibility](#)

5.0 EFFECTIVE COMMUNICATION

People with disabilities won't participate in government-sponsored programs, services, or activities if they don't understand what is being communicated. According to the ADA: "...A public entity shall take appropriate steps to ensure that communications with applicants, participants, and members of the public with disabilities are as effective as communications with others." [28 CFR § 35.160(a)] This directive includes ensuring that written materials (publications) such as meeting notices, agendas

and public information documents incorporate the following sample wording:

“To accommodate persons with disabilities, this document is available in alternate formats upon request.”

At present public notices on the City of Clayton’s website are available in HTML format and also available in alternate formats when requested.

To ensure that communications with members of the public with disabilities are as effective as communications with others the City of Clayton PW offers auxiliary aids and services at no cost when requested in advance by qualified individuals with disabilities. Examples of auxiliary aids and services for individuals who are deaf or have difficulty hearing can include qualified sign-language and oral interpreters, headset amplifiers, listening devices, TTYs (teletypewriters or text telephones), note takers and written materials. For individuals who are blind or have low vision, examples can include large print materials, Braille materials, materials in electronic format on compact disc or email, audio recordings, screen reader software, qualified readers, description of visually presented materials and assistance filling out forms. For individuals with speech impairments, examples can include TTYs, speech synthesizers and communications boards. Periodically, the City of Clayton should review the [Chapter III: General Effective Communication Requirements Under Title II of the ADA](#) contained in the [ADA Tool Kit for State and Local Governments](#).

6.0 SERVICE/EQUIPMENT ACCOMODATIONS

PW provides accommodation (auxiliary aids and services) to qualified persons with disabilities who need such support to equitably participate in the City’s programs, services and activities. PW ensures that it provides readers for individuals with visual impairments; interpreters or other alternative communication measures, as appropriate, for individuals with hearing impairments; and amanuenses for individuals with manual impairments. The City-wide ADA Coordinator is responsible for securing these services. Where equipment is used as part of a public entity’s program, activity, or service, an assessment should be made to ensure that the equipment is usable by individuals with disabilities, particularly individuals with hearing, visual, and manual impairments. It is suggested the City document its commitment to reasonable accommodation through the adoption of a Reasonable Accommodation Policy and posting of this statement and an accommodation request form to its website (see **Figure 9**).

The screenshot shows the 'ADA Accommodation Request or Complaint Form' on the City of Monterey website. The form is titled 'ADA Title II Program' and includes the following sections:

- Contact Us:** John Kuehl, Chief of Inspection, Services/Building Official, ADA Coordinator, email.
- Mailing Address:** (Empty field)
- You may be looking for...** (Dropdown menu with options: Website Accessibility, ADA Request for Accommodation/Complaint Form)
- Name of person making the request:** (Text input field)
- Date of request:** (Date input field)
- Address:** (Text input field)
- City:** (Text input field)
- State:** (Text input field)
- Zip:** (Text input field)
- Telephone:** (Text input field)
- Email:** (Text input field)
- Person needing accommodation if other:** (Text input field)
- Telephone of other person:** (Text input field)
- Other contact information:** (Text input field)
- Accommodation:** (Checkboxes for Website Accessibility, Barrier Removal, Accommodation needed or location of barrier)

Figure 9: City of Monterey, CA [ADA Accommodation Request Form](#).



7.0 EMERGENCY EVACUATION PROCEDURES

The City of Clayton must adopt emergency preparedness plans that establish procedures for evacuating persons with disabilities who may need special assistance in an emergency (see **Figure 10**). These plans should: (i) address what to do when an alarm is triggered; (ii) establish meeting places for assistance and evacuation chairs; and, (iii) provide direction on what to do if assistance is not available.

PW should: (i) join other City staff in the conduct of emergency evacuation procedures with periodic drills, both announced and unannounced;

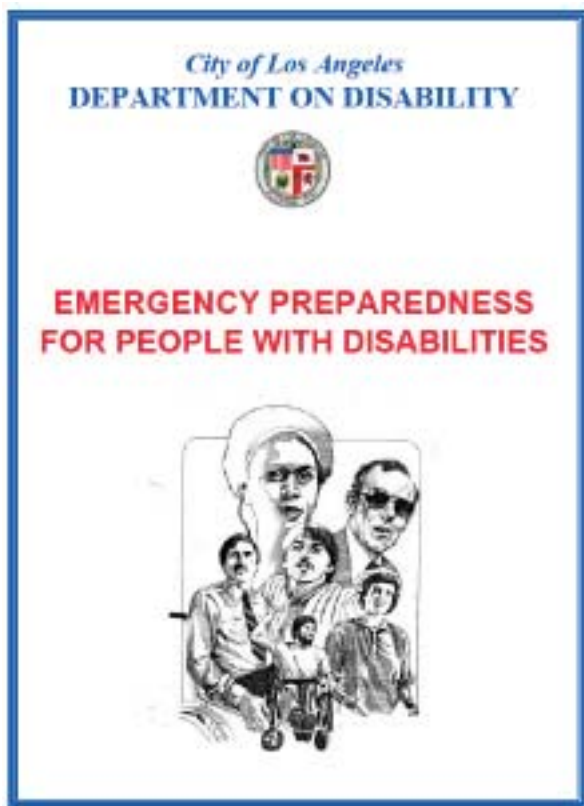


Figure 10: *City of Los Angeles [Manual on Emergency Preparedness for People with Disabilities](#)*

(ii) review existing procedures dealing with emergencies to ensure that persons with disabilities can be alerted and that they can alert emergency service providers; and, (iii) provide all evacuation policies and procedures in alternative formats when requested.

Specific guidance for complying with Title II of the ADA in the preparation of evacuation plans and procedures can be found in [Chapter VII: Emergency Management Under Title II of the ADA](#) contained in the [ADA Tool Kit for State and Local Governments](#). The following are additional resources on this topic: (i) the [Emergency Procedures for Employees with Disabilities in Office Occupancies](#) published by FEMA and the US Fire Administration; (ii) [Promising Practices for Evacuating People with Disabilities](#) produced by the Inclusion Research Institute; and, (iii) [ADA related links on the Federal Communication Commission website](#).

8.0 WRITTEN AND AUDIO VISUAL MATERIAL

The City of Clayton and all its Departments should review all audio visual material to ensure they are appropriate in content, as related to persons with disabilities to ensure demeaning stereotypes and outdated language are not contained in any publication. To be consistent with the U.S. Department of Justice's mandates within settlement agreements, the City of Clayton should develop or procure a training program or guide on disabilities and disability etiquette and distribute it to staff.

Training will ensure that staff is familiar with a variety of types of disabilities and that they are sensitive to the abilities and needs of people with

disabilities in order not to offend or demean them. The training should be periodically updated to ensure that it includes current acceptable language for talking about disabilities. The City should consult with interested persons, including individuals with disabilities, in developing or procuring the ADA training program (see **Figure 11**).

9.0 REGISTERED HISTORIC SITES

This section is found within the U.S. Department of Justice’s Guidance on setting the scope for a Self Evaluation. Although the City of Clayton has a registered historic site – the Martin Franklin Hanley House – this self-evaluation report does not consider this site as PW does not have any program responsibilities of this facility.



Figure 11: In 2007, the City of Milton, GA established by resolution a [Disability Awareness Committee](#) to promote the inclusion of persons of all ability levels through education, advocacy, and influence on public policy. This Citizen Advisory Committee developed a [training program](#) on disability etiquette for City of Milton employees.



10.0 DETERMINATION OF UNDUE BURDEN

As required by [28 CFR § 35.130\(b\) \(7\)](#) the City of Clayton makes reasonable modifications in policies, practices, and procedures that deny equal access to individuals with disabilities unless a fundamental alteration in the program would result. As the entity responsible for authorizing the planning, design, construction, operation, and maintenance of public street rights-of-way and related activities, the City of Clayton PW is required to ensure that new pedestrian facilities be accessible to people with disabilities in accordance with the [Proposed Accessibility Guidelines for Pedestrian Facilities in the Public Right-of-Way](#) (PROWAG).

PROWAG does not apply to existing pedestrian facilities unless the facilities are included within the scope of an alteration undertaken at the discretion of PW or other covered entities. PROWAG further defines alteration as “a change to a facility in the public right-of-way that affects or could affect pedestrian access, circulation, or use. Alterations include, but are not limited to, resurfacing, rehabilitation, reconstruction, historic restoration, or changes or rearrangement of structural parts or elements of a facility.” See [USDOJ-FHWA technical assistance dated 6-28-13](#) for additional clarification on alterations including items such as reconstruction, rehabilitation, widening, resurfacing, signal installation and upgrades, and projects of similar scale and effect.

The ADA permits agencies such as City of Clayton PW to defer upgrades of existing facilities to ADA standards if it can demonstrate “undue financial and administrative burden.” According to the [Title II Technical Assistance Manual Covering State and Local Government Programs and Services](#):

“A public entity does not have to take any action that it can demonstrate would result in a fundamental alteration in the nature of its program or activity or in undue financial and administrative burdens. This determination can only be made by the head of the public entity or his or her designee and must be accompanied by a written statement of the reasons for reaching that conclusion. The determination that undue burdens would result must be based on all resources available for use in the program. If an action would result in such an alteration or such burdens, the public entity must take any other action that would not result in such an alteration or such burdens but would nevertheless ensure that individuals with disabilities receive the benefits and services of the program or activity.”

It is incumbent upon the City of Clayton PW to have a system in place to equitably address accommodation requests from a qualified person with a disability and to appropriately determine when such a request would fundamentally alter the program and result in an undue burden [[28 CFR §35.150\(a\)\(3\)](#)]. The test for being unduly burdensome is the proportion of the cost for accessibility improvements compared to the agency's overall budget, not simply the project cost. The decision that pedestrian access would be unduly burdensome must be made by the head of a public agency or that official's designee, accompanied by a written statement of the reasons for the decision. If the conclusion is reached that any particular action would result in an undue burden, PW would need to take any other action that would not result in such burden but would nevertheless ensure that individuals with disabilities receive the benefits or services provided by the department.

11.0 ACCESS TO PUBLIC MEETINGS AND EVENTS

Title II entities must ensure that all public meetings and events sponsored are readily accessible to persons with disabilities. Consistent with this guidance, PW at the City of Clayton ensures that all of its public meetings, hearings, or comment periods are accessible. When hosting public meetings and hearings, the City of Clayton notifies all persons that these events will be held in accessible locations. An accessible location includes, but is not limited to, the following: wheelchair accessible path-of-travel to the meeting room, accessible restrooms, accessible parking, and an accessible route from transit and parking to the meeting facility.

All public notices should contain contact information for accommodation requests similar to:

“In compliance with the Americans with Disabilities Act, it is the policy of the City of Clayton to offer its public programs, services, and meetings in a manner that is readily accessible to everyone, including individuals with disabilities. If you are a person with a disability and require information or materials in an alternative format; or if you require any other accommodation, please contact [name] at [phone] or [email] at least five [5] days in advance of the event. Advance notification within this guidance will enable the City to make reasonable arrangements to ensure accessibility.”

A modification request may involve moving a disability-related agenda item at a public meeting to the beginning of agendas. Some people with disabilities are unable to stay late at meetings

because they have fixed schedules, as they need to use personal care attendants.

It is important to make information available to City staff on the types of modification requests that may be made by persons with different types of disabilities. Along these lines it is important to provide information to staff about auxiliary aids such as different types of assistive listening systems, sign language interpreters, readers, and other assistive technologies like real-time captioning. For example, interpreters are hired as requested for the hearing impaired and text materials that are accessible by screen readers are made available to users. Periodically, the City of Clayton should review the [Department of Justice Guide to Conducting Accessible Meetings](#) for additional information that may be helpful in planning an accessible public meeting and hearing.

Consistent with best practices, when private organizations sponsor special events on City property, the City of Clayton requires these entities complete a checklist that informs them of their responsibilities under the ADA. The City should periodically review its practices for allowing special events - including, but not limited to, indoor and outdoor concerts, plays, festivals, fairs, town hall meetings, luncheons, and ceremonies that are open to the public, whether held on city property or at other sites. Furthermore it is suggested that the following actions be taken:

- *In situations where private organizations sponsor events within public Right-of-Way, the City should require private organizations to comply with applicable ADA requirements.*
- *Event application requires a Layout Plan.*



Recommend the Layout Plan should include the location of all exposed wiring that crosses the pedestrian access route (P.A.R.). If wiring crosses the P.A.R., a ramping system must be provided. Ramp must be ADA compliant.

- *Recommend the Layout Plan show pedestrian access route (P.A.R.) throughout the event site and indicate all accessible features including P.A.R., toilets, parking, etc. Note: location of Accessible Parking for Special Events should be located as close as possible to accessible entrance.*
- *Accessible parking for Special Events shall be compliant with Section 208 & 502, 2010 ADA and R214, R215, R309 & R310 of PROWAG. Accessible parking spaces shall be indicated on required Event Parking plan.*
- *Recommend providing ADA compliant portable toilets and portable sinks for Special Events in accordance with Section 203 & 604, 2010 ADA. Layout Plan shall indicate location of portable toilets and sinks and facilities shall be located on level sites with (2% slopes or less) with appropriate clear space.*

Finally, it is important to note that the Missouri Human Rights Act makes it illegal for places of public accommodation to discriminate because of an individual's race, color, religion, national origin, ancestry, sex or disability. More information is available on the Missouri Commission on Human Rights (MCHR) [Discrimination in Place of Public Accommodation is Prohibited](#) flyer (the City of Creve Coeur, MO has established a link to this flyer from its [accessibility webpage](#) to demonstrate its recognition of this guidance).

12.0 EMPLOYMENT

Title II of the Americans with Disabilities Act prohibits a public entity from discriminating against a qualified applicant or employee with a disability. It is unlawful for a public entity to discriminate in any aspect of employment including: application, testing, interviewing, hiring, evaluation, discipline, compensation, promotion, termination and benefits.

To ensure compliance with the provisions of Title II of the ADA, the City is required to conduct a review of its employment policies and practices, including the protocol for handling requests for reasonable accommodations and the essential job functions associated with each job category. As PW does not promulgate employment policy, employment was not within the scope of this review.

13.0 EXISTING FACILITY AND ALTERATION POLICIES

PROWAG ([R201.1 Scope](#)) states that: "all newly constructed facilities, altered portions of existing facilities, and elements added to existing facilities for pedestrian circulation and use located in the public right-of-way shall comply with the requirements." Alterations include items such as reconstruction, rehabilitation, widening, resurfacing (see [USDOJ-FHWA technical assistance dated 6-28-13](#) for additional clarification), signal installation and upgrades, and projects of similar scale and effect. Since 2009, all City of Clayton PW resurfacing projects have included the review and upgrades of curb ramps to PROWAG standards.

The scope of an alteration project is determined

by the extent the alteration project directly changes or affects the public right-of-way within the project limits. The public agency must improve the accessibility of only that portion of the public right-of-way changed or affected by the alteration. If a project resurfaces the street, for accessibility purposes the curbs and pavement at the pedestrian crosswalk are in the scope of the project, but the sidewalks are not. Any of the features disturbed by the construction must be replaced so that they are accessible. All remaining access improvements within the public right-of-way shall occur within the schedule provided in the public agency's planning process.

Currently, the City of Clayton PW ensures the quality and consistency of pedestrian facilities in the public right-of way by requiring that all design and construction adhere to the City's Public Right-of-Way and Streetscape Standard Details. A review of PW's current design and construction policies for addressing new, altered, temporary, and permanent pedestrian facilities is provided in this section.

MAINTENANCE OF PEDESTRIAN FACILITIES

Section 3.2.4.3 of the [Access Board's Accessible Rights of Way: A Design Guide](#) states that:

“Public works departments should respond quickly to citizen reports of damaged surfaces along high-priority routes, so that pedestrians with mobility impairments do not have to seek alternate routes.”

The City of Clayton PW maintains its existing pedestrian facilities through an annual sidewalk replacement program. Areas of need are identified by field maintenance personnel and by requests from the public. It is the goal of the Department – through programs that address abrupt level changes in the sidewalk (such as lifting caused by tree roots) – to provide a timely response to the public's maintenance requests to assure accessibility for all persons.

The City of Clayton also operates a number of additional programs devoted to making sidewalks more accessible; including installation, repair, and maintenance programs a complaint/request process, and pedestrian related capital improvement projects.

1. *Community Development Block Grant Program: The City receives \$22,500 per year from HUD through St. Louis County. Program requirements stipulate that funding be expended to assist disadvantaged groups. The City is utilizing the funds to replace non-compliant ADA curb ramps in conjunction with upcoming street resurfacing projects.*
2. *Curb & Sidewalk Improvement Program: The average funding for this project is \$40,000 to \$50,000 per year. The project is mainly used to repair sidewalks, curb ramps and gutters throughout the City.*



3. *Street Overlay Program: The funding varies from year to year. Projects include repair, replacement and construction of new ADA compliant curb ramps and sidewalk and in conjunction with street resurfacing projects.*
4. *Streetscape Improvement Program: Funding for this program varies from year to year. In addition to providing improvements and enhancements to the City's streetscape, the project also is used to improve accessibility along the pedestrian access route within the project corridor.*
5. *Street Tree Trimming Program: The Forestry Section of the Public Works Operations Division is responsible for the management of trees located in the public rights-of-way. Tree limbs that protrude into the Pedestrian Access Route are scheduled for removal or trimming.*

Finally, developers are required to install ADA compliant driveways, sidewalks, and curb ramps through the submittal of a Right-of-Way Activity permit application and approval by the City.

13.2 PROTRUDING OBJECTS

PROWAG ([R210.2 Pedestrian Circulation Paths](#)) states: "objects along or overhanging any portion of a pedestrian circulation path shall comply with R402 and shall not reduce the clear width required for pedestrian access routes." In many instances City of Clayton residents and businesses are likely unaware that common items left on sidewalks and walkways can create unintended barriers. Natural obstacles include branches, shrubs or leaves; examples of manufactured objects are vehicles, street furniture, and temporary signs. Overgrown plants or debris can trip pedestrians, and low-

hanging branches can cause head injuries. Street furniture and temporary signs placed on sidewalks or walkways can limit accessibility.

Based on survey respondent feedback, it appears that PW's goals of maintaining the clear width required for Pedestrian Access Routes (PAR) is challenged by restaurants creating obstructions with outdoor dining areas. Field review of movable and fixed obstructions within the PAR identified numerous obstructions encountered due to outdoor dining facilities. The City of Clayton annual outdoor dining permit provides an opportunity for restaurants to install outdoor dining facilities within the public right-of-way. The City will notify establishments with outdoor furniture or temporary fencing that obstructs the three foot wide accessible route. Applicants for the Outdoor Dining Permit are required to depict the location of proposed furniture and fencing in the right-of-way that maintains the accessible route. The City of Clayton should consider instituting a public information campaign aimed at explaining how residents and businesses are expected to eliminate sidewalk obstructions in front of their properties. Clayton's City Views Newsletter could be leveraged in this information campaign to impress upon the public that removal of obstructions is a process that relies on residents and businesses as much as City government.

13.3 SNOW REMOVAL

A public agency must maintain its walkways in an accessible condition with only isolated or temporary interruptions in accessibility [[28 CFR §35.133](#)]. This maintenance obligation includes reasonable snow removal efforts.

The City of Clayton requires property owners to clear the public sidewalks on or adjacent to their

property within a reasonable time after snow or ice storms. The Planning & Development Services Department recognizes that even though the City of Clayton has a sidewalk maintenance ordinance, that it needs to assume responsibility for communicating requirements, inspecting conditions, addressing complaints, and ultimately enforcing regulations. PW recognizes that while sidewalk maintenance may be required of property owners, it does not absolve the City of Clayton of the ADA requirement to provide accessible, well-maintained pedestrian facilities; as such, it often ends up clearing curb ramps once crews are done plowing the roads. There are multiple citizen assistance program strategies to comply with ADA snow removal requirements (see [Winter Maintenance of Pedestrian Facilities in Delaware: A Guide for Local Governments](#)). By way of example, the City of Milwaukee (WI) Department of Public Works Sanitation Division has in place a winter snow removal and sidewalk clearing program for older citizens and disabled persons (see **Figure 12**). Alternatively, PW might consider implementing a volunteer-matching service similar to City of Chicago's [Snow Corps program](#) that enables seniors and residents with disabilities to request a volunteer to shovel their block in case of extreme snowfall.

13.4 ALTERNATIVE PEDESTRIAN ACCESS ROUTES

Title II of the ADA states that:

“A public entity shall maintain in operable working condition those features of facilities and equipment that are required to be readily accessible to and usable by persons with disabilities by the Act or this part. This section does not prohibit isolated or temporary interruptions in service or access due to maintenance or repairs.”

[\[28 C.F.R. § 35.133\]](#) Additionally, PROWAG ([R205](#)) states: “When a pedestrian circulation path is temporarily closed by construction, alterations, maintenance operations, or other conditions, an alternate pedestrian access route complying with sections 6D.01, 6D.02, and 6G.05 of the MUTCD (incorporated by reference, see R104.2) shall be provided. Where provided, pedestrian barricades and channelizing devices shall comply with sections 6F.63, 6F.68, and 6F.71 of the MUTCD (incorporated by reference, see R104.2).”

The City of Clayton PW abides by standards from the Manual of Uniform Traffic Control Devices to assure the safe passage of all persons through or around construction zones. By way of example, the Clayton Right-Of-Way Activity Information Sheet (Notes for Figure 6H-28—Typical Application 28) states: “When crosswalks or other pedestrian facilities are closed or relocated, temporary facilities shall be detectable and shall include accessibility features consistent with the features present in the existing pedestrian facility.”

The City of Clayton informs contractors performing construction adjacent to or within the public Right-of-Way that accessible routes must be maintained throughout the project. Compliance with these requirements is handled through the permit review process. The following recommendations are suggested relative to the Right-of-Way Information Sheet:

- Amend paragraph 10 to include a Sidewalk or Diversion Plan if P.A.R. is impacted and alters pedestrian access. All facilities installed, including those in the public right-of-way must accommodate pedestrians with disabilities. Sidewalks (P.A.R.) that are proposed to closed or diverted shall provide a plan that complies with 205 PROWAG and



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Friday, November 15, 2013

ABOUT DPW CONTACT DPW DPW DIVISIONS PARKING DPW SERVICES

Sanitation Forestry Fleet Services

A A Print

Sanitation Services

Elderly & Disabled Assistance

Garbage & Recycling Carts, Senior Citizen and Disability Service

If a senior citizen is not physically able to roll the garbage cart to the curb and back, and has no other able-bodied person in the household to do this, they will be given a Senior Citizen Hardship designation and a Sanitation employee will retrieve their garbage cart from its storage area, empty it and return it to the storage area.

If a person is not a senior citizen but is not physically able to roll the garbage cart to the curb and back, and has no other able-bodied person in the household to do this, they will be given a Disability Hardship designation. In some cases, doctor certification of disability may be required.

For information on these services, call **(414) 286-CITY (2489)**

Winter Snow Removal, Winter sidewalk clearing for older citizens and disabled persons

Residents living in single family homes who are physically unable to clear the sidewalk in front of their home may apply to the Sanitation Division for assistance with clearing the walk after a snowstorm. To be eligible, residents must certify that they and all other occupants of their home are senior citizens or handicapped and are physically unable to shovel the sidewalk. They must certify that they were unable to obtain the services of any other individual. The service is referred to as "hardship exception case."

The Sanitation Division will send a plow to clear a path on the sidewalk in front of the residence only. The walk from the sidewalk to a door is not cleared. The service is provided only when plowing operations are called, and only the public sidewalk is cleared. Driveways and side walkways on private property are not cleared by Sanitation crews. The charges will be included in the property tax bill.

NEW Highlights

Extra Cart Fee. Beginning January 1, 2013 households with more than one garbage cart will incur a \$13 per quarter charge for each additional garbage cart.

To avoid this charge, contact the DPW Call Center at 286-CITY (2489) to request the City remove the extra garbage carts from your property. There is no extra charge for more than one recycling cart.

NEW Construction Related Material Charge. There is a charge for all City residents bringing construction related material to the self help stations. This includes roofing material, brick, stones, concrete, lumber, drywall, paneling and other construction material.

The charge is \$15 for up to 8 cubic yards, and \$30 for 8-16 cubic yards. (16 cubic yard maximum.) No contractor vehicles allowed.

Figure 12: Residents living in single-family homes who are physically unable to clear the sidewalk in front of their home may [apply](#) to the City of Milwaukee (WI) Sanitation Division for assistance with clearing the walk after a snowstorm. To be eligible, residents must certify that they and all other occupants of their home are senior citizens or handicapped and are physically unable to shovel the sidewalk. They must certify that they were unable to obtain the services of any other individual. The service is referred to as "hardship exception case."

Sections 6D.01 and 6G.05 of M.U.T.C.D.. See Figure 6H-28.

- *Recommend requiring applicants to provide an ADA Post Construction Inspection Report, similar to MODOT's Post Construction Checklist.*

The City of Clayton provides advance warning of road closures for City-directed projects through notification on the City webpage. Advance warning signs are posted at the area of undergoing alterations or repair. The PW has initiated this practice. The recent Pedestrian Enhancement Project, located in the downtown area, includes maps of alternative routes to be used during construction (posted on City website).

13.5 CURB RAMPS AND DETECTABLE WARNING SURFACES

Consistent with PROWAG ([R207.1](#)), for all street projects that involve new construction or an alteration such as repaving or reconstruction, the City of Clayton PW requires that new or existing pedestrian facilities and access routes be made accessible through the construction of curb ramps where curbs exist. To formalize these practices, the City of Clayton may consider adopting a comprehensive policy that outlines its curb ramp requirements including that portion related to the detectable warning surface (see City of Cincinnati, OH [example](#)).

Consistent with PROWAG ([R208.1](#)), the City of Clayton PW requires that all curb ramps, blended transitions, and pedestrian refuge islands include the provision of detectable warning surfaces. As resurfacing and streetscape projects occur in downtown Clayton and on Wydown, the City of

Clayton PW installs concrete paver crosswalks, which provide tactile guidance. Since there is no description in accessibility guidelines recommending material type or identifying durability requirements for detectable warning systems, PW has demonstrated initiative in testing and implementing a new product called Vanguard Detectable Warning, which allows it to better meet the PROWAG requirements, and other municipalities are following the City of Clayton's lead.

13.6 ACCESSIBLE PARKING

The City of Clayton provides, maintains and or leases seven municipal parking facilities, with five parking facilities located in downtown Clayton and two parking facilities in the Wydown Business District. The majority of these facilities provide designated parking spaces for individuals with disabilities. [2010 ADA Standards for Accessible Design](#) requires that a minimum number of accessible parking spaces be provided for each parking facility. Section 208.2 requires that each parking facility provide the number of accessible spaces per Table 208.2 Parking Spaces. The location of accessible spaces should meet the requirements of Section 208.3 and placed in locations that provide the shortest accessible route. The City should consider updating current City Ordinances that outline the requirement for accessible parking spaces within a City parking facility.

PROWAG ([R214](#)) requires the provision of accessible on-street parking spaces "where on-street parking is provided on the block perimeter and the parking is marked or metered" and "where parking pay stations are provided and the parking is not marked" to comply with R309.



The City of Clayton PW has made progress in complying with this requirement (e.g., a 2010 on-street accessible parking feasibility study and construction projects completed that include ADA compliant on-street parking). To build on this foundation of improving access for drivers with disabilities, it is recommended that PW develop and adopt: (1) standards for providing accessible on-street parking where on-street parking is provided; and, (2) a policy and procedure

to evaluate individual citizen requests for on street accessible parking spaces in residential areas. See Accessible Parking Map in Appendix for recommended locations for off/on street accessible parking.

PW should also consider reviewing the strategies of other communities to enhance its own policies and practices for managing accessible parking (see [Accessible Parking Policies and Practices in](#)



Figure 13: : City of Tampa, FL [map](#) of disabled on-street parking

[Other Jurisdictions](#)). The following is a sampling of best practices posted on the webpages of other jurisdictions: accessible parking requirements (see City of San Diego, CA [example](#)); discretionary program under which on-street disabled parking zones may be installed upon request by qualified individuals (see Saint Paul, MN [example](#)); progress report (see City of Encinitas, CA [example](#)); and, map of disabled on-street parking locations (see **Figure 13**)

13.7 ACCESSIBLE PEDESTRIAN SIGNALS

According to PROWAG ([R209.1 Accessible Pedestrian Signals and Pedestrian Pushbuttons](#)): “Where pedestrian signals are provided at pedestrian street crossings, they shall include accessible pedestrian signals and pedestrian pushbuttons complying with sections 4E.08 through 4E.13 of the MUTCD (incorporated by reference, see R104.2). Operable parts shall comply with R403.”

The City of Clayton has 22 signalized intersections that are being evaluated as part of the PROWAG self-evaluation process. The self-evaluation is evaluating whether pedestrian signals comply with minimum requirements for button height, design and placement. Future installations and/or alterations will require compliance to the PROWAG requirements. As new technologies develop, PW should also evaluate these technologies in concert with the local community of persons with disabilities.

Recent construction projects completed by the City have included the following:

- Replaced ped-heads with LED heads with countdown timers and audible tones.
- Replaced old recesses pushbuttons with the current 2” “mashable” type.
- Installed Brickprint Crosswalks throughout CBD and arterials.

13.8 CONTRACTING

The City currently uses criteria that does not discriminate based on disability when selecting contractors, consultants or vendors for City projects or services. However, federal regulations also require that any outside contractors receiving City funding, or receiving federal funding through the City, comply with the requirements and regulations of Title II of the ADA. When preparing contracts for services by architects, engineers, and contractors involved in building and altering roads, sidewalks, other walkways, and curb ramps, PW should consider:

- *Including a statement in all Request for Proposals, contracts or other bid solicitation documents or Web pages explaining that businesses, organizations or individuals contracting with the City of Clayton must comply with Title II of the ADA.*
- *Requiring all City contractors, consultants or vendors sign a statement attesting to their intent to comply with Title II of the ADA. This statement can be added to existing contract documents or can be a separate form.*

PW should also consider requiring that the architects, engineers, and contractors who work for them to become certified in the PROWAG through ADA training. By way of example, the City of Columbus (OH) offers five ADA ramp training sessions annually. The free classes are open to City staff, contractors, inspectors, designers, and consultants. The classes are mandatory for all contractors and inspectors involved in the curb ramp program who have not attended a previous class (see **Figure 14**).




14.0 STAFF TRAINING

A key deficiency revealed by the self-evaluation survey conducted in May 2013 was the need for regular and recurring employee training regarding a variety of subjects related to the requirements and regulations of the ADA. Providing dedicated, trained staff within the Department for ADA compliance has a high correlation with successful implementation of the Transition Plan and future updates. PW currently offers annual ADA training (seminars and classes) to its engineers and inspectors. As part of the adoption of its Transition Plan, the City of Clayton has the opportunity to consider a range of ADA training opportunities (see "Trainings, Courses, and Presentations on page 99 of the [Special Report on Accessible Public Rights-of-Way](#)).

In addition to technical ADA training arranged through PW for its engineers and inspectors, the City of Clayton may want to consider broadening ADA training opportunities to all employees, regardless of their position. For example, PW should consider providing "Disability Awareness" and sensitivity training opportunities on the appropriate ways of

communicating with and providing modifications for persons with disabilities (see Paraquad's "[Words with Dignity](#)" flyer). This form of training would be relevant to all City employees who have contact with the public (see City of St. Louis website, [Communicating with People with Disabilities](#)).

Jurisdictions will frequently demonstrate their staff's commitment to disability etiquette by posting this information on their website (see the City of Long Beach website, [Interacting with People with Disabilities](#)). Educating Department staff in the requirements of Title II of the ADA results in better flow of information regarding non-compliant rights-of-way and can create a "buy-in" to the process by all staff.

In addition to providing this information to all existing employees and new hires, PW should provide all employees with annual reminders about the ADA requirements by providing links to instructional information. Any changes to the law affecting the Department should also be monitored and shared with employees as appropriate. This continuing education effort should occur in coordination with the annual updates that will be made to the ADA Transition Plan. 

SP-13 ADA WHEELCHAIR RAMP TRAINING REQUIREMENT

Any contractor or subcontractor involved in layout, design or construction of ADA wheelchair ramps shall have previously attended a City sponsored training session. This shall include anyone responsible for planning, directing, and constructing wheelchair ramps in the City of Columbus, i.e. project superintendent, supervisor, and/or foreman. If a new superintendent or foreman is hired, they must attend a training session before they will be permitted to execute work on this project. Laborers are not required to attend.

For more information, contact the City's ADA Section at 645-RAMP (7267)

Figure 14: City of Columbus (OH) standard contract language with construction firms working in the PROW.

03/
SELF-EVALUATION
OF INFRASTRUCTURE
Sidewalks,
Curb Ramps & Signals





The following section discusses how the sidewalk and curb ramp data was collected as well as the process of analysis for barrier ranking, to ensure effective prioritization for future barrier removal planning.

► PROJECT SCOPE & PURPOSE

The ADA requires every state and local government to prepare a self-evaluation plan to identify program access issues. The City of Clayton undertook an ADA sidewalk, signal, and curb ramp self-evaluation to assess its program accessibility responsibilities for existing pedestrian facilities in the public rights-of-way.

This report is a comprehensive analysis of the city's existing sidewalk and curb ramp facilities. Data collected from this assessment enables city staff to: 1) determine if a sidewalk, signal, curb ramp, accessible parking or street furniture meets intended design specifications and guidelines; 2) document feature and maintenance information; 3) identify portions of sidewalks, signals or pedestrian access areas requiring modifications, 4) quantify the extent of the work required; and, 5) include all data in the City's Geographic Information Systems (GIS) database. The barrier ranking analysis used in this process took into account usage, severity, and proximity to key facilities and involved a public consultation process, which the City believes responds to the needs of people with disabilities in the community.

The City of Clayton ADA Transition Plan references the barrier rankings of non-standard pedestrian facilities documented in the self-evaluation inventory report to identify corrective measures in the city's public rights-of-way. Recognizing that the City has limited funds and cannot immediately make all sidewalks, signals, curb ramp, and pedestrian facilities fully accessible, the City's ADA Transition Plan sets forth the schedule for making access modifications. The City of Clayton reserves the right to update the barrier removal priorities to allow flexibility in accommodating changes in programs, community requests, and petitions for reasonable modifications from persons with disabilities.

► PUBLIC RIGHT-OF-WAY FACILITIES

The project team worked together to develop an initial list of infrastructure study areas that would provide a representative cross section of the City. The general categories included signalized intersections, un-signalized intersections, sidewalk corridors, parking lots, accessible on-street parking, accessible parking pay stations, and street furniture. The area locations were selected based on their current use, location, services provided, ridership, and several other factors. The following sections detail these initial study areas.

Self-evaluations were completed for the initial study areas. The purpose of these evaluations was to determine the existing conditions of the facilities and if they are in compliance with the ADA and to identify solutions to remove any barriers. The following sections detail the initial study area locations, the areas within each location that were evaluated, and a summary of general issues that were found. Self-evaluation reports for each facility can be found in the Appendix.

Signalized Intersections

A total of 22 signalized intersections were evaluated during this project. This accounted for all of the existing signals in the City. These signals were located along arterials throughout the City and within the downtown area. Criteria that were evaluated for each signal included running and cross slopes of curb ramps, access to the pedestrian pushbuttons, diameter of pushbuttons, pushbutton contrast, pushbutton locator tone, mounting height of pushbuttons, presence and condition of crosswalk markings, and clear floor space in front of the pushbuttons. The self-evaluation reports for these signals can be found in the Appendix.

There were several common issues observed at these signals. They included:

- **Non-compliant curb ramps:** ramps had noncompliant running, side, and cross slopes, noncompliant landings, or no landings.

Un-Signalized Intersections (Curb Ramps & Island Curb Ramps)

A total of 272 un-signalized intersections (including ADA complaint locations) were evaluated during this project. These intersections accounted for 793 curb ramp locations. The curb ramps were located throughout the City residential areas and within the downtown area. Curb Ramps were evaluated for running and cross



“The grade and alignment of ramps affects my ability to get to my desired locations. Where there are no ramps pose the greatest obstacle.” - Frequent visitor of downtown Clayton businesses and shops.

slopes, landings, side flares, detectable warnings, gutter slopes, and gutter lips that would present trip hazards. The self-evaluation reports for these curb ramps can be found in the Appendix.

There were several common issues observed at these intersections. They included:

- **Non-compliant curb ramps:** ramps had noncompliant running, side, and cross slopes, noncompliant landings, or no landings.
- **Detectable Warning Surfaces:** there was no detectable warning surface present or if present was offset too far from the curb

Sidewalk Corridors

The self-evaluation for the arterial sidewalk corridors included assessments of the sidewalk and marked crosswalks. A total of 55.5 miles of sidewalk were evaluated. This accounted for all of the sidewalks in the City. Areas that were evaluated along each sidewalk corridor included sidewalk running and cross slopes, driveway locations, obstructions, sidewalk width, and heaving and cracking.

There were several common issues observed along the corridors. They included:

- **Non-compliant sidewalks:** sidewalks that had noncompliant running and cross slopes.
- **Heaving and cracking:** heaving adjacent to trees and sidewalk sections that were cracking.

Parking Lots & On-Street Parking

A total of seven (7) City parking lots were evaluated as part of this project. In addition, accessible on-street parking was assessed.

There were several common issues observed at these Parking lot locations. They included:

- **Accessible parking lots:** accessible parking was either not provided, or the accessible aisles have some non-compliant conditions based on running and cross slopes and signage.



- **Accessible on-street parking:** *recommendations for additional parking to meet requirements of PROWAG are shown on a map in Appendix.*

▶ RIGHT-OF-WAY INFORMATION SHEET

The City of Clayton provides an opportunity for private organizations to install new facilities within public Right-of-Way. The City of Clayton informs private organizations performing construction adjacent to or within the public Right-of-Way that accessible routes must be maintained throughout the project. Compliance with these requirements is handled through the permit review process.

Recommended Actions:

- *Amend paragraph 10 to include a Sidewalk or Diversion Plan if P.A.R. is impacted and alters pedestrian access. All facilities installed, including those in the public right-of-way must accommodate pedestrians with disabilities. Sidewalks (P.A.R.) that are proposed to closed or diverted shall provide a plan that complies with 205 PROWAG and Sections 6D.01 and 6G.05 of M.U.T.C.D.. (See Figure 6H-28 of M.U.T.C.D.)*
- *Recommend requiring applicants to provide an ADA Post Construction Inspection Report, similar to MODOT's Post Construction Checklist.*

The City of Clayton provides advance warning of road closures for City directed projects through notification on the City webpage. Advance warning signs are posted at the area of

undergoing alterations or repair.

Recommend posting location and mapping of alternative routes when existing path of travel is affected by construction or repair.

▶ ANNUAL OUTDOOR DINING PERMIT

The City of Clayton provides an opportunity for restaurants to install outdoor dining facilities within the public right-of-way. Field review of movable and fixed obstructions within the P.A.R. identified numerous obstruction encountered due to outdoor dining facilities.

Recommended Actions:

- *Requiring applicants to provide the City's 4 foot wide Pedestrian Access Route in relation to outdoor dining facilities.*

APPROACH TO INVENTORY COLLECTION & ANALYSIS

▶ DATA COLLECTION

The traditional ADA inventory process in the public rights-of-way can be labor intensive while still offering inexact information. Many public entities rely on collection methods that provide limited information or assess barriers intermittently. This does not offer a precise data or allow for accurate cost estimates for barrier removal. The City of Clayton indicated an interest in utilizing a new technology that would quickly and accurately document the type, severity, and location of sidewalk and ramp barriers in the City. A new technology was identified that would allow for a more efficient and effective process. In 2013, the City of Clayton engaged Cole & Associates, Inc. utilizing an exclusive technology called the ULIP-ADA, to complete the city's self-evaluation process for infrastructure within the public rights of way.

The technology was originally developed through a pilot program funded by the Federal Highway Administration. The Ultra-Light Inertial Profiler (ULIP) is mounted on a Segway. The device's displacement laser, three accelerometers, optical trigger, distance measurement instrument, and gyroscope are designed to measure the sidewalk surface at a rate of 10,000 records per second. Together, these devices capture highly accurate information about cross and running slope and small surface variations that can make a sidewalk difficult to navigate. A mounted computer, or





Toughbook tablet, offers an interactive display during data collection. The technical precision offered by this technology was identified as a best practice in ***ADA Compliance at Transportation Agencies: A Review of Practices*** (NCHRP 20-07 Task 249), a National Cooperative Highway Research Program study.

Cole utilized the ULIP-ADA synched with custom built software tools for precise and efficient data collection. The method can measure approximately six times more sidewalk per day than traditional methods. With a smart level, the slope is only measured where the instrument is laid; however, while driving the ULIP-mounted Segway, the slope is measured constantly. Field Specialists also collected the required information for the curb ramps and pedestrian signal actuators throughout the project area. A data collection checklist based on the 2010 ADA Standards & Regulations requirements and PROWAG recommendations were pre-programmed into handheld data collectors. Using traditional measuring and smart-level equipment, the Field Specialists entered data directly into the data collectors.

Data collection, data validation, and linking to location and digital photo files happened automatically as the Field Data Specialists entered data and moved from point to point. The Field Data Specialists accessed both the data entry and validation forms and aerial orthophoto images along with right-of-way, utility, topographic, or other feature data sets that were preloaded and appeared on the data collectors for easy reference in the field. Digital photos were automatically logged for location and linked to the database, based on synchronized time and date stamps.

The Team's measuring procedures and technologies, based on disability access code requirements, afforded for both rapid and

accurate assessment of access barriers. The process included checklists integrated into the field data collectors for both the sidewalk measurements and the curb ramps, ensuring that all relevant features or associated dimensional characteristics were recorded, photos and video were properly linked to features, and accurate location data was logged in the database.

Quality control was an important aspect of field data collection. The data entry forms prompted the user for the required information, thus reducing the incidence of missing or inaccurate data. The input was based on a standardized format, which reduced subjective interpretation. Mobile GIS removed the error-prone conversion of spatial data from paper maps to digital format by synchronizing digital files directly between a handheld device and the desktop GIS. A custom routine was run on the collected data regularly to check for errors or missing information.

Data was synced directly to a GIS database thus reducing the risk of errors in data conversion. The process involved highly effective data synching and analysis through Cole's customized software - increasing accuracy and improving project prioritization. This ultimately created a streamlined approach to understanding and evaluating barriers and the cost estimations for barrier removal to allow for effective planning for pedestrian access within the city's public rights of way.

DATABASE ANALYSIS

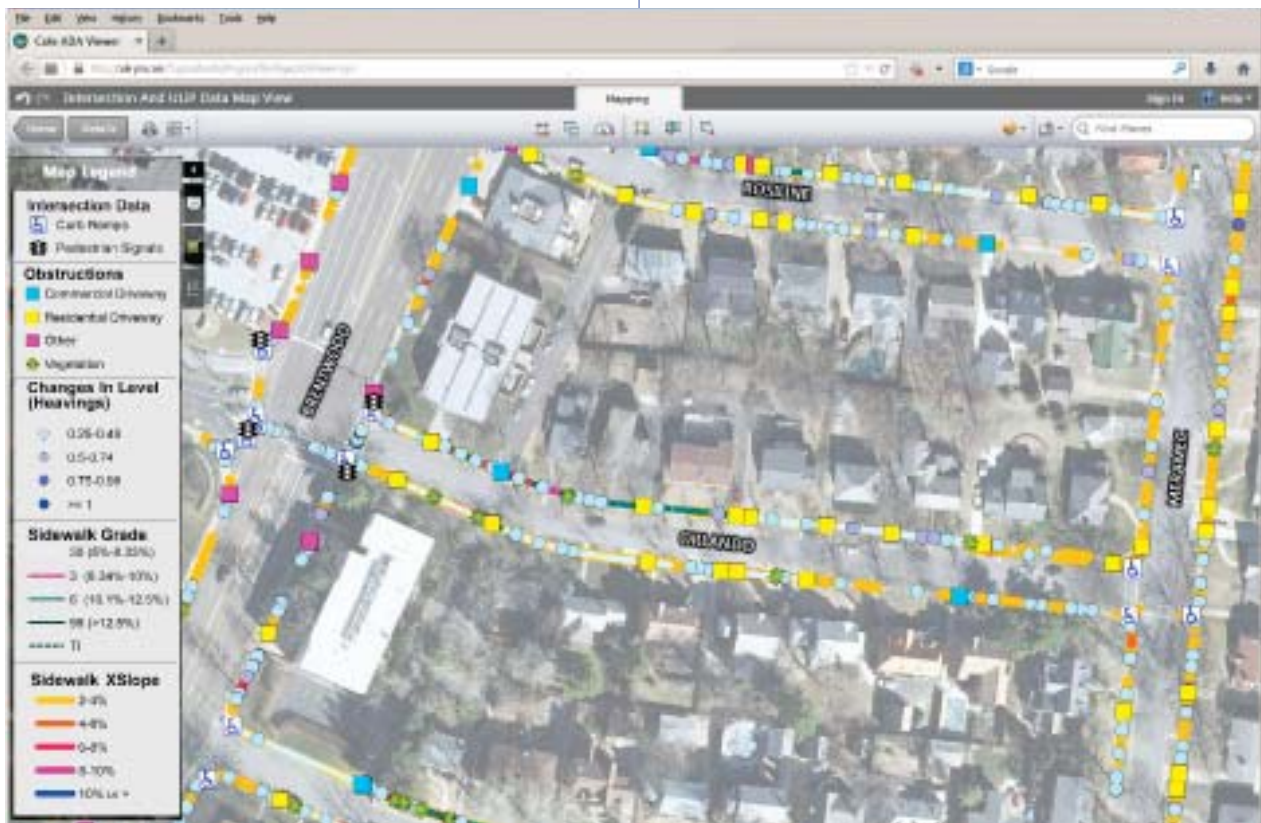
The City of Clayton's ADA sidewalk and curb ramp data provides staff with geographic data with **1)** positional accuracy, the digital representation of a barrier conforms to the actual location found

in the field; and, 2) attribute accuracy, the digital representation of a barrier is represented in a manner that best represents actual conditions found in the field (% running slope, % cross-slope, inches of vertical separation, etc.).

Guidance in defining the window size over which to assess the data was found in *Designing Sidewalks and Trails for Access (FHWA, 1999)*. This report advises that grade and cross-slope “should be measured over 2 ft intervals, the approximate length of a wheelchair wheelbase, or a single walking pace.” Adherence to Federal Highway Administration interpretation of

features in the data set provided quality assurance in the attribute accuracy of the resulting database.

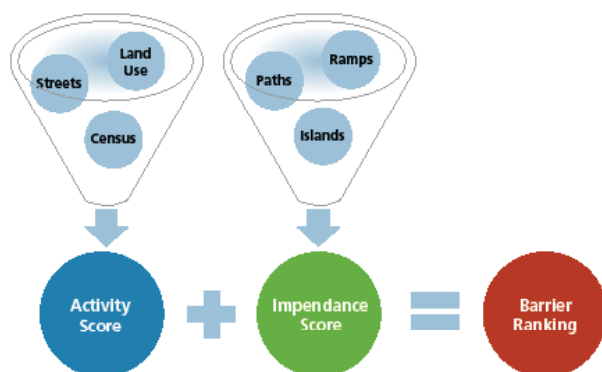
Once the field data collection and validity checks were performed, the raw data was processed so it could be stored in the City’s centralized GIS database for analysis and reporting. GIS played a pivotal role in the project from data acquisition (organizing the millions of data points generated during the study) to creating an ArcPad user interface for asset management and compliance monitoring. Additional available data point attributes can be used for compliance tracking. Compliance reporting capabilities have been deployed to track progress.





▶ BARRIER RANKING

The GIS-based barrier ranking analysis results in a combined activity and impedance score for every sidewalk and curb ramp. In order to prioritize barriers for correction, activity and impedance scores were used. A high activity score is representative of areas where pedestrian activity (especially among persons with disabilities) is likely to be greatest, based on demographic, land use, and transportation conditions. A high impedance score is representative of areas where the quality of existing pedestrian infrastructure is poor for persons with disabilities, based on barriers documented in the sidewalk and curb ramp inventory.



▶ ACTIVITY SCORE

Activity factors that describe the likelihood of disability community usage of an area's pedestrian facilities are based on the following ten activity score categories (summing to a possible 100 points).

- i. **Places of public accommodation.** *The ADA emphasizes the importance of “walkways serving local government offices and facilities” as such, these locations, along with community*

centers, social service providers, libraries, and hospitals are prioritized. This activity score category is determined by the proximity (expressed in linear feet) to these destinations.

Value	Proximity	Weight
20	0-500	100%
	501-1500	70%
	1501-2640	40%
	2640+	10%

- ii. **Transit.** *Dependable, accessible public transportation is especially important for people with disabilities, many of whom rely on it to get to work and actively participate in their communities. This activity score category is determined by the distance (expressed in linear feet) to the nearest Bus Stop location.*

Value	Proximity	Weight
20	0-500	100%
	501-1500	50%
	1501-2640	20%
	2640+	0%

- i. **Central Business District.** *Removing barriers to retail establishments facilitates equality of opportunity and full participation in civic life for people with disabilities. This activity score category is determined by the proximity (expressed in linear feet) to these destinations.*

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Value	Proximity	Weight
15	within limits	100%
	outside limits	0%

iii. **Housing density.** Higher density communities with mixed land-use patterns tend to have higher levels of pedestrian activity. This activity score category is determined by Comprehensive Land Use Plan densities (expressed as units/ acre).

Value	Proximity	Weight
15	R7&R6	100%
	R5&R4	70%
	R3	40%
	R2	10%
	R3	0%

iv. **Park Facilities-Recreation.** There are numerous parks in City of Clayton with a broad range of amenities and features, including pools, community centers, hike/bike trails, etc. The City is committed to providing access to its parks, recreational facilities, and programs, for people with disabilities. This activity score category is determined by the proximity (expressed in linear feet) to these destinations.

Value	Proximity	Weight
10	0-500	100%
	501-1500	70%
	1501-2640	40%
	2640+	0%

v. **Schools.** Parents often consider “good” schools as an important factor when buying a new home. How their children get to and from school is part of that equation. The City is committed to working with the Clayton School District through safety improvements to increase the number of children who walk to school. This activity score category is determined by the proximity (expressed in linear feet) to these destinations.

Value	Proximity	Weight
10	0-500	100%
	501-1500	70%
	1501-2640	40%
	2640+	0%

vi. **Retail.** Removing barriers to retail establishments facilitates equality of opportunity and full participation in civic life for people with disabilities. This activity score category is determined by the proximity (expressed in linear feet) to these destinations.



Value	Proximity	Weight
10	0-500	100%
	501-1500	70%
	1501-2640	40%
	2640+	0%

▶ IMPEDANCE SCORE

Activity scores are then combined with sidewalk and curb ramp impedance scores representing the areas of greatest constraint on mobility in the public rights-of-way. The impedance score calculations follow design guidance found in the ADA PROWAG Proposed Guidelines on dimensions and details for new construction and alterations of both sidewalks and curb ramps.

Sidewalk Impedance Score –

Focuses on sidewalk characteristics that directly affect the usability of a sidewalk and determines whether the facility's features represent a low, medium, or high barrier to accessibility.

The score is based on the number and severity of incidents of each of the following barriers over a given block face: fixed obstructions, changes in level, cross-slope, and running slope. Scores are further adjusted by the ratio of non-standard features relative to the total length of the block face (a possible 100 points).

- (i) **Fixed Obstructions.** *Obstructions and protrusions in the pedestrian environment are defined as objects that limit the vertical and horizontal passage space, protrude into the circulation route, or reduce the clearance width of the sidewalk. PROWAG R402.2*

states that a minimum clear width of 48 inches be preserved in the sidewalk area. And, PROWAG R402.2 states that objects projecting from walls that have leading edges between 27" and 80" should not protrude more than 4" into walks and passageways. Freestanding objects mounted on posts or pylon may overhang a maximum of 12" from 27" to 80" above the ground.

Value	Weight	Calculation
100	25%	$((\# \text{ incidents} * \text{ Value}) / \text{ Total Block Length}) * \text{ Weight}$

- (ii) **Changes in Level.** *Changes in level are defined as vertical height transitions between adjacent surfaces or along the surface of a path. Federal accessibility standards (PROWAG R302.7.2) permit changes in level less than 0.25" high to be vertical but require changes in level between 0.25" and 0.50" to have a maximum level of 50 percent. A ramp is required for changes in level that exceed 0.50".*

Impedance Category	Value	Weight	Calculation
1/4"-1/2"	5	25%	$((\# \text{ incidents} * \text{ Value}) / \text{ Total Block Length}) * \text{ Weight}$
1/2"-3/4"	25		
3/4"-1"	80		
1"+	100		

- (iii) **Cross Slope.** *Cross slope is defined as the slope measured perpendicular to the direction of travel. Changes in cross slope are commonly found at driveway crossings without level*

landings. Steep cross slopes can make it difficult for wheelchair or crutch users to maintain lateral balance and can cause walkers and wheelchairs to veer downhill or into the street. PROWAG R302.6 does not permit cross slopes to exceed 2%.

Impedence Category	Value	Weight	Calculation
2-4%	5	25%	(Linear Feet * Value) / Total Block Length * Weight)
4.1-6%	25		
6.1-8%	50		
8%+	100		

(iv) **Running Slope (Grade).** *Grade is defined as the slope parallel to the direction of travel. Both powered and manual wheelchairs can become unstable and/or difficult to control on sloped surfaces. PROWAG R302.5 & R407.2 specifies that any sidewalk that is greater than 5 percent is considered a ramp. Allowances are made to permit the grade of the sidewalk to be consistent with the grade of adjacent roadways.*

Impedence Category	Value	Weight	Calculation
5-8.33%	5	25%	(Linear Feet * Value) / Total Block Length * Weight)
8.34-10%	10		
10.1-12.5%	50		
12.6%+	100		

Signal Impedance Score –

Focuses on pedestrian street crossings with accessible traffic signals characteristics that directly affect the usability of pedestrian street crossing and determines whether the facility’s features represent a low, medium, or high barrier to accessibility. The signal impedance score is determined by two levels of consideration locations where pedestrian signals are warranted but missing (priority 1); or, locations where pedestrian signals exist but have non-standard features (priority 2).

(i) **Button Height.** *Button heights in the pedestrian environment provides for a mounting height of approximately 42 inches, but no more than 48 inches, above the sidewalk. PROWAG section R209 states that pushbuttons must comply with section 4E.08 of the Manual of Uniform Traffic Control Devices (MUTCD).*

Impedence Category	Value	Weight
0”-42”	50	10%
42”-48”	0	
48”+	50	

(ii) **Button Diameter.** *Button diameter in the pedestrian environment provides for a minimum diameter of 2 inches. PROWAG section R403.4 requires operable parts be able to operate with one hand and shall not require a tight grasping.*

Impedence Category	Value	Weight
0”-2”	100	10%
2”+	0	



- (iii) **Button Pressure.** *Button pressure in the pedestrian environment provides for a maximum operable force. PROWAG section 403.4 requires operable parts be able to operate with one hand with a force that does not exceed 5 lbs.*

Impedence Category	Value	Weight
0-5lbs	0	10%
+ 5lbs	100	

- (iv) **Button Contrast.** *Button contrast in the pedestrian environment provides for a visual contrasting button compared to the button housing mechanism. Visual contrast and a locator tone enable blind or visually impaired pedestrians to locate the pushbutton. PROWAG requires that pushbuttons shall have a visual contrast with the body background of at least 70 percent.*

Impedence Category	Value	Weight
yes	0	10%
no	100	

- (v) **Locator Tone.** *There shall be a locator tone complying with section 4E.10 of the Manual of Uniform Traffic Control Devices (MUTCD).*

Impedence Category	Value	Weight
yes	0	10%
no	100	

- (vi) **Closed Fist Option.** *Pedestrian pushbutton shall be operable with a closed fist per PROWAG requirements.*

Impedence Category	Value	Weight
yes	0	10%
no	100	

- (vii) **Clear Floor Space.** *The pushbutton shall be mounted adjacent to a clear floor space or a landing on the pedestrian access route leading to the crosswalk. The clear ground space shall be at least 36 inches by 48 inches.*

Impedence Category	Value	Weight
yes	0	10%
no	100	

- (viii) **Clear Floor Space Slope.** *The slope no more than 2% in any direction, and shall be provided with a stable, firm and slip resistant surface from which to operate controls. This clear floor space may overlap entirely with the pedestrian access route*

Impedence Category	Value	Weight
0-2%	0	10%
2-4%	20	
4-6%	40	
6%+	100	

- (ix) **Clear Floor Space Running Slope (Grade).** *Grade is defined as the slope parallel to the direction of travel. Both powered and manual wheelchairs can become unstable and/or difficult to control on sloped surfaces. PROWAG specifies that the ground space adjacent to the pushbutton be no greater than 2 percent.*

Impedance Category	Value	Weight
0-2%	0	10%
2-4%	20	
4-6%	40	
6%+	100	

Curb Ramp Impedance Score –

Focuses on curb ramp characteristics that directly affect the usability of a ramp and determines whether the facility’s features represent a low, medium, or high barrier to accessibility. The curb ramp impedance score is determined by two levels of consideration – locations where curb ramps are warranted but missing (*priority 1*); or, locations where curb ramps exist but have non-standard features (*priority 2*).

Existing curb ramps with non-standard features (*priority 2 locations*) mean that: (a) the ramp width is too narrow; (b) the top landing is either missing or too narrow; (c) the ramp slope is too steep; etc. Impedance factors that describe the barriers to accessibility at priority 2 ramp locations are based on the following twelve factors (a possible 100 points):

- (i) **Ramp Surface Obstructions.** *Space is needed at the top and bottom of ramps so that people using wheelchairs can align*

with the running slope and maneuver from ramps, including when making turns (which is difficult on sloped surfaces). PROWAG R304.2.1 & R304.3.1 defines obstructions in the pedestrian environment as objects that limit the vertical passage space, protrude into the circulation route, or reduce the clearance width of the curb ramp.

Weight	Indicator	Rating Value
10%	no obstruction	0%
	obstruction	100%

- (ii) **Alignment with Marked Crosswalks.** *Crosswalks are a critical part of the pedestrian network. A crosswalk is defined as “the portion of a roadway designated for pedestrians to use in crossing the street” and may be either marked or unmarked (ITE). PROWAG R207.1 specifies that curb ramps at marked crossings shall be wholly contained within the markings, excluding any flared sides.*

Weight	Indicator	Rating Value
10%	aligned	0%
	not aligned	100%

- (iii) **Detectable Warning Surface (DWS).** *Raised tactile surfaces used as warnings employ textures detectable with the touch of a foot or sweep of a cane to indicate hazards or changes in the pedestrian environment. PROWAG R208 & R305 specifies that tactile surfaces used as detectable warnings must also provide color contrast with surrounding surface materials.*



Weight	Indicator	Rating Value
10%	present	0%
	non-standard	100%
	missing	50%

- (iv) **Gutter Lip.** Vertical changes that exceed 1/4 inch in elevation at the bottom of a ramp can cause front casters to swivel and impede the momentum needed to propel the chair up-slope at a curb ramp threshold. PROWAG R302.7.1 specifies that transitions from ramps to gutter and streets should be flush and free of level changes.

Weight	Indicator	Rating Value
10%	0"-1/4"	0%
	1/4"-1/2"	20%
	1/2"+	100%

- (v) **Landing Panel Size.** Landings allow wheelchair users space to maneuver off the curb ramp and onto the sidewalk. Curb ramps without landings force wheelchair users entering the ramp from the street, as well as people turning the corner, to travel on the ramp flares. Furthermore, people who are continuing along the sidewalk will not have to negotiate a surface with a changing grade or cross slope. PROWAG R304.2.1, R304.3.1 & R407.6 specifies that landings with a minimum length of 48 inches, the length of an occupied wheelchair, will provide sufficient turning space. Landing length is measured in the direction of travel to and from the ramp.

Landings at a minimum of 48 inches wide prevent pedestrians from having to cross the curb ramp flare. If ramps change direction at landings, commonly known as parallel ramps, the minimum landing size should be 60 inches by 60 inches. This additional space helps avoid trapping the footrest of a wheelchair between opposing up-slopes.

Weight	Indicator	Rating Value
10%	48"+	0%
	36"-47"	40%
	0"-35"	60%
	none	100%

- (vi) **Landing Panel Slope.** Level landings at the tops of curb ramps make it possible to change direction after completing the ascent, rather than during the rise, and to avoid traveling across the compound slope of a side flare when using the sidewalk rather than the curb ramp. PROWAG R407.6 specifies that ramps shall have level landings at bottom and top of each ramp and each ramp run. Landings are considered "level" when their slopes in the two perpendicular directions of travel do not exceed 2%.

Weight	Indicator	Rating Value
10%	0-2%	0%
	2.1-4.0%	50%
	4.1%+	100%

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- (vii) **Ramp Width.** *The recommended minimum curb ramp width is 48 inches. Where space is restricted, the width of the ramp can be reduced to 36 inches. PROWAG R302.3 specifies that the curb ramp width should never be less than 36 inches (exclusive of flared sides) because there is not enough space for people using assistive devices (e.g., wheelchairs, scooters, and crutches) to travel.*

Weight	Indicator	Rating Value
10%	0-2%	0%
	2.1-4.0%	50%
	4.1%+	100%

- (viii) **Ramp Flare Slope.** *The flared sides of curb ramps provide a graded transition between the ramp and the surrounding sidewalk. Flares are not considered an accessible path of travel because they are generally steeper than the ramp and often feature significant cross-slopes with excessive rate of change of cross-slope. Flares with no adjoining walkable surface are excluded from evaluation. PROWAG R302.7.4 specifies that a 10 percent flare slope is acceptable.*

Weight	Indicator	Rating Value
10%	0-10%	0%
	10.1-12%	50%
	12.1%+	100%

- (ix) **Ramp Panel Running Slope (Grade).** *The ramp panel running slope is the sloped*

transition between the street and the sidewalk. Steep grades are difficult to negotiate for people who use walking aids and manual wheelchairs because more energy is needed to travel on sloped surfaces. PROWAG R304.2.2, R304.3.2 & R304.4.1 specifies that the ramp panel running slope should not exceed 8.33%.

Weight	Indicator	Rating Value
10	0-8.33%	0%
	8.34-12%	50%
	12.1%+	100%

- (x) **Ramp Panel Cross Slope.** *A curb ramp allows people who use wheelchairs and other wheeled devices to negotiate the elevation change between the roadway and the sidewalk without having to negotiate the curb. People with mobility impairments often have difficulty negotiating a grade and cross slope simultaneously. Since the grade of the ramp will be significant, the cross slope should be minimized. PROWAG R302.6 specifies that ramp panel cross slopes should not exceed 2%.*

Weight	Indicator	Rating Value
10%	0-2%	0%
	2.1-4%	50%
	4.1%+	100%

- (xi) **Gutter Running Slope (Grade).** *The gutter is the roadway surface immediately next to the curb ramp that runs along the curb. At a curb ramp, the grade of the gutter is generally*



counter to the grade of the ramp. Excessive grade differences between gutter and ramp can cause a wheelchair to tip forward or flip over backward. According to PROWAG R304.5.4, the running slope of the gutter – measured parallel to the path of travel – should not exceed 5 percent. If the gutter running slope exceeds 5 percent, the rate of change of grade is likely to exceed 13 percent, depending upon the grade of the ramp.

Weight	Indicator	Rating Value
5%	0-5%	0%
	5.1-8%	50%
	8%+	100%

- (xii) **Gutter Cross Slope.** *People with mobility impairments often have difficulty negotiating a grade and cross slope simultaneously. Since the grade of the ramp and gutter will be significant, the cross slope of both the ramp and gutter should be minimized. PROWAG R304.5.3 specifies that gutter cross slopes should not exceed 2%.*

Weight	Indicator	Rating Value
5%	0-2%	0%
	2.1-5.0%	50%
	5.1%+	100%

- (xiii) **No Ramp.** *Where public sidewalks are provided and pedestrian are directed to street crossings, curb ramps must be provided to eliminate a substantial barrier.*

Weight	Indicator	Rating Value
100%	no ramp	100%
0%	ramp present	0%

Island Impedance Score –

Focuses on cut-through pedestrian refuge island characteristics that provide adequate stopping, queuing and passing places for pedestrian refuge safety. Facility location and characteristics determine whether the facility's features represent a low, medium, or high barrier to accessibility. There shall be a flush landing within the cut and the edges of the cut shall be aligned perpendicular to the street being crossed, or parallel to the direction of the pedestrian access route if the pedestrian access route is not perpendicular to the street. The island impedance score is determined by two levels of consideration – locations where island cut-throughs are warranted but missing ; or, locations where islands cut-through exist but have non-standard features.

- (i) **Detectable Warning Surface (DWS).** *Raised tactile surfaces used as warnings employ textures detectable with the touch of a foot or sweep of a cane to indicate hazards or changes in the pedestrian environment. PROWAG R208.1 or R208.2 specifies that tactile surfaces used as detectable warnings must also provide color contrast with surrounding surface materials.*

Weight	Indicator	Rating Value
15%	present	0%
	non-standard	100%

03/ SELF-EVALUATION OF INFRASTRUCTURE Sidewalk and Curb Ramps

- (ii) **Gutter Lip.** Vertical changes that exceed 1/4 inch in elevation at the bottom of a ramp can cause front casters to swivel and impede the momentum needed to propel the chair up-slope at a curb ramp threshold. PROWAG 302.7.1 specifies that transitions from ramps to gutter and streets should be flush and free of level changes.

Weight	Indicator	Rating Value
15%	0"-1/4"	0%
	1/4"-1/2"	20%
	1/2"+	100%

- (iii) **Refuge Island Width.** The recommended minimum accessible route width is 60 inches. PROWAG R305.2.4 requires a five foot by five foot space as the minimum required for two persons traveling in opposite directions to wait, out of the street, for opportunities to continue crossing the street.

Weight	Indicator	Rating Value
15%	60"+	0%
	48"-60"	50%
	0"-47"	100%

- (iv) **Refuge Island Floor Running Slope (Grade).** The access route running slope is the sloped transition between the street and the sidewalk. Steep grades are difficult for people who use walking aids and manual wheelchairs to negotiate because more energy is needed to begin and to travel on sloped surfaces.

PROWAG R302.5 specifies that the access route running slope should not exceed 5%.

Weight	Indicator	Rating Value
15%	0-8.33%	0%
	8.34-12%	50%
	12.1%+	100%

- (v) **Refuge Island Floor Cross Slope.** The pedestrian access route within the island refuge area allows people who use wheelchairs and other wheeled devices to negotiate the elevation change along the route without having to negotiate changing slopes. People with mobility impairments often have difficulty negotiating a grade and cross slope simultaneously. Since the grade of the ramp will be significant, the cross slope should be minimized. PROWAG R302.6 specifies that island refuge cross slopes should not exceed 2%.

Weight	Indicator	Rating Value
10%	0-2%	0%
	2.1-4%	50%
	4.1%+	100%

- (vi) **Gutter Running Slope.** The gutter is the roadway surface immediately next to the island entry point. At the island entry point, the grade of the gutter may be counter to the grade of the island entry. Excessive grade differences between gutter and island entry can cause a wheelchair to tip forward or



flip over backward. According to PROWAG R304.5.4, the running slope of the gutter – measured parallel to the path of travel – should not exceed 5 percent. If the gutter running slope exceeds 5 percent, the rate of change of grade is likely to exceed 13 percent, depending upon the grade of the island access route.

Weight	Indicator	Rating Value
10%	0-5%	0%
	5.1-8%	50%
	8%+	100%

(vii) **Gutter Cross Slope.** *People with mobility impairments often have difficulty negotiating a grade and cross slope simultaneously. Since the grade at the island entry and gutter will be significant, the cross slope of both the island entry point and gutter should be minimized. PROWAG R304.5.3 specifies that gutter cross slopes should not exceed 2%.*

Weight	Indicator	Rating Value
10%	0-2%	0%
	2.1-5%	50%
	5%+	100%

(viii) **No Cut-Through.** *Where street crossing direct pedestrian access to refuge islands, refuge must be provided to a safe crossing.*

Weight	Indicator	Rating Value
100%	none	100%

SUMMARY



SUMMARY REPORT OF SIDEWALK, SIGNALS, CURB RAM, ISLAND & ACCESSIBLE PARKING INVENTORY

To comply with ADA requirements, sidewalks and curb ramps must meet specific standards for width, running slope, cross slope, placement, and other features.

The ADA Sidewalk and Curb Ramp Self-Evaluation project generated a significant amount of data regarding the accessibility of public rights-of-way in City of Clayton. At total of 55.5 miles of sidewalk and 709 curb ramps were evaluated. The following represents a summary of findings regarding the information gathered. The transition plan denotes planned action steps to improve access where barriers were identified.

Sidewalk Data –

Detailed documentation and data on the accessibility of City of Clayton sidewalks is found in the Appendix of this report.

1. Sidewalk obstructions

Fixed obstructions	42
Vegetative obstructions	*525

* Per city ordinances private property owners are responsible for vegetation encroaching into ROW

2. Sidewalk changes in level

Total	14,410 instances
0.25" - 0.49"	10,735 instances
0.50" - 0.74"	2,800 instances
> 0.75"	875 instances

3. Non-standard Sidewalk grade

Total	12.51 miles
5% - 8.33%	2.3 miles
8.34% - 10%	0.33 miles
10.1% - 12.5%	0.18 miles
> 12.5%	0 miles
Deemed technically infeasible	9.7 miles

4. Non-standard sidewalk cross slope

Total:	28.2 miles
2 - 4%:	27.9 miles
4.1 - 6%:	0.31 miles
6.1 - 8%	0 miles
8.1 - 10%	0 miles
> 10%	0 miles

5. Sidewalks with either a non-standard cross slope or grade

Including grade deemed technically infeasible	35.4 miles
Excluding grade deemed technically infeasible	29.5 miles

Signal Data –

Detailed documentation and data on the accessibility of City of Clayton signals is found in the Appendix of this report.

1. Pedestrian Pushbutton

Pushbutton exists	45
No pushbutton	41

2. Pushbutton size

Diameter > 2” (standard)	45
Diameter < 2” (non-standard)	0

3. Pushbutton height

Height < 42” (non-standard)	1
Height 42 - 48” (standard)	34
Height > 48” (non-standard)	10

3. Pushbutton constrast

Yes (standard)	43
No (non-standard)	2

3. Locator tone

Yes (standard)	42
No (non-standard)	3

3. Clear Floor Space

Yes (standard)	36
No (non-standard)	9

Accessible Parking Data

1. Accessible Parking Space

Accessible Spaces (standard)	2
Accessible Spaces (non-standard)	27

2. Accessible Aisle

Accessible Aisle (standard)	27
Accessible Aisle (non-standard)	2



Street Furniture (Benches)

1. Benches

Benches (standard)	2
Benches (non-standard)	16

Curb Ramp Data - Detailed documentation on the accessibility of City of Clayton curb ramps is found in the Appendix of this report.

1. Ramp type data

Perpendicular ramp configuration	664
Parallel ramp configuration	25
Blended Transition ramp configuration	8
Island Perpendicular ramp configuration	12
Island Parallel ramp configuration	0
No ramp configuration	83

2. Flare data

Slopes $\leq 10\%$ (standard)	15
Slopes between 10.1% – 12% (non-standard)	28
Slopes $> 12\%$ (non-standard)	592

* Note: Flares adjacent to non-walkable areas were not evaluated.

3. Ramp landing panel data

Depth greater than or equal to 48" (best practice)	432
Depth 48" (standard)	165
Depth smaller than 48" (non-standard)	47
None present (non-standard)	65
Slopes $> 2\%$ (non-standard)	315

4. Ramp panel data

Width greater than or equal to 48" (best practice)	513
Width $< 48\%$ (non-standard)	196
Running slope $< 8.3\%$ (standard)	353
Running slope between 8.31% - 10% (non-standard)	124
Running slope $> 10\%$ (non-standard)	230
Cross slope $\leq 2\%$ (standard)	367
Cross slope between 2% - 4% (non-standard)	196
Cross slope $> 4\%$ (non-standard)	144

Table continues.

03/ SELF-EVALUATION OF INFRASTRUCTURE Sidewalk and Curb Ramps

continues

Ramps with both running slope < 8.3% (standard) and cross slope < 2% (standard)	195
Ramps with both running slope > 8.33% (non-standard) and cross slope > 2% (non-standard)	183

5. Gutter data

Running slope ≤ 5% (standard)	483
Running slope > 5% (non-standard)	224
Cross slope ≤ 2% (standard):	327
Cross slope > 2% (non-standard)	380
Gutter Lip	209

6. Ramps lacking detectable warning surface (non-standard)

Warning present (standard)	251
Warning present but incorrectly installed (non-standard)	193
Warning not present (non-standard)	265

Island Refuge Data -

Detailed documentation on the accessibility of City of Clayton island refuge is found in the Appendix of this report.

1. Refuge Width

Width > 60"	9
Width < 60"	7

2. Refuge Slope

Slope > 5%	3
Slope < 5%	13


3. Refuge Cross Slope

Slope > 2%	10
Slope < 2%	6



SIDEWALK, SIGNALS, CURB RAMP & ISLAND INVENTORY - FINDINGS

One of the primary functions of any city is to provide safe and inviting pedestrian paths of travel.

Pedestrian traffic encourages interaction between citizens, strengthens neighborhoods and contributes to the vitality of its community. People with disabilities depend on a safe and accessible pedestrian transportation system to conduct their daily lives. However, many of the elements and conditions that go unnoticed by nondisabled persons pose significant impediments to disabled persons. Ambulatory pedestrians can simply walk around an obstruction in the sidewalk or step off a curb face without much notice. However for individuals who use wheelchairs, these ordinary features become a major impediment. A sighted person can duck under an overhanging tree limb, but to a blind person, the presence of the limb is not readily apparent and may cause physical injury. As compared to the general population, people with disabilities are generally more reliant on pedestrian networks. A large portion of the disabled population does not drive and depend on self-mobility or public transportation to get around. The percentage (prevalence rate) of people with disabilities of all ages in the US is 12.1%; this rate increases to 25% for the 65-74 age range. These factors, coupled with an aging population (where disabling conditions increase dramatically) highlight the importance of pedestrian systems that will serve all populations within the community, both in the present and into the future. 



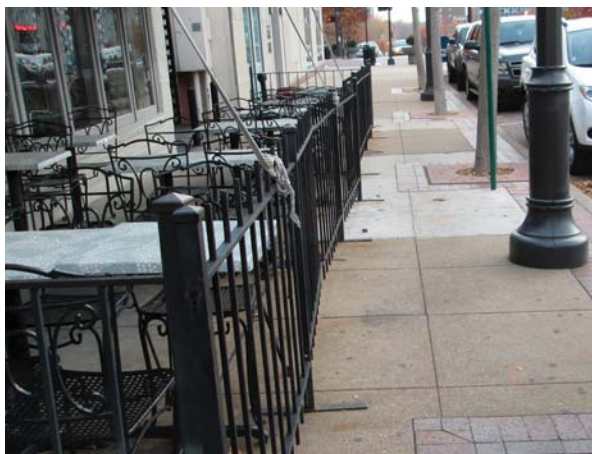
SIDEWALK INVENTORY - FINDINGS

This section of the ADA Self-Evaluation Report provides an overview of the findings of the City of Clayton's sidewalk network.

Accessible sidewalks enable people with disabilities to reach their desired destinations in the community and to enjoy the benefits of city services, programs, and activities. Where sidewalks are provided, public agencies are required to ensure that continuous, unobstructed sidewalks are maintained in operable working condition.

Total Sidewalk collected: 55.5 miles

▶ OBSTRUCTIONS



Obstructions in the pedestrian environment are defined as objects that limit the vertical and horizontal passage space, protrude into the circulation route, or reduce the clearance width of the sidewalk. Obstructions come in a variety of forms, ranging from movable obstructions such as a car parked across a sidewalk or an overgrown flowerbed to more durable obstructions like utility poles, fire hydrants, or sidewalks that are too narrow to accommodate the 48" wide by 80" high path of travel. Obstructions with large overhangs that protrude into the path of



travel can be hazardous for people with visual impairments if they are difficult to detect. Obstructions that reduce the minimum clearance width, such as fire hydrants on a narrow sidewalk, can create significant barriers for those who use wheelchairs, walkers, or crutches.

The following is a table summarizing the obstruction type and instances that were identified.

Obstruction Type	Number	Percentage of Total
Fencing	1	2.38%
Light Poles	19	45.2%
Tree Wells	4	9.5%
Fire Hydrant	1	2.38%
Signal Posts	4	9.5%
Signal Controller Box	1	2.38%
Utility Poles	6	14.28%
Parking Meter	6	14.28%
Total Fix Obstructions	42	100%
Total Vegetation Obstructions	525	100%

Recommended Actions: -

Work-around solutions are often recommended to address fixed obstructions. Work-around

solutions usually either relocate an obstruction or widen a small portion of the sidewalk to restore an accessible route. Examples of work-around solutions include an obstructing utility pole, where a sidewalk can be widened to accommodate both a utility pole and an accessible route. The second example might be a hydrant obstructing a curb ramp and how the hydrant can be located out of the pedestrian route and camouflaged by landscaping. Another might be a signal post obstructing the top landing of a curb ramp and a signal post built into a concrete bulb to accommodate both pedestrian pole access and sidewalk and landing access.

Vegetation is by far the most common obstruction in most cities' pedestrian networks. When bushes or tree branches force pedestrians to turn back or move into the path of automobile traffic, they present serious obstacles to accessibility. The City of Clayton Forestry Division has a successful on-going campaign to address tree trimming along public right-of-way corridors.

▶ CHANGES IN LEVEL



Changes in level are defined as vertical height transitions between adjacent surfaces or along the surface of a path. In the sidewalk environment,

03/ SELF-EVALUATION OF INFRASTRUCTURE Sidewalk and Curb Ramps

curbs with cracks, and dislocations in the surface material are examples of changes in level. The texture and continuity of sidewalk surfaces has a significant impact on sidewalk accessibility. People who use wheelchairs, crutches, canes, or walkers are particularly sensitive to tripping hazards. Abrupt changes in level can be painful for those individuals who utilize wheelchairs or other mobility devices. People with mobility impairments need a stable and regular surface for unimpeded access to sidewalks.

Heaving can affect all kinds of sidewalk surfaces. Concrete sidewalks are harder and whole panels (or panel pieces) tend to move as separate units, creating problems at the panel transitions where changes in level occur. Asphalt is more malleable and deforms in response to pressure. Thus, when trees roots push up under a sidewalk, asphalt tends to conform to the shape of the roots creating a situation where several rounded protrusions can disrupt ease of pedestrian movement.

City of Clayton's sidewalk inventory project identified a total of 8,293 sidewalk locations with a change in level (heave). A majority - 65% of the heaves fall in the 0.25 to 0.5 inch category. Only 62 (1%) one percent have a vertical displacement of 0.76 inch or greater. The data show that although minor sidewalk surface irregularities are common, only a small number of heaves have the potential to severely affect accessibility. The City of Clayton has an established Sidewalk Improvement program that targets the elimination of joint displacement through grinding or removal and replacement of sidewalk. The program's average funding is \$40,000 to \$50,000 per year.

Every Summer, PW evaluates changes in level by having their interns walk every sidewalk;

documenting potential trip hazards. This list is used to plan the following year's replacements, as well as immediate grinding.

Heave Category	Heave Height	Total Number
C1	0.25"-0.49"	10,735
C2	0.50"-0.74"	2,800
C3	0.75"-0.99"	599
C4	>1"	276

▶ GRADE (RUNNING SLOPE)



Steep sidewalk grades are a barrier for many pedestrians. Powered and manual wheelchairs can become unstable and/or difficult to control on sloped surfaces. These areas are even more difficult for sidewalk users who are disabled to access when wet.

Grade (running slope) is defined as the slope parallel to the direction of travel and is calculated by dividing the vertical change in elevation by



the horizontal distance covered. Although some guidelines use the term “running slope” instead of “grade,” the term “grade” is used in this report to avoid confusion with cross-slope.

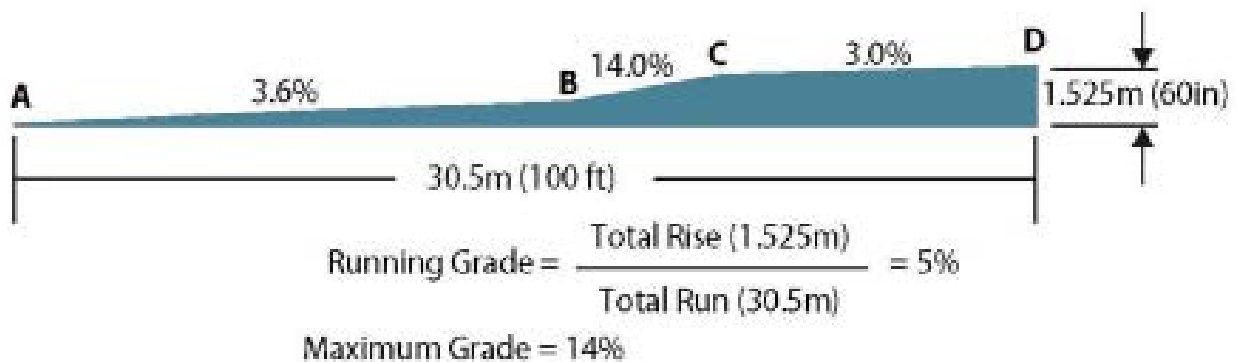
Running grade is defined as the average grade along a contiguous sidewalk segment. Measuring running grade only does not give an accurate understanding of the sidewalk environment because small steep sections may not be detected.

Maximum grade is defined as a limited section of path that exceeds the typical running grade. The image below provides an example of a typical grade that is fairly negotiable, with a maximum grade that could be very difficult for some users to traverse. In the illustration, the running grade between Points A and D is 5 percent, but the grade between Points B and C is 14 percent. A person who could negotiate a 5 percent grade

might not be able to negotiate a 14 percent grade, even for short distances

Reflected in the following table are the results of the sidewalk grade analysis, which shows the total miles of sidewalk in the city that do not meet grade standards. To accurately reflect the rate of change for grade over small distances, City of Clayton’s self-evaluation inventory classifies non-standard sidewalk segments into four grade designations. City of Clayton inventory results show 44.6 miles (82%) of sidewalk that are compliant and meet minimum grade requirements.

Grade Category	Length	%
0-5%	49.24	88.7%
5-8.33%	5.43	9.8%
8.34-10%	0.52	0.9%
10.1-12.5%	0.21	0.4%
>12.5%	0.1	0.2%
Subtotal	55.5	100%



Under some conditions, the City is limited in its ability, or completely unable, to provide sidewalks and curb ramps that meet PROWAG grade requirements because of existing site constraints. According to PROWAG guidelines, sidewalk grade should not exceed 5 percent. However, grades are often too difficult to control in the sidewalk environment because sidewalks follow the path of the street and the natural topography of the area. The GIS database played a pivotal role in determining which of the non-standard sidewalk grade profiles were deemed technically infeasible due to roadway topographic factors. Cole & Associates developed a GIS script that enabled a comparison of every non-standard sidewalk segment to the grade of the adjacent roadway. Adjacent roadway grade profiles were derived from a digital elevation model, a representation of ground surface topography. Criteria were then used to filter out 9.34 miles of non-standard sidewalk grade locations deemed technically infeasible.

Cole and the city's decision to screen profile data is consistent with currently recommended best practices from the U.S. Access Board and FHWA to make allowances for the sidewalk grade that follow the grade of the associated roadway, instead of the 5% maximum grade for walkways or an 8.3% maximum grade for ramps. In situations like these, the Department of Justice notes that "occasionally the nature of a facility makes it impossible to comply with all of the alteration standards. In such a case, features must only be made accessible to the extent that it is technically feasible to do so."

Determining Technical Infeasibility: DEM Lidar data for Clayton was obtained from the MSDIS website. According to ADAAG standards and PROWAG guidelines, sidewalk grade should not exceed 5 percent. However, grades are often too

difficult to control in the sidewalk environment because sidewalks follow the path of the street and the natural topography of the area. The DEM data and the street centerlines feature class were brought into Arcmap. The *Add Surface Information (3D Analyst)* tool, within ArcGIS, was used to determine street slope. The resulting data was then summarized by casename to calculate the average slope for each street. The final street elevation feature class was then applied to the running slope feature class to determine technical infeasibility. Criteria were then used to filter out 9.7 miles of non-standard sidewalk grade locations deemed technically infeasible.

► CROSS SLOPE



Cross slope is defined as the slope measured perpendicular to the direction of travel. Most sidewalks are built with some degree of cross slope to allow water to drain into the street and to prevent water from collecting on the path. Excessive cross slope on sidewalks is a major issue for pedestrians who use wheelchairs, scooters,



walkers, and crutches. In severe cross slopes, sidewalk users must maintain their lateral balance because they must work against the force of gravity. People using canes or crutches may be forced to turn sideways in order to keep their base support at a manageable angle. Because the cross slope of a sidewalk is typically toward the roadway, the pedestrian who loses traction or balance will be directed toward the street.

Reflected in the table below are the results of the sidewalk cross slope analysis, which shows that there are approximately 30.3 miles of non-standard (greater than 2 percent cross slope) sidewalk in the city. To accurately detail the rate of change for cross slope over small distances, City of Clayton's self-evaluation inventory classifies all non-standard sidewalk segments. City of Clayton inventory results show 23.7 miles (43%) of sidewalk that is compliant and meets minimum grade requirements.

Cross Slope Category	Total Length (Miles)
2-4%	22.5
4-6%	5.75
6-8%	1.3
8-10%	0.4
>10%	0.35
Sub-total	30.3
Compliant	25.2
Total	55.5

Driveway crossings are often built with grade changes in the sidewalk corridor that have cross slopes greater than 2 percent. To maintain an acceptable cross slope and facilitate wheelchair movement at driveways, the City of Clayton employs a standard construction detail for accessible driveway design. Standard Detail SD-5 located in the "Public Right-of-Way Standard Details" provide the design limits with built in tolerances to ensure compliance with ADA guidelines. The detail can be found on the City's website.

▶ **SIDEWALK BARRIER RANKING ANALYSIS**

Although ADA tells us which sidewalk features are non-standard it does not offer guidance on the priority for which non-standard sidewalk location should be repaired or replaced first. The final stage of the ADA Self-Evaluation Report process was to screen the total sidewalk block faces in Clayton (55.5 miles) to determine which of these sites required more immediate attention.

Sidewalk activity and impedance scores found in the technical appendix report were sorted according to their magnitude and divided into three categories (high/ medium/low) using natural breaks. The sidewalk block face natural break categories are as follows: (i) activity score (low = 0-33; medium = 34-56; high = 57-100); and, (ii) impedance score (low = 0-5; medium = 6-35; high = 36-100).

As indicated in the table below, of the 814 sidewalk block faces in Clayton: (i) no sidewalk (the smallest share) had a combined high impedance and high activity score and, (ii) 31 percent (the largest share) had a combined low impedance and low activity score (together, these 171 sidewalk block faces represent 17.1 miles of sidewalk).

Sidewalk Block Face Activity and Impedence Ranking

Activity	Impedence	Block Faces	Mileage	%
High	High	17	1.65	3%
High	Medium	44	1.3	2%
High	Low	124	7.2	13%
Medium	High	13	0.9	1.6%
Medium	Medium	43	1.4	2.5%
Medium	Low	319	24.4	44%
Low	High	4	0.5	1%
Low	Medium	19	0.4	1%
Low	Low	231	17.8	32%
Total		814	55.5	100%

Data for sidewalk activity and impedence was layered to derive a composite barrier ranking score for a particular block face. Reflected in the table below are the final results of the barrier ranking analysis that have been summarized into three categories. The final results have been

categorized using natural breaks as follows: low = 0-35; medium = 36-58; high = 59-121. As indicated, 24% (the smallest share) had a high barrier ranking. Together, this represents 130 block faces and 12 miles of sidewalk.

Barrier Ranking	Block Faces	Mileage	%
High	179	9.5	17.1%
Medium	267	18.7	33.7%
Low	368	27.3	49.2%
Total	814	55.5	100%



SIGNAL INVENTORY – FINDINGS

This section of the ADA Self-Evaluation Report assesses the status of City of Clayton’s pedestrian signal locations.

An accessible pedestrian signal and pedestrian pushbutton is an integrated device that communicates information about the WALK and DON'T WALK intervals at signalized intersections in non-visual formats to pedestrians who are blind or have low vision. Pedestrian signals and all connecting pathways shall be designed, constructed, operated and maintained so that all pedestrians, including people with disabilities, can travel safely and independently.

Title II of the ADA requires state and local governments to make pedestrian crossings accessible to people with disabilities by providing accessible pedestrian signals where warranted with appropriate engineering studies. To comply with ADA requirements, the accessible pedestrian signals provided must meet specific standards for pushbuttons, clear floor space, audible walk indicators, signage, and other features. These guidelines are summarized below.

▶ SIGNAL ASSESSMENT

Over the course of the pedestrian signal inventory, Cole assessed 22 pedestrian signalized intersections. At each of these locations the field, technicians noted the absence or presence of a pushbutton, and in the case of an existing pushbutton, measured its features based on PROWAG Guidelines and MUTCD standards. Technicians began by classifying each pedestrian street crossing location by noting “none”

Signal Feature	# of Type	% of Total Signals	# non-standard	% non-standard
Button height	45	52%	11	24.4%
Button diameter	45	52%	0	0%
Button pressure	45	52%	8	17.7%
Button contrast	45	52%	2	4.4%
Locator tone	45	52%	3	6.6%
Closed Fist Option	45	52%	2	4.4%
Clear Floor Space	45	52%	9	20%

(meaning there were no pedestrian signal/pushbutton features); or identifying and assessing the components of pedestrian signal/pushbutton features for each signal location. Cole assessed 94 separate pedestrian signalized street crossing locations while collecting data on 45 individual pedestrian signal locations with pedestrian pushbutton features. The remaining pedestrian signal locations, that do not possess a pedestrian pushbutton, have pre-timed signals that do not require a pedestrian pushbutton, per MUTCD standards.

▶ SIGNAL FEATURES

- i. **Pushbutton Height** - *The mounting height of the centerline of the pushbutton shall be mounted 42 inches above the clear ground space for approach to the device. The operable reach limits for the pushbutton should be a maximum height of 48 inches. Where a pushbutton is provided, it shall be integrated into the accessible pedestrian signal and shall comply with PROWAG R306.2.*
- ii. **Pushbutton Diameter** - *The diameter of the pushbutton shall be a minimum of 2 inches across in at least one dimension - 100% of the pushbuttons measured where in the 2 inch or larger category.*
- iii. **Pushbutton Pressure** - *The force applied to the pushbutton to activate the pushbuttons shall be no greater than 5 pounds. Compliance with this component shall follow PROWAG R403.4.*
- iv. **Pushbutton Contrast** - *Button contrast in the pedestrian environment provides for a visual contrasting button compared to the button housing mechanism. Visual contrast and a locator tone enable blind or visually impaired pedestrians to locate the pushbutton. PROWAG requires that pushbuttons shall have a visual contrast with the body background of at least 70 percent.*
- v. **Locator Tone** - *A pushbutton locator tone is "A repeating sound that informs approaching pedestrians that they are required to push a button to actuate pedestrian timing receive additional information and that enables pedestrians with visual disabilities to locate the pushbutton." (MUTCD 2009, Section 4E.12)*
- vi. **Closed Fist Option** - *A pedestrian pushbutton shall be operable with a closed fist per PROWAG requirements. Recessed buttons with small diameters does not meet the closed fist test for controls and operating mechanisms.*
- vii. **Clear Floor Space** - *The pushbutton shall be mounted adjacent to a clear ground space or a landing on the pedestrian access route leading to the crosswalk. The clear floor space shall be at least 36 inches by 48 inches, and shall be provided with a stable, firm and slip resistant surface from which to operate controls. This clear floor space may overlap entirely with the pedestrian access route.*
- viii. **Clear Floor Space Slope & Running Slope** - *The slope shall be no more than 2% in any direction. Users of wheelchairs and mobility aids need to be able to push the button from a level surface.*



▶ SIGNAL BARRIER ANALYSIS RANKING

Although ADA tells us which signal features are non-standard; it does not offer guidance on which non-standard signal location should be replaced first. The final stage of the ADA Self-Evaluation Report process was to screen the 94 signal locations at 22 signalized intersections documented in the field inventory to determine which of these sites required more immediate attention.

Signal activity and impedance scores – found in the technical appendix report – were sorted according to their size and divided into three

categories (high/medium/ low) using natural breaks. The signal natural break categories are as follows: (i) activity score (low = 0-45; medium = 46-64; high = 65-100); and, (ii) impedance score (low = 0-12; medium = 13-32; high = 33-100).

As indicated in the table below, of the 94 signal locations recorded:

- 2 percent (*the smallest share*) had a combined medium impedance and medium activity score.
- 38 percent (*the largest share*) had a combined low impedance and high activity score.

Activity	Impedance	Signals	Percent
High	High	5	11.1%
High	Medium	6	13.3%
High	Low	11	24.4%
Medium	High	3	6.7%
Medium	Medium	3	6.7%
Medium	Low	5	11.1%
Low	High	5	11.1%
Low	Medium	2	4.5%
Low	Low	5	11.1%
Total		45	100%

Data for the signal activity and impedance were layered to derive a composite barrier ranking score for a particular intersection location. Reflected in the table below are the final results of the barrier ranking analysis; categorized using natural breaks as follows: low = 0-58; medium = 59-88; high = 89-132. As indicated, 26.5% had a high barrier ranking. Together, this represents xx pedestrian signal locations .

Barrier Ranking	Signals	%
High	14	31.1%
Medium	22	48.9%
Low	9	20%
Total	45	100%

CURB RAMP INVENTORY - FINDINGS

This section of the ADA Self-Evaluation Report assesses the status of City of Clayton’s ramp locations.

Curb ramps are structural features that ease access to and from sidewalks, and are a critical part of the City of Clayton’s pedestrian network. Well-built and strategically located curb ramps greatly

improve connectivity and safety on the sidewalk network. Without curb ramps, people who use wheelchairs would not be able to independently access the sidewalk and street.

Title II of the ADA requires state and local governments to make pedestrian crossings accessible to people with disabilities by providing curb ramps. To comply with ADA requirements, the curb ramps provided must meet specific standards for width, running slope, cross slope, placement, and other features. These guidelines are summarized below:

▶ CURB RAMP CATEGORIES



Over the course of the curb ramp inventory, Cole assessed 793 ramp locations. At each of these locations the field technicians noted the absence or presence of a ramp, and in the case of an existing ramp, measured its features based on PROWAG Guidelines. Surveyors began by classifying the ramp as one of the following:



“none” (meaning there was sidewalk but no ramp access and a new curb ramp is warranted); or one of four existing ramp types (perpendicular, parallel, island/median, or blended transition). Cole’s field technicians identified 83 “none” locations. The figure below represents the 709 existing ramps and denotes their ramp type. The figure below represents the 709 existing ramps and denotes their ramp type.

Existing Curb Ramps By Category

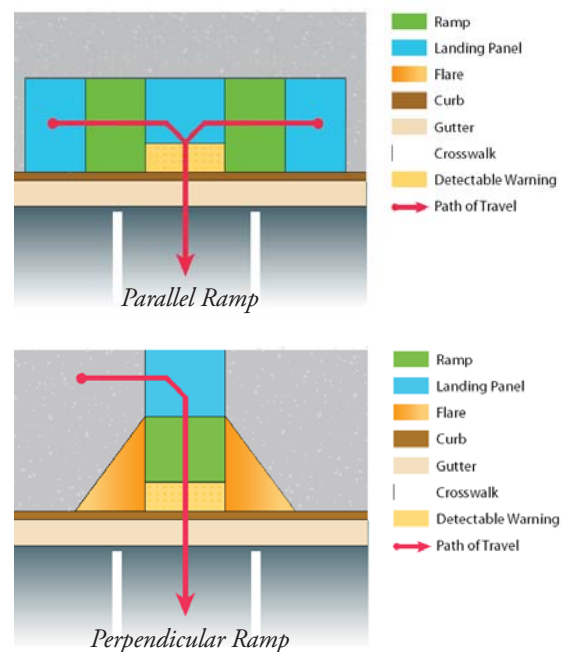
Ramp Type	Total	Percentage
Perpendicular Ramps	664	93.7%
Parallel Ramps	25	3.5%
Island/Median Ramp	12	1.7%
Blended Ramps	8	1.1%
Total	709	100%

Ramp Descriptions:

- **Perpendicular ramps** are situated perpendicular to the sidewalk path of travel and users will generally be traveling perpendicular to vehicular traffic when they enter the street at the bottom of the ramp. They often include flares to mediate the transition between the sidewalk level and the downward sloping ramp panel.
- A **parallel curb ramp** has two ramps leading down toward a center level landing at the bottom between both ramps with a level landing at the top of each ramp. A parallel curb ramp is one that is oriented so that

the path of travel on the ramp is parallel to the vehicular path of travel on the adjacent street and user’s path of travel on the sidewalk.

- **Island/median ramps** are not defined by their structural form, but by their location on an island or median. Island/median ramps can be configured as perpendicular, or parallel ramps. They often share a large landing panel which directs pedestrians from the ramp on one side of an island to the ramp on the other side. They sometimes have returned curbs (raised curb segments that separate the pedestrian path of travel from potential hazards) as a way to direct pedestrians through the cut through.
- Although not required by PROWAG, cut-throughs are preferred over ramps, as most islands are not large enough to comfortably fit two ramps and a 4-foot level landing between the ramps as required by ADA.



► CURB RAMP ASSESSMENT



PROWAG sets guidelines for each ramp feature. The federal standards outlined below correspond to the following features: (i) flares; (ii) ramp panels; (iii) transitions; (iv) gutters; (v) landing panels; and (vi) detectable warnings. For each feature there is also a discussion of City of Clayton's curb ramp inventory findings.

- i. **Ramp flares** are panels on either side of a ramp to mediate the difference between the sidewalk level and the descending ramp panel. Because they are accessible from the sidewalk and function as an entry point from the sidewalk to the ramp when the ramp landing is blocked or missing, they should not be excessively steep. Ramps do not need to have flares to align with PROWAG Guidelines, but if they do have them, the flare slopes must not exceed 10% in any direction. The field team found that

Curb Feature	# of type	% of Total curbs	# non-compliant	% non-compliant
Ramp Flares	635	80%	601	95%
Ramp Landing	644	91%	339	53%
Ramp Panels	709	89%	197	28%
Ramp Running Slope	709	89%	347	49%
Ramp Cross Slope	709	89%	345	49%
Gutter Running Slope	709	89%	202	28.5%
Gutter Cross Slope	709	89%	365	51.6%
Gutter Lip	209	23%	46	22%
Detectable Warning	444	56%	193	43%



603 (85%) of the 709 existing ramps had flares. Of the 635 ramps with flares, 34 (5%) were standard (flare is $\leq 10\%$) and 601 were non-standard (flare is $10.1\%+$).

- ii. **Ramp landings** are the level panel at the top of a perpendicular or directional ramp and at the bottom of a parallel ramp. Landing panel accessibility is based on slope and size. PROWAG R304.3.1, R304.2.1 & R407.6 specifies that landings with a minimum length of 48 inches (the length of an occupied wheelchair) will provide sufficient turning space. Landing length is measured in the direction of travel to and from the ramp. The field team found that of the 709 existing ramps: (i) 597 (84%) were standard with landing panels $\geq 48"$ and 65 (9%) were with landing panels that were non-standard with no landing panel. Slope is an important accessibility measure for landing panels. PROWAG specifies that landing panel slopes should not exceed 2% in any direction. Since landing panels are a place where pedestrians will pause and reorient themselves either toward the sidewalk (in the case of a perpendicular ramp) or toward the street, these spaces should be flat. The field team found that 347 (49%) of the 709 existing ramps had non-standard ramp panel slopes of greater than 2%.
- iii. **Ramp panels** bring a pedestrian from the level of the sidewalk down to the level of the street. They are components of all ramps. Ramp panel standards are based on ramp width, ramp running slope, and ramp cross slope. Most wheelchairs have a minimum width of 28"; 48" of width is necessary to ensure adequate space for wheelchair users or pedestrians with

crutches, walkers, or canes to pass safely and comfortably. The City's standard construction detail specifies a 60" wide ramp. Of the 709 ramps surveyed for ramp width, the survey team found that 513 (72%) followed best practices & PROWAG Guidelines with a width greater than or equal to 48" and (ii) 196 (28%) were non-standard with a width of less than 48".

- iv. **Running slope** is measured parallel to the direction of pedestrian travel. Regardless of ramp type, PROWAG requires ramp panel running slopes to measure 8.3% or less. A ramp that is too steep makes it difficult for pedestrians with limited mobility to ascend or descend. Of the 709 ramps measured for ramp panel running slope, the survey team found 353 (50%) had standard running slopes of less than 8.3%, 124 (17%) had non-standard running slopes of 8.3-10%; and 230 (32%) had highly non-standard running slopes greater than 10%.
- v. **Ramp panel cross slope** is measured perpendicular to the direction of travel. Like running slope, excessive cross slope poses a challenge for pedestrians with limited mobility by impairing stability. Regardless of ramp type, PROWAG requires cross slopes of less than 2%. of the 709 ramps measured for ramp panel cross slope, the survey team found 367 (52%) met PROWAG standards with cross slopes of less than 2%;and 340 (48%) were non-standard with cross slopes in excess of 2%.
- vi. **Gutter running slope** is measured in the gutter below the ramp, parallel to the path of travel. Steep gutter running slopes can make it difficult for a person in a wheelchair to make the transition between the ramp

and the street. PROWAG requires gutter running slope to be no more than 5%. The survey team found that of the 709 ramps measured: 483 (68%) had standard gutter running slopes no more than 5% and 224 (32%) had non-standard gutter running slopes in excess of 5%.

vii. **Gutter cross slope** is measured in the gutter below the ramp, perpendicular to the path of travel. An excessive gutter cross slope can decrease stability for disabled pedestrians. PROWAG requires gutter cross slope to be no more than 2%. The survey team found that 380 ramps, or 53% of the 709 measured, had non-standard gutter cross slopes in excess of 2 percent.

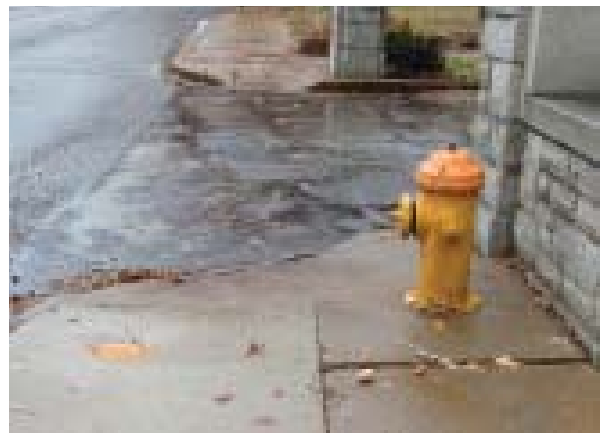
viii. **Gutter Lip:** Vertical changes that exceed 1/4 inch in elevation at the bottom of a ramp can cause front casters to swivel and impede the momentum needed to propel the chair up-slope at a curb ramp threshold. PROWAG 3027.2 specify that transitions from ramps to gutter and streets should be flush and free of level changes. The survey team found that 209 (30%) of the 709 ramps measured had gutter lip heights that exceeded 0.5" which is considered a trip hazard.

ix. **Detectable warning surfaces** are textured pads that provide pedestrians who are blind a tactile warning at the street edge of the change from pedestrian to vehicular way. Detectable warning surfaces are required on all ramps. Standard detectable warnings consist of a high-contrast color panel with truncated domes (bumps) located adjacent to the gutter transition. Of the 709 ramps measured, the survey team found that 193 (27%) did not have a warning and 265

(37%) had incorrectly installed warnings. The City's dark red detectable warning pad by "Vanguard" provides a suitable color contrast when constructed with non-standard concrete sidewalk.

x. **Transition** is the place where the ramp crosses the curb area to meet the gutter. In addition to contributing to a user's dynamic instability, curb ramp lips will also change the angle of the wheelchair as if the wheelchair were on an increased grade. For example, if a ramp is designed with an 8.3 percent slope and has a 3/4" lip at the bottom of the ramp, the actual grade the wheelchair user has to negotiate is 11.6 percent. PROWAG requires a smooth transition between the gutter and ramp

► CURB RAMP BARRIER ANALYSIS RANKING



Although ADA tells us which ramp features are non-standard; it does not offer guidance on which non-standard ramp location should be replaced first. The final stage of the ADA Self-Evaluation Report process was to screen the 705 ramp locations documented in the field inventory to determine which of these sites required more immediate attention.



Curb ramp activity and impedance scores found in the technical appendix report were sorted according to their size and divided into three categories (high/medium/ low) using natural breaks. The curb ramp natural break categories are as follows: (i) activity score (low = 0-26; medium = 27-51; high = 52-100); and, (ii) impedance score (low = 0-32; medium = 33-72; high = 73-100).

As indicated in the table below, of the curb ramp

locations recorded:

- 1 percent (the smallest share) had a combined high impedance and high activity score.
- 23 percent (the largest share) had a combined low impedance and medium activity score.

Activity	Impedence	Ramps	Percent
High	High	10	1%
High	Medium	73	9%
High	Low	92	12%
Medium	High	50	6%
Medium	Medium	196	25%
Medium	Low	127	16%
Low	High	29	4%
Low	Medium	104	13%
Low	Low	111	14%
Total		792	100

Data for the curb ramp activity and impedance were layered to derive a composite barrier ranking score for a particular block face. Reflected in the table below are the final results of the barrier ranking analysis; categorized using natural breaks as follows: low = 0-59; medium = 61-98; high = 99-156. As indicated, 18.2% (the smallest share) had a high barrier ranking. Together, this represents 129 curb ramp or no curb ramp locations .

Barrier Ranking	Curb Ramps	Percent
High	209	26.4%
Medium	374	47.2%
Low	209	26.4%
Total	792	100%

ISLAND REFUGE INVENTORY - FINDINGS

This section of the ADA Self-Evaluation Report assesses the status of City of Clayton’s Island refuge (curb cut-through) locations.

Island refuge locations are structural features that ease access to and from sidewalks, and are a critical part of the City of Clayton’s pedestrian network. Island refuge areas are typically established to provide a resting area and minimize street intersection crossing distance. Island refuge routes shall have a flush landing within the cut and the edges of the cut shall be aligned perpendicular to the street being crossed, or parallel to the direction of the pedestrian access route if the pedestrian access route is not perpendicular to the street.

▶ ISLAND REFUGE ASSESSMENT

Title II of the ADA requires state and local governments to make pedestrian street crossings accessible to people with disabilities by providing island refuge areas. To comply with ADA requirements, the island refuge areas provided must meet specific standards for width, running slope, cross slope, placement, and other features. 25 individual islands were present along accessible routes and within public right-of-way. 9 of the 25 islands identified had no curb ramps or refuge access aisles necessary for a pedestrian access route. These guidelines are summarized below.



Island Curb Feature	# of Type	% of Total Island Curbs	# non-compliant	% non-compliant
Refuge Width	16	64	5	31
Refuge Length	16	64	2	8
Refuge Slope	16	64	3	12
Refuge Cross Slope	16	64	7	28
Gutter Running Slope	16	64	2	13
Gutter Cross Slope	16	64	5	31
Gutter Lip	16	64	0	0
Detectable Warning	16	64	5	31

▶ ISLAND REFUGE FEATURES

- i. **Island Refuge Width** - *The recommended minimum accessible route width is 60 inches. PROWAG R305.2.4 requires a five foot by five foot space is the minimum required for two persons traveling opposite directions to wait, out of the street, for opportunities to continue crossing the street.*
- ii. **Island Refuge Floor Running Slope** - *The pedestrian access route running slope is the sloped transition between street entry points. Steep grades are difficult for people who use walking aids and manual wheelchairs to negotiate because more energy is needed to begin and to travel on*

sloped surfaces. PROWAG R302.5 specifies that the island refuge running slope should not exceed 5%. Of the 25 islands measured for island refuge running slope, the survey team found that 13 islands met PROWAG standards with running slopes of less than 5%; 3 were non-standard with running slopes greater than 5%.

- iii. **Island Refuge Floor Cross Slope** - *The pedestrian access route within the island refuge area allows people who use wheelchairs and other wheeled devices to negotiate the elevation change along the route without having to negotiate changing slopes. People with mobility impairments*

often have difficulty negotiating a grade and cross slope simultaneously. Since the grade of the ramp will be significant, the cross slope should be minimized. PROWAG R302.6 specifies that island refuge cross slopes should not exceed 2%. Of the 25 islands measured for island refuge cross slope, the survey team found that 9 met PROWAG standards with cross slopes of less than 2% and 7 were non-standard with cross slopes greater than 2%.

- iv. **Island Refuge DWS** - Raised tactile surfaces used as warnings employ textures detectable with the touch of a foot or sweep of a cane to indicate hazards or changes in the pedestrian environment. PROWAG specifies that tactile surfaces used as detectable warnings must also provide color contrast with surrounding surface materials. Detectable warning surfaces are required at both ends of accessible route, within the confines of the island refuge area, when the refuge length exceeds 72 inches. Detectable warning surfaces should not be placed where the refuge length is less than 72 inches.
- v. **Gutter Running Slope** - The slope is measured in the gutter, at the entrance to the island refuge route, parallel to the path of travel. Steep gutter running slopes can make it difficult for a person in a wheelchair to transition between the crosswalk and the island refuge route. PROWAG requires gutter running slope to be no more than 5%. The team found that of the 16 islands measured there were no gutter running slopes no more than 5%.
- vi. **Gutter Cross Slope** - The slope is measured in the gutter at the entrance to the island

refuge route, perpendicular to the path of travel. An excessive gutter cross slope can decrease stability for disabled pedestrians. PROWAG requires gutter cross slope to be no more than 2%. The survey team found that 10 islands had standard cross slopes, or 63% of the 16 measured; and 6 (37%) had non-standard gutter cross slopes in excess of 2 percent.

- vii. **Gutter Lip** - The island refuge route crosses the curb area to meet the gutter. PROWAG requires a smooth transition between the gutter and the island refuge route. The survey team found that 11 (69%) of the 16 islands measured had non-standard transitions that are considered tripping hazards.



▶ ISLAND REFUGE BARRIER ANALYSIS RANKING

Although ADA tells us which ramp features are non-standard it does not offer guidance on which non-standard islands should be replaced first. The final stage of the ADA Self-Evaluation Report process was to screen the 25 island locations documented in the field inventory to determine which of these sites required more immediate attention.

Island refuge activity and impedance scores found in the technical appendix report were sorted according to their size and divided into three categories (high/medium/ low) using


natural breaks. The island refuge natural break categories are as follows: (i) activity score (low = 0-25; medium = 26-40; high = 41-100); and, (ii) impedance score (low = 0-22; medium = 23-50; high = 51-100).

As indicated in the table below, of the 25 island locations recorded:

- 4 percent (the smallest share) had a combined high impedance and high activity score.
- 24 percent (the largest share) had a combined low impedance and high activity score.

Activity	Impedance	Island Curbcuts	Percent
High	High	1	4
High	Medium	3	12
High	Low	2	8
Medium	High	2	8
Medium	Medium	2	8
Medium	Low	4	16
Low	High	6	24
Low	Medium	0	0
Low	Low	5	20
Total		25	100

03/ SELF-EVALUATION OF INFRASTRUCTURE Sidewalk and Curb Ramps

Data for the island refuge (island cut-throughs) activity and impedance were layered to derive a composite barrier ranking score for a particular intersection. Reflected in the table below are the final results of the barrier ranking analysis; categorized using natural breaks as follows: low = 0-40; medium = 41-90; high = 91-156. As indicated, 44 percent (the highest share) had a high barrier ranking. 

Barrier Ranking	Island Refuge	Percent
High	11	44%
Medium	8	32%
Low	6	24%
Total	25	100%



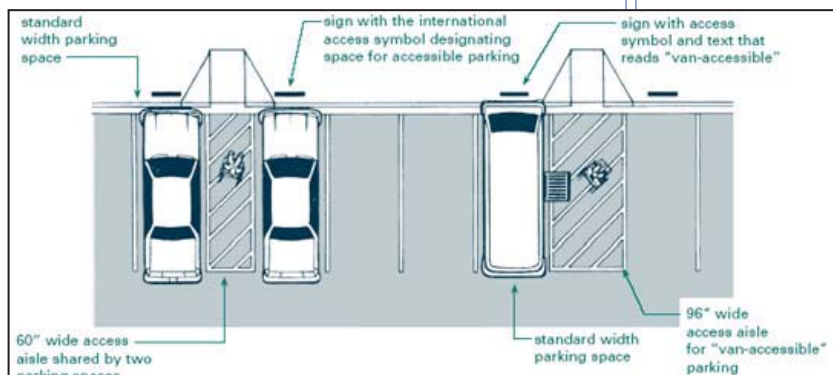
ACCESSIBLE PARKING – FINDINGS

This section of the ADA Self-Evaluation Report assesses the status of City of Clayton's accessible parking.

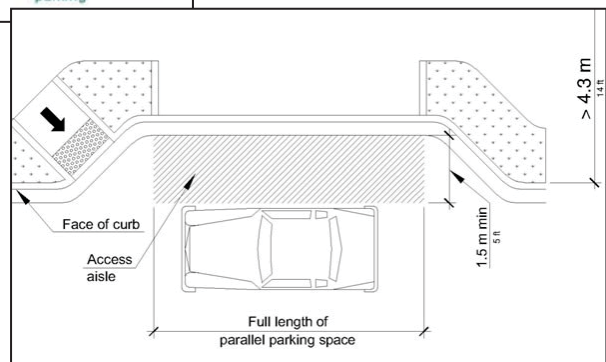
This section of the ADA Self-Evaluation Report assesses the status of City of Clayton's City owned/ leased Parking Lots and On-Street accessible parking. An accessible parking space is a space with an access aisle that adjoins an accessible route. An accessible route is defined as a continuous, unobstructed way of pedestrian passage by means of which an area can be approached, entered, and exited, and which

connects the area with an exterior approach such as a sidewalk, street, or parking area. What are a must be made accessible via an accessible path of travel is broadly defined as an area's "primary function".

The guidelines for identifying barriers with City Parking Lots and On-Street parking are guided by ADAAG and PROWAG, respectively. The Department of Justice places high emphasis for barrier removal for site arrival points and accessible routes. These regulations suggest that a public accommodation's first priority should be to enable individuals with disabilities to physically enter its facility. This priority on "getting through the door" recognizes that providing physical access to a facility from public sidewalks, public transportation, or parking is generally preferable to any alternative arrangements in terms of both business efficiency and the dignity of individuals with disabilities.



Accessible Parking Space



03/ SELF-EVALUATION OF INFRASTRUCTURE Sidewalk and Curb Ramps

▶ ACCESSIBLE PARKING ASSESSMENT

Title II of the ADA requires state and local governments to make parking spaces with parking lots or along a public street accessible to people with disabilities by providing

accessible parking spaces. To comply with ADA requirements, the accessible parking spaces provided, must meet specific dimensional standards for width and length, running slope, cross slope, markings, signage and other features. These guidelines are summarized below.

Accessible Parking Feature	# of Type	% of Total Accessible Parking	# non-compliant	% non-compliant
Identification Sign	27	27	0	0
Sign Height	27	27	23	85
Parking Dimension	27	27	7	24
Parking Slope	27	27	21	72
Access Isle Markings	24	27	3	11
Access Isle Dimensions	24	27	6	22
Access Isle Slope	24	27	18	66
Accessible Route	24	27	13	48

City Parking Lot	# Accessible Spaces	Total # of Spaces	# of Accessible Spaces Required	# of Accessible Spaces Needed
Lot A	4	29	2	0
Lot C	3	51	3	0
Lot D	2	43	2	0
Lot E	11	151	6	0
Lot F	0	7	1	1
Lot G	0	12	1	1
Lot H	0	2	1	1
On Street	7	7	N/A	N/A

See Appendix For current City Parking Lot map.



▶ ACCESSIBLE PARKING FEATURES

- i. **Identification Sign & Height** - Parking space identification signs shall include the International Symbol of Accessibility complying with 703.7.2.1. Signs identifying van parking spaces shall contain the designation "van accessible." Signs shall be 60 inches minimum above the finish floor or ground surface measured to the bottom of the sign.
- ii. **Parking Space Dimensions** - Car parking spaces shall be 96 inches wide minimum and van parking spaces shall be 132 inches wide minimum, shall be marked to define the width, and shall have an adjacent access aisle complying with 502.3. Van parking spaces shall be permitted to be 96 inches wide minimum where the access aisle is 96 inches wide minimum.
- iii. **Parking Space Slope** - Parking spaces shall comply with ADAAG 302. Running and Cross slope to be no more than 2% in any direction. Access aisles shall be at the same level as the parking spaces they serve. Changes in level are not permitted. Ground surfaces shall be stable, firm, and slip resistant.
- iv. **Access Aisle Markings** - Access aisles shall be marked so as to discourage parking in them. The method and color of marking may be addressed by State or local laws or regulations.
- v. **Access Aisle Dimensions** - Access aisles serving car and van parking spaces shall be 60 inches wide minimum and shall extend the full length of the parking spaces they serve.

vi. **Access Aisle Slope** - The access aisle serving the parking spaces shall comply with ADAAG 302. Running and Cross slope to be no more than 2% in any direction. Access aisles shall be at the same level as the parking spaces they serve. Changes in level are not permitted. Ground surfaces shall be stable, firm, and slip resistant.

vii. **Accessible Route** - shall consist of one or more of the following components: walking surfaces with a running slope not steeper than 1:20, other components of accessible routes, such as ramps and curb ramps, are permitted to be more steeply sloped.

▶ ACCESSIBLE PARKING BARRIER ANALYSIS RANKING

Although ADA tells us which accessible parking features are non-standard; it does not offer guidance on which non-standard islands should be replaced first. The final stage of the ADA Self-Evaluation Report process was to screen the 20 accessible parking spaces located within City parking lots and the 7 on-street accessible parking space locations documented in the field inventory to determine which of these parking lots or accessible spaces required more immediate attention.


Accessible parking activity and impedance scores – found in the technical appendix report – were sorted according to their size and divided into three categories (high/medium/ low) using natural breaks. The accessible parking space natural break categories are as follows: (i) activity score (low = 0-25; medium = 26-40; high = 41-100); and, (ii) impedance score (low = 0-22;

03/ SELF-EVALUATION OF INFRASTRUCTURE Sidewalk and Curb Ramps

medium = 23-50; high = 51-100).

As indicated in the table below, of the recorded 20 accessible parking spaces located within City parking lots and the 7 on-street accessible parking spaces:

- 11 percent had a combined high impedance and high activity score.
- 14% percent had a combined high impedance and medium activity score.

Data for the accessible parking spaces activity and impedance were layered to derive a composite barrier ranking score for a particular intersection. Reflected in the table below are the final results of the barrier ranking analysis; categorized using natural breaks as follows: (i) activity score (low = 0-56; medium = 56.1-74; high = 74.1-87); and , (ii) impedance score (low = 0-10; medium = 10.1-30; high = 30.1-75). 

Activity	Impedance	Accessible Parking Spcaes	Percent
High	High	3	11%
High	Medium	0	0%
High	Low	0	0%
Medium	High	14	52%
Medium	Medium	6	22%
Medium	Low	1	3.7%
Low	High	1	3.7%
Low	Medium	1	3.7%
Low	Low	1	3.7%
Total		27	100

04/ TRANSITION PLAN



TRANSITION PLAN -

The Transition Plan combines the findings of the policy, practice and procedures assessments, with the rights-of-way inventory assessment and community review to define specific actionable items over time.

▶ OFFICIAL RESPONSIBLE

The Director of the Public Works Department is responsible for implementing the Transition Plan relative to improving ADA compliance for pedestrian access in the public rights of way. The current contact is:

Matt Malick, PE
Principal Civil Engineer
10 N. Bemiston, Clayton, MO 63105
(314) 290-8540
mmalick@claytonmo.gov

▶ BARRIER REMOVAL PRIORITIZATION

Once each barrier identified was given a barrier ranking score (based on the combined activity and

impedance scores), areas requiring improvement were prioritized within high, medium and low categories. Areas identified for improvement were given weighted points as discussed in the self evaluation report. This prioritization process ensures that the most dangerous issues are remedied first.

▶ FUNDING OPPORTUNITIES

As can be seen in the previous sections and in the extensive reports provided in the Appendix, there is a significant need for barrier removal in the City. Normal funding mechanisms may not be able to address all the needs. There are several alternative funding sources that may be available for the City to address these issues, including federal and state funding, local funding, and private funding. The following information details these different funding sources. While these are not assured, and represent competitive grants, many are worth investigating and pursuing. The City intends to pursue any funding sources which may aid in barrier removal and access improvement.

Federal Sources

- *STP - Surface Transportation Program*
- *HSIP - Highway Safety Improvement Program*
- *TAP - Transportation Alternatives Program*
- *FTA - Federal Transit Capital, Urban & Rural Funds*
- *TrE - Transit Enhancements*
- *402 - State and Community Traffic Safety Program*
- *PLA - State/Metropolitan Planning Funds*



- *TCSP - Transportation and Community and System Preservation Program*
- *SRTS - Safe Routes to School*

Local Sources -

There are several options for local funding for the City to consider. They include:

- *Scheduled / Funded CIP projects that are funded through bonds and sales tax.*
- *CDBG – Community Development Block Grants – identified elements in alignment with priorities that have been adopted by the City.*
- *General fund (from sales tax and bond issue)*
- *Allocation of departmental budgets – requests for larger share to address needs in a more timely fashion*
- *Maintenance funds*
- *Special taxing districts*
- *Tax Increment Financing District (TIF) – A TIF allows cities to create special districts and to make public improvements within those districts that will generate private-sector development. During the development period, the tax base is frozen at the predevelopment level. Property taxes continue to be paid, but taxes derived from increases in assessed values (the tax increment) resulting from new development either go into a special fund created to retire bonds issued to originate the development, or leverage future growth in the district.*
- *Sidewalk or Access Improvement Fee*
- *Transportation User Fee*

Private Sources -

Private funding may be available through local or national foundations, endowments, private development or individuals. Corporate sponsorships could also be considered to help address improvements for accessibility.

▶ COST ESTIMATES & IMPLEMENTATION SCHEDULE

It is not financially feasible to immediately remove all barriers to access. The implementation schedules detailed below will be updated annually by (ADA Coord) to account for progress during the year and also to provide for inclusion of new information or ADA complaints.

The City reserves the right to modify barrier removal priorities in order to allow flexibility in accommodating community requests, petitions for reasonable modifications from persons with disabilities, changes in City programs, and funding constraints and opportunities. It is the goal of this Transition Plan to provide access to the programs, activities and services provided by the City. Interim measures will be explored and implemented in order to provide programmatic access to the public pending the implementation of physical barrier removal projects. The City has ongoing programs that monitor proposed alteration projects and since 2009 all City of Clayton PW resurfacing projects have included the review and upgrades of curb ramps to PROWAG standards.

The City plans to remove barriers within the sidewalk corridors and intersections (both signalized and unsignalized) within a 30 year time

04/TRANSITION PLAN

frame. Sidewalk corridors and intersections will be addressed based on their priority as shown in the Appendix.

The following chart details the estimated citywide costs for addressing improvements, the proposed schedule in years, and the approximate yearly funding needed. Implementation schedules for sidewalks, signals, curb ramps, accessible parking, and street furniture can be found in the Appendices.



Cost Estimate Allocation by Barrier Rank per

Facility Type	Barrier Rank High - Estimate	Barrier Rank Med - Estimate	Barrier Rank Low - Estimate	Total Cost all Barrier-Estimate	Schedule Implement (years)	Approx. Annual Budget
Sidewalks	\$2,624,390	\$4,834,380	\$3,602,440	\$11,061,210	30	*
Interseciton	\$544,350	\$878,450	\$103,450	\$1,526,250	15	*
Accessible Parking	\$4,550	\$16,100	\$4,100	\$24,750	10	*
Street Furniture	\$5,000	\$4,450	\$600	\$10,050	5	*
Curb Ramps	\$416,200	\$718,450	\$291,300	\$1,425,950	5	*
Island Curb Cuts	\$50,000	\$34,700	\$14,200	\$98,900	15	*
Signals	\$8,800	\$7,550	\$4,250	\$20,600	15	*
Total	\$3,653,290	\$6,494,080	\$4,020,340	\$14,167,710	30	\$100k+*

*Approx. Annual Budget expected to incrementally increase over implementation period (initial value shown)
Costs shown is present day estimate, actual costs will be greater due to yearly inflation over implementation period.

05/ APPENDIX



▶ DEFINITIONS

Following is a summary of many definitions found in 2010 ADA Standards for Accessible Design. Please refer to the Americans with Disabilities Act of 1990, the Title II Technical Assistance Manual, and the 2010 ADA Standards for Accessible Design as amended through September 2002 for the full text of definitions and explanations.

Accessible Route

An accessible route is a continuous unobstructed path that connects all accessible elements and spaces of a building or facility and that complies with Chapter 4 of ADAAG. Interior accessible routes may include corridors, floors, ramps, elevators, lifts, and clear floor space at fixtures. Exterior accessible routes may include parking access aisles, curb ramps, crosswalks at vehicular ways, walks, ramps, and lifts.

Auxiliary Aids and Services

The term auxiliary aids and services includes:

- *Qualified interpreters or other effective methods of making orally delivered materials available to individuals with hearing impairments;*
- *Qualified readers, taped texts, or other effective methods of making visually delivered materials available to individuals with visual impairments;*
- *Acquisition or modification of equipment or devices; and*
- *Other similar services and actions.*

Complaint

A complaint is a claimed violation of the ADA.

Curb Ramp

A curb ramp is a short ramp cutting through a curb or built up to it.

Detectable Warning

Detectable warnings are standardized surface features built in or applied to walking surfaces or other elements to warn individuals with visual impairments of hazards on a circulation path.

Disability

The term disability means, with respect to an individual:

- *A physical or mental impairment that substantially limits one or more of the major life activities of such*



individual;

- *A record of such impairment; or*
- *Being regarded as having such impairment.*

A. Qualified Individual with a Disability

A qualified individual with a disability means an individual with a disability who, with or without reasonable modification to rules, policies, or practices; the removal of architectural, communication, or transportation barriers; or the provision of auxiliary aids and services, meets the essential eligibility requirements for the receipt of services or the participation in programs or activities provided by the City.

B. Regarded as Having a Disability

An individual is disabled if she or he is treated or perceived as having an impairment that substantially limits major life activities, although no such impairment exists.

Discrimination on the Basis of Disability

Discrimination on the basis of disability means to:

- *Limit, segregate, or classify a citizen in a way that may adversely affect opportunities or status because of the person's disability;*
- *Limit, segregate, or classify a participant in a program or activity offered to the public in a way that may adversely affect opportunities or status because of the participant's disability;*
- *Participate in a contract that could subject a citizen with a disability to discrimination;*
- *Use any standards, criteria, or methods of administration that have the effect of discriminating on the basis of disability;*
- *Fail to make reasonable modifications to accommodate known physical or mental limitations of an otherwise qualified individual with a disability unless it can be shown that the modification would impose an undue burden on the City's operations;*
- *Use selection criteria that exclude otherwise qualified people with disabilities from participating in the programs or activities offered to the public; and*
- *Fail to use tests in a manner that ensures that the test results accurately reflect the qualified applicant's skills or aptitude to participate in a program or activity.*

HIV

The infectious agent known as Human Immune Virus, Human Immunodeficiency Virus, HTLVIII, LAC, or AIDS virus.

A. HIV Condition

AIDS, ARC, or HIV Infection.

Physical or Mental Impairments

Physical or mental impairments may include, but are not limited to: vision, speech, and hearing impairments; emotional disturbance and mental illness; seizure disorders; mental retardation; orthopedic and neuromotor disabilities; learning disabilities; diabetes; heart disease; nervous conditions; cancer; asthma; hepatitis B; HIV infection (HIV condition); and drug abuse if the drug user has successfully completed or is participating in a rehabilitation program and no longer uses illegal drugs.

The following conditions are not physical or mental impairments: transvestitism; transexualism; current illegal drug use; homosexuality or bisexuality; compulsive gambling; kleptomania; pyromania; pedophilia; exhibitionism; voyeurism; pregnancy; height; weight; eye color; hair color; left-handedness; poverty; lack of education; a prison record; and poor judgment or quick temper that are not symptoms of a mental or physiological disorder.

Having a Record of Impairment

An individual is disabled if he or she has a history of having an impairment that substantially limits the performance of a major life activity; or has been diagnosed, correctly or incorrectly, as having such impairment.

Marked Crossing

A marked crossing is a crosswalk or other identified path intended for pedestrian use in crossing a vehicular way.

Program Accessibility

A public entity's services, programs, or activities, when viewed in their entirety, must be readily accessible to and usable by individuals with disabilities. This standard, known as program accessibility, applies to all existing City facilities.

Reasonable Modification

If individuals' disabilities prevent them from performing the essential functions of the program or activity, it is necessary to determine whether reasonable modification would enable these individuals to perform the essential functions of the program or activity.

Reasonable modification is any change in program or activity or in the way things are customarily done that enables an individual with a disability to enjoy equal program opportunities. Modifications may mean adjustments:

- *To a registration or application process to enable an individual with a disability to be considered for the program or activity; and*
- *That enable individuals with disabilities to enjoy equally the benefits of the program or activity as other similarly situated individuals without disabilities enjoy.*

Modification includes making existing facilities and equipment used by individuals readily accessible and usable by individuals with disabilities.

Modification applies to:



- *All decisions and to the application or registration process;*
- *All services provided in connection with the program or activity; and*
- *Known disabilities only.*

Modification is not required if:

- *It changes the essential nature of a program or activity;*
- *It creates a hazardous situation; or*
- *It poses an undue burden.*

Service Animal

The ADA defines a service animal as any guide dog, signal dog, or other animal individually trained to provide assistance to an individual with a disability. If they meet this definition, animals are considered service animals under the ADA regardless of whether they have been licensed or certified by a state or local government. Service animals perform some of the functions and tasks that the individual with a disability cannot perform for him or herself. Some of the typical uses of service animals are:

- *Guiding persons with visual impairments;*
- *Alerting persons with hearing impairments to sounds;*
- *Pulling wheelchairs or carrying and picking up things for persons with mobility impairments; or*
- *Assisting persons with mobility impairments to maintain their balance. Although a number of states have programs to certify service animals, agencies or businesses may not insist on proof of state certification before permitting the service animal to accompany the person with a disability.*

Substantial Limitation of Major Life Activities

An individual is disabled if she or he has a physical or mental impairment that:

- *Renders her or him unable to perform a major life activity, or*
- *Substantially limits the condition, manner, or duration under which she or he can perform a particular major life activity in comparison to other people.*

Major life activities are functions such as caring for oneself, performing manual tasks, walking, seeing, hearing, speaking, breathing, learning, and working.

In determining whether a physical or mental impairment substantially limits the condition, manner, or duration under which an individual can perform a particular major life activity in comparison to other people, the following factors shall be considered:

- *The nature and severity of the impairment;*
- *The duration or expected duration of the impairment; and*
- *The permanent or long-term impact (or expected impact) of or resulting from the impairment*

▶ ADA GRIEVANCE FORM

CITY OF CLAYTON, MISSOURI

**Title II of the Americans with Disabilities Act Section 504 of the
Rehabilitation Act of 1973 Discrimination Complaint Form**

Instructions: Please fill out this form completely, in black ink or type. Sign and return to the address on page 3.

Complainant: _____

Address: _____

City, State and Zip Code: _____

Telephone: Home: _____ Business/Other: _____

Person Discriminated Against: _____

(if other than the complainant)

Address: _____

City, State, and Zip Code: _____

Telephone: Home: _____ Business/Other: _____

Government, or organization, or institution which you believe has discriminated:

Name: _____

Address: _____

County: _____

City: _____

State and Zip Code: _____

Telephone Number: _____



Discrimination Complaint Form

When did the discrimination occur? Date: _____

Describe the acts of discrimination providing the name(s) where possible of the individuals who discriminated (use space on page 3 if necessary):

Have efforts been made to resolve this complaint through the internal grievance procedure of the government, organization, or institution? Yes _____ No _____ If yes: what is the status of the grievance?

Has the complaint been filed with another bureau of the Department of Justice or any other Federal, State, or local civil rights agency or court? Yes _____ No _____

If yes: Agency or Court: _____

Contact Person: _____

Address: _____

City, State, and Zip Code: _____

Telephone Number: _____

Date Filed: _____

Discrimination Complaint Form

Do you intend to file with another agency or court? Yes_____ No_____

Agency or Court: _____

Address: _____

City, State and Zip Code: _____

Telephone Number: _____

Additional space for answers:

Signature: _____

Date: _____

Return to:

City of Clayton, ADA Coordinator 10 Bemiston, Ave.,

Clayton, MO 63105



▶ ADA GRIEVANCE PROCEDURE

Purpose of Guidelines:

These guidelines are intended to ensure that discrimination complaints are handled promptly, effectively and equitably.

Overview of Grievance Procedure:

1. A written grievance should be filed with the City of Clayton, using the City's ADA Grievance Form. The ADA Grievance Form can be found at www.claytonmo.gov. If the grievance is not filed on the City of Clayton ADA Grievance Form, it should nonetheless contain the following information:

- ***The name, address, and telephone number of the person alleging the ADA violation, if other than the person filing the complaint.***
- ***A description of the alleged violation and the remedy sought. Information regarding whether a grievance has been filed with the Department of Justice or other agency. If a grievance has been filed, the name of the agency or court where the complaint was filed, the date the grievance was filed, and the name, address and telephone number of a contact person with the agency with which the complaint was filed.***

2. An oral grievance can be filed by contacting the Lead ADA Coordinator. The oral grievance will be documented in writing by the ADA Coordinator utilizing the ADA Grievance Form and will be signed by the complainant.

3. The grievance will be responded to within 20 working days of receipt of grievance from complainant.


4. Within 60 calendar days of receipt, the ADA Coordinator will forward the grievance to Compliance Investigations who will conduct the investigation necessary to determine the validity of the alleged violation. If appropriate, the ADA Coordinator and/or Compliance Investigator will arrange to meet with the complainant to discuss the matter and attempt to reach, or mediate a

resolution of the grievance.

5. If an informal resolution and mediation of the grievance is not reached, a written determination as to the validity of the complaint and description of the resolution, if appropriate, shall be issued by the City of Clayton's City Manager and a copy forwarded to the complainant no later than 90 days from the date of the City's receipt of the complaint.

6. The complainant may request reconsideration if he/she is dissatisfied with the written determination. The request for reconsideration shall be in writing and filed with the City of Clayton, Human Rights Department, 10 N. Bemiston Ave., Clayton, MO, 63105 within 30 days after the City Manager's determination has been mailed to the complainant. The City of Clayton's City Attorney shall review the request for reconsideration and make a final determination within 90 days from the filing of the request for reconsideration.

7. If the complainant is dissatisfied with City's handling of the grievance at any stage of the process or does not wish to file a grievance by utilizing the City's ADA Grievance Procedure, the complainant may file a grievance directly with the United States Department of Justice or other appropriate state or federal agency. Use of the City's grievance procedure is not a prerequisite to the pursuit of other remedies.

The resolution of any specific complaint will require consideration of varying circumstances, such as, the specific nature of the disability, the nature of the access to services, programs, or facilities at issue and the essential eligibility requirements for participation. Also, areas to consider would be the health and safety of others, the degree to which an accommodation would constitute a fundamental alteration to the program, service, or facility, or cause an undue hardship to the City. Accordingly, the resolution by the City of any one grievance does not constitute a precedent upon which the City is bound or upon which other complaining parties may rely. 

▶ NOTICE OF PUBLIC HEARING/OPEN HOUSE

PUBLIC HEARING/OPEN HOUSE- March 13th, 2014, 4-7pm, the City of Clayton will hold a public meeting discussing the City's Draft ADA Transition Plan for Public Rights of Way in the Council Chambers, Room 201 of City Hall.

The City has developed a Transition Plan under the Americans with Disabilities Act (ADA) for Pedestrian Facilities in the Public Right of Way. The Plan describes how the City intends to remove barriers for disabled persons who enter or who are in the public right of way. As the City of Clayton works to ensure that programs and services within the public right of way are accessible to individuals with disabilities, residents are encouraged to participate and offer feedback. This Transition Plan focuses and describes the needs within the community for curb ramps, pedestrian signals, sidewalks and other pedestrian facilities, as well as a process to implement improvements over time in order to comply with the law. The draft ADA Transition Plan is intended to identify needs and plan improvements, creating safe and usable pedestrian facilities for each and every member within the community, and assuring compliance with all federal, state, and local regulations and standards.

The public is invited to attend the meeting. There will be a brief project presentation at 4:45pm and again at 6pm. Before and after each presentation, the meeting will be conducted in an open-house format to increase the opportunity for public comment and to allow one-on-one discussion with project team members. Comments can be submitted for the public record in writing at the meeting. The City will also provide assistance for individuals to record their verbal comments during the meeting. In addition, written or e-mailed comments will be accepted through 5pm March 31, 2014. The draft plan is available in alternate formats, such as Braille and audio recordings, at no charge. To request an alternate format, please contact the project manager listed below.

Comments and questions should be directed to the project manager: Steve Meyer, P.E., City of Clayton, Public Works Department, 10 North Bemiston Avenue, Clayton, MO 63105. Comments may also be submitted via email to smeyer@claytonmo.gov.

Please note: Reasonable efforts will be made to assist and accommodate disabled persons desiring to attend the meeting. Requests for auxiliary aids or services to assist individuals with disabilities or limited English proficiency should be made to City Clerk at 290.8469 or Relay Missouri at 1.800.735.2966 (TDD) at least two working days prior to the meeting.



▶ MEETING MINUTES - DISABILITY COMMUNITY FOCUS GROUP

Meeting Minutes

City of Clayton Open House
Draft ADA Transition Plan
4/9/2014

The Society of the Blind and Visually Impaired contacted the City of Clayton requesting input on the City of Clayton's Draft ADA Transition Plan. The City of Clayton and Cole & Associates met with representatives of SBVI and walked the streets of Clayton on April 9th, 2014. The group discussed pros and cons of different environmental adaptations at the intersections and the path of travel between corners.

SVBI provided a detailed letter following the meeting with a list of beneficial intersection components. List of suggestions is as follows:

- Curb cuts with raised edges provide a barrier and assist in aligning at crossings.
- Textured crosswalks help a person with a white cane avoid veering during the crossing into or away from a parallel street.
- Increase the accessibility of pedestrian signal button by placing within arm's reach of truncated mat.
- Audible signals, including countdown, street name, and change of traffic light indicator.
- Positioning of truncated mats.
- At intersections with greater pedestrian traffic (including intersections where pedestrians with disabilities may regularly cross), the following would be helpful:
 - a. Increased intersection signal timing length.
 - b. Pedestrian crossing signage such as "No Right Turn on Red" and "Stop or Yield to Pedestrians in crosswalk."

▶ SOCIETY FOR THE BLIND RECOGNITION LETTER



From the City of Clayton
10 N. Bemiston
Clayton, MO 63105
www.claytonmo.gov

Contact: Judy Kekich 314 290 8473
jakekich@ci.clayton.mo.us

FOR IMMEDIATE RELEASE

Clayton Is Honored for Its Commitment to Accessibility

September 27, 2011 - Clayton, MO: At the Family Festival on Saturday, September 17, representatives from the Society for the Blind and Visually Impaired presented an award to City of Clayton Mayor Linda Goldstein recognizing the city for pedestrian improvements in downtown Clayton.

The St. Louis Society for the Blind acknowledged the Clayton for its commitment to full accessibility for those who are blind and visually impaired. Over the years, the city has worked to make sidewalks and street-crossings safe which encourages independence through the use of curb-cuts and audible crossing lights at pedestrian intersections.

"The City of Clayton has been one of the earliest and most aggressive municipalities to recognize and embrace the importance of these being included in the planning and design in new construction," said the Society's President David Ekin.

The Society for the Blind has also recognized Clayton for the development and recent installation of the Sensory Garden and fully accessible playground in Shaw Park. # # #

Society for the Blind and Visually Impaired: Celebrating its 100th anniversary in 2011, the not-for-profit Society serves an increasing number of older adults who are newly visually impaired or blind due to age-related eye conditions with home-based services, specialized agency services and



▶ ACCESSIBLE PARKING MAP



▶ COST & DATA SUMMARIES

The following pages contain cost projection summaries and data summaries for the following categories:

- Intersection Cost Projection Summary
- Intersection Data Summary
- Sidewalk Cost Projection Summary
- Sidewalk Data Summary
- Curb Ramp Cost Projection Summary
- Curb Ramp Data Summary
- Signal Cost Projection Summary
- Signal Data Collection Summary
- Island Refuge Cost Projection Summary
- Island Refuge Data Summary
- Accessible Parking Cost Projection Summary
- Accessible Parking Data Summary
- Benches Cost Projection Summary
- Benches Data Summary
- Crosswalk Data Summary



City of Clayton ADA Transition Plan
Intersection Cost Projection Summary



Intersection ID	Location/Intersection	Priority	Cost
193	WHITBURN & CLAYSHIRE	High	\$9,000.00
167	WYDOWN & SHEPLEY	High	\$9,000.00
53	BRENTWOOD & UNIVERSITY	High	\$22,850.00
10	FRANCIS & LANGTON	High	\$9,000.00
187	DAVIS & BILTMORE	High	\$11,650.00
270	SHAW PARK & CENTRAL	High	\$9,000.00
56	MARYLAND & BEMISTON	High	\$16,000.00
223	CONCORDIA & PRIVATE	High	\$2,000.00
116	BRENTWOOD & BONHOMME	High	\$8,000.00
21	CRESCENT & SEMINOLE	High	\$5,700.00
82	BRENTWOOD, CORPORATE PK & DAYTONA	High	\$24,000.00
48	BRENTWOOD & PERSHING	High	\$13,000.00
76	BRENTWOOD & FORSYTH	High	\$18,100.00
134	CARSWOLD & WALINCA	High	\$2,000.00
191	BEMISTON & MOHAWK	High	\$7,000.00
106	HANLEY & WESTMORELAND	High	\$4,000.00
220	CLAYTON & RIDGEMOOR	High	\$4,000.00
75	TOPTON & MARYLAND	High	\$14,950.00
218	SEMINARY & ALAMO	High	\$10,000.00
77	BRENTWOOD & MARYLAND	High	\$20,900.00
231	BRENTWOOD & SHAW PARK	High	\$6,200.00
57	MARYLAND & LINDEN	High	\$4,000.00
123	BRENTWOOD & ROSILINE	High	\$4,000.00
79	MARYLAND & BRIGHTON	High	\$6,000.00
44	HANLEY & REAR	High	\$6,000.00
121	HANLEY & SHIRLEY	High	\$6,000.00
294	ST RITA & PRIVATE	High	\$2,000.00
176	WESTWOOD & BYRON	High	\$8,000.00
71	BRENTWOOD & WESTMORELAND	High	\$6,000.00
166	WYDOWN & WESTWOOD	High	\$2,000.00
45	HANLEY & COUNTRY CLUB	High	\$4,000.00
114	CARONDELET, LYLE & COLORADO	High	\$17,000.00
54	CENTRAL & MARYLAND	High	\$22,000.00
175	WESTWOOD & PARKDALE	High	\$8,000.00
279	CARONDELET & MERAMEC	High	\$8,000.00
55	MARYLAND & MERAMEC	High	\$29,000.00
245	ALAMO & DE MUN	High	\$14,000.00
118	EDGEWOOD & CARSWOLD	High	\$4,000.00
80	MARYLAND & FORSYTH	High	\$12,350.00
142	FOREST & WYDOWN	High	\$8,900.00
136	BRENTWOOD, CORPORATE PK & ORLANDO	High	\$18,750.00
216	SAN BONITA & DE MUN	High	\$4,000.00
102	ALAMO & PRIVATE	High	\$6,000.00
190	WESTWOOD & OXFORD	High	\$8,000.00
63	SUDBURY & ROXBURGH	High	\$4,000.00



City of Clayton ADA Transition Plan
Intersection Cost Projection Summary



Intersection ID	Location/Intersection	Priority	Cost
301	SHAW PARK & MERAMEC	High	\$8,000.00
9	BRENTWOOD & WALINCA	High	\$14,000.00
98	CONCORDIA & PRIVATE	High	\$6,000.00
43	ELLENWOOD & UNIVERSITY	High	\$8,000.00
246	SAN BONITA & DE MUN	High	\$10,000.00
199	WESTWOOD & YORK	High	\$8,000.00
61	GLEN RIDGE & YORK	High	\$16,000.00
300	CORPORATE PK & SHAW PARK	High	\$4,000.00
3	CROMWELL & GLEN RIDGE	High	\$16,000.00
171	BYRON & GLEN RIDGE	High	\$16,000.00
40	BONHOMME & MERAMEC	Medium	\$24,000.00
87	SAN BONITA & ST RITA	Medium	\$4,000.00
311	RITZ CARLTON & COLORADO	Medium	\$7,200.00
109	CENTRAL & FORSYTH	Medium	\$30,000.00
137	POLO & POLO	Medium	\$2,000.00
244	CARONDELET & CENTRAL	Medium	\$12,000.00
181	GLEN RIDGE & BUCKINGHAM	Medium	\$16,000.00
74	TOPTON & MARK TWAIN	Medium	\$4,000.00
113	BRENTWOOD & CARONDELET	Medium	\$8,400.00
222	CLAYTON & CLAYTONIA	Medium	\$2,000.00
130	HANLEY & WALINCA	Medium	\$4,000.00
133	UNIVERSITY & CECIL	Medium	\$8,000.00
39	BONHOMME & CENTRAL	Medium	\$20,000.00
147	HANLEY & POLO	Medium	\$2,000.00
47	BRENTWOOD & PERSHING	Medium	\$4,000.00
125	POLO & N POLO	Medium	\$4,000.00
183	WESTWOOD & BUCKINGHAM	Medium	\$10,000.00
108	MERAMEC & FORSYTH	Medium	\$21,050.00
107	MARYLAND & LEE	Medium	\$6,000.00
198	AUDUBON & YORK	Medium	\$6,000.00
115	CARONDELET & RITZ CARLTON	Medium	\$10,200.00
210	AUDUBON & SOMERSET	Medium	\$8,000.00
67	GAY & LADUE & MARYLAND	Medium	\$12,000.00
207	CRESTWOOD & HILLVALE	Medium	\$16,000.00
259	BEMISTON & SHAW PARK	Medium	\$4,000.00
247	DE MUN & SOUTHWOOD	Medium	\$6,000.00
213	CRESTWOOD & CLAVERACH	Medium	\$6,000.00
62	FRANCIS & ROXBURGH	Medium	\$4,000.00
257	BIG BEND & SOUTHMOOR	Medium	\$8,000.00
89	TOPTON & MARK TWAIN	Medium	\$4,000.00
73	MARYLAND & LANCASTER	Medium	\$4,000.00
211	BIG BEND & TUSCANY	Medium	\$4,000.00
243	BEMISTON & CARONDELET	Medium	\$10,000.00
15	BRENTWOOD & VENETIAN	Medium	\$4,000.00
304	HANLEY & CLAYTON	Medium	\$4,000.00



City of Clayton ADA Transition Plan
Intersection Cost Projection Summary



Intersection ID	Location/Intersection	Priority	Cost
99	CONCORDIA & ALAMO	Medium	\$4,000.00
201	BIG BEND & OAK KNOLL	Medium	\$4,000.00
214	SAN BONITA & SEMINARY	Medium	\$14,000.00
274	WALLACE & FORSYTH	Medium	\$6,500.00
72	BRENTWOOD & WESTMORELAND	Medium	\$7,650.00
127	HANLEY & REAR	Medium	\$4,000.00
150	POLO & POLO	Medium	\$2,000.00
90	POLO & MIDDLE POLO	Medium	\$8,000.00
122	CARSWOLD & SHIRLEY	Medium	\$8,000.00
197	WESTWOOD & WELLINGTON	Medium	\$8,650.00
111	PARKSIDE & BRIGHTON	Medium	\$6,000.00
182	AUDUBON & BUCKINGHAM	Medium	\$8,000.00
91	POLO & MIDDLE POLO	Medium	\$8,000.00
296	BIG BEND & NORTHMOOR	Medium	\$8,000.00
172	AUDUBON & PARKDALE	Medium	\$8,000.00
49	CENTRAL & PERSHING	Medium	\$6,650.00
309	JACKSON & MARYLAND	Medium	\$8,000.00
33	KINGSBURY & CENTRAL	Medium	\$8,650.00
170	AUDUBON & BYRON	Medium	\$8,000.00
238	FORSYTH, JACKSON & CARONDELET	Medium	\$25,800.00
51	PERSHING & BEMISTON	Medium	\$16,000.00
69	MARYLAND & CRANDON	Medium	\$4,000.00
110	FORSYTH & BEMISTON	Medium	\$21,050.00
248	DE MUN & SAN BONITA	Medium	\$4,000.00
8	WALINCA & MERAMEC	Medium	\$4,000.00
32	KINGSBURY & BEMISTON	Medium	\$12,000.00
174	PARKDALE & GLEN RIDGE	Medium	\$16,000.00
153	BRENTWOOD & WATKINS	Medium	\$4,000.00
217	BIG BEND & TUSCANY	Medium	\$4,000.00
165	WYDOWN & EDGEWOOD	Medium	\$3,550.00
186	ARUNDEL & UNIVERSITY	Medium	\$10,000.00
234	DE MUN & ROSEBURY	Medium	\$6,000.00
276	DE MUN & NORTHWOOD	Medium	\$4,000.00
124	SHIRLEY & POLO & REAR	Medium	\$2,000.00
2	AUDUBON & CROMWELL	Medium	\$10,000.00
184	DARTFORD & ARUNDEL	Medium	\$8,000.00
6	BRENTWOOD & KINGSBURY	Medium	\$4,000.00
70	MERAMEC & WESTMORELAND	Medium	\$4,000.00
78	FORSYTH & PARKSIDE	Medium	\$4,000.00
168	UNIVERSITY & WYDOWN	Medium	\$16,750.00
81	BRENTWOOD & DAVIS	Medium	\$4,000.00
169	WYDOWN & WYDOWN	Medium	\$4,000.00
258	BIG BEND & FOREST RIDGE	Medium	\$8,000.00
177	UNIVERSITY & WYDOWN	Medium	\$8,000.00
307	BLAND & NORTHMOOR	Medium	\$2,000.00



City of Clayton ADA Transition Plan
Intersection Cost Projection Summary



Intersection ID	Location/Intersection	Priority	Cost
195	GLEN RIDGE & WELLINGTON	Medium	\$16,000.00
68	FORSYTH & WESTMORELAND	Medium	\$4,950.00
158	POLO & POLO	Medium	\$4,000.00
265	DE MUN & N/A	Medium	\$4,000.00
250	DE MUN & ARUNDEL	Medium	\$12,000.00
226	BRENTWOOD & WHITBURN	Medium	\$4,000.00
138	EDGEWOOD & WALINCA	Medium	\$4,000.00
253	DE MUN & WYDOWN	Medium	\$4,000.00
154	UNIVERSITY & ABERDEEN	Medium	\$16,000.00
7	BRENTWOOD & STRATFORD	Medium	\$4,000.00
291	FORSYTH & LYLE	Medium	\$4,000.00
192	FRANCIS & WHITBURN	Medium	\$12,000.00
41	BONHOMME & BEMISTON	Medium	\$18,000.00
129	MERAMEC & ROSILINE	Medium	\$500.00
52	FORSYTH & UNIVERSITY	Medium	\$13,250.00
86	CLAYTON & SOMERSET	Medium	\$2,000.00
188	AUDUBON & OXFORD	Medium	\$10,000.00
157	WYDOWN & ASBURY	Medium	\$8,000.00
148	DARTFORD & ABERDEEN	Medium	\$8,000.00
225	BRENTWOOD & FRANCIS	Medium	\$10,650.00
27	BRENTWOOD & CLAYTON LANE	Medium	\$4,000.00
233	DE MUN & ROSEBURY	Medium	\$4,000.00
232	SHAW PARK & SHAW PARK	Medium	\$2,000.00
131	BIG BEND & SHEPLEY	Medium	\$4,000.00
251	DE MUN & ABERDEEN	Medium	\$14,600.00
200	BIG BEND & SOUTHMOOR	Medium	\$4,000.00
263	RITZ CARLTON & CARONDELET	Medium	\$4,000.00
196	AUDUBON & WELLINGTON	Medium	\$10,000.00
278	WYDOWN & WYDOWN TERRACE	Medium	\$8,000.00
35	KINGSBURY & MERAMEC	Medium	\$4,400.00
189	OXFORD & GLEN RIDGE	Medium	\$16,000.00
292	FORSYTH & LYLE	Low	\$3,000.00
164	WYDOWN & GLEN RIDGE	Low	\$500.00
132	BIG BEND & BRENTMOOR	Low	\$4,000.00
277	DE MUN & ARUNDEL	Low	\$4,650.00
112	FORSYTH & LEE	Low	\$2,700.00
13	MERAMEC & ORLANDO	Low	\$1,950.00
149	WYDOWN & DARTFORD	Low	\$4,700.00
297	BIG BEND & PRIVATE	Low	\$4,000.00
4	KINGSBURY & BRENTWOOD	Low	\$2,650.00
203	CRESCENT & OLETA	Low	\$6,250.00
50	MERAMEC & PERSHING	Low	\$2,400.00
205	BILTMORE & BILTMORE	Low	\$3,150.00
119	EDGEWOOD & SHIRLEY	Low	\$4,000.00
23	BEMISTON & BILTMORE	Low	\$2,250.00



City of Clayton ADA Transition Plan
Intersection Cost Projection Summary



Intersection ID	Location/Intersection	Priority	Cost
20	DAVIS & CENTRAL	Low	\$2,450.00
14	MERAMEC & VENETIAN	Low	\$2,200.00
204	OLETA & SEMINOLE	Low	\$2,250.00
141	MERAMEC & DAVIS	Low	\$3,550.00
194	BILTMORE & MOHAWK	Low	\$700.00
36	FORSYTH, BRIGHTON & PERSHING	Low	\$9,850.00
161	WYDOWN & BRENTMOOR	Low	\$5,750.00
5	FORSYTH, TOPTON & KINGSBURY	Low	\$13,700.00
152	MERAMEC & DAYTONA	Low	\$2,400.00
19	DAVIS & CENTRAL	Low	\$1,800.00
31	GAY & COLONIAL	Low	\$2,650.00
18	FORSYTH & OLYMPIAN	Low	\$1,450.00
159	WYDOWN & BRENTMOOR	Low	\$500.00
162	WYDOWN & AUDUBON	Low	\$900.00
160	WYDOWN & CRESTWOOD	Low	\$2,200.00
266	FORSYTH & EAST	Low	\$2,900.00
17	FORSYTH & STRATFORD	Low	\$2,000.00

Intersection Data Reference Guide

The following list is reference for the headers (1-15) in the intersection data spreadsheet.

1. Intersection ID
2. Location
3. Ramp Count
4. Signal Count
5. Island Refuge Count
6. Activity Score
7. Impedance Score
8. Final Score
9. Activity Ranking
10. Impedance Ranking
11. Final Ranking
12. Ramps Cost
13. Signals Cost
14. Island Refuges Cost
15. Total Cost



City of Clayton ADA Transition Plan
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1	2	3	4	5	6	7	8	9	10	11	12	13	14	15
193	WHITBURN & CLAYSHIRE	0	2	1	31.5	173.5	205.0	Med	High	High	\$4,000	\$0	\$5,000	\$9,000
167	WYDOWN & SHEPLEY	0	2	1	29.5	145.0	174.5	Med	High	High	\$4,000	\$0	\$5,000	\$9,000
53	BRENTWOOD & UNIVERSITY	0	8	2	55.5	118.8	174.3	High	High	High	\$12,850	\$0	\$10,000	\$22,850
10	FRANCIS & LANGTON	0	2	1	19.5	152.5	172.0	Low	High	High	\$4,000	\$0	\$5,000	\$9,000
187	DAVIS & BILTMORE	0	4	1	19.5	140.5	160.0	Low	High	High	\$6,650	\$0	\$5,000	\$11,650
270	SHAW PARK & CENTRAL	0	2	1	79	80.0	159.0	High	Med	High	\$4,000	\$0	\$5,000	\$9,000
56	MARYLAND & BEMISTON	4	6	0	72	80.9	152.9	High	Med	High	\$12,000	\$4,000	\$0	\$16,000
223	CONCORDIA & PRIVATE	0	1	0	44.5	100.0	144.5	Med	High	High	\$2,000	\$0	\$0	\$2,000
116	BRENTWOOD & BONHOMME	3	4	0	71.5	70.5	142.0	High	Med	High	\$8,000	\$0	\$0	\$8,000
21	CRESCENT & SEMINOLE	0	2	1	15.5	125.0	140.5	Low	High	High	\$700	\$0	\$5,000	\$5,700
82	BRENTWOOD, CORPORATE PK & DAYTONA	2	8	1	48.5	91.6	140.1	Med	High	High	\$14,600	\$4,400	\$5,000	\$24,000
48	BRENTWOOD & PERSHING	0	4	1	40.5	97.5	138.0	Med	High	High	\$8,000	\$0	\$5,000	\$13,000
76	BRENTWOOD & FORSYTH	3	6	0	69	67.7	136.7	High	Med	High	\$12,000	\$6,100	\$0	\$18,100
134	CARSWOLD & WALINCA	0	1	0	35.5	100.0	135.5	Med	High	High	\$2,000	\$0	\$0	\$2,000
191	BEMISTON & MOHAWK	0	2	1	22.5	112.5	135.0	Low	High	High	\$2,000	\$0	\$5,000	\$7,000
106	HANLEY & WESTMORELAND	0	2	0	46.5	85.0	131.5	Med	Med	High	\$4,000	\$0	\$0	\$4,000
220	CLAYTON & RIDGEMOOR	0	2	0	29.5	100.0	129.5	Med	High	High	\$4,000	\$0	\$0	\$4,000
75	TOPTON & MARYLAND	4	8	0	45	83.9	128.9	Med	Med	High	\$14,450	\$500	\$0	\$14,950
218	SEMINARY & ALAMO	0	5	0	55.5	73.0	128.5	High	Med	High	\$10,000	\$0	\$0	\$10,000
77	BRENTWOOD & MARYLAND	2	4	0	73	54.3	127.3	High	Med	High	\$8,000	\$12,900	\$0	\$20,900
231	BRENTWOOD & SHAW PARK	1	4	0	94	32.0	126.0	High	Low	High	\$6,200	\$0	\$0	\$6,200
57	MARYLAND & LINDEN	0	2	0	64.5	60.0	124.5	High	Med	High	\$4,000	\$0	\$0	\$4,000
123	BRENTWOOD & ROSILINE	0	2	0	64	60.0	124.0	High	Med	High	\$4,000	\$0	\$0	\$4,000
79	MARYLAND & BRIGHTON	0	3	0	55	68.3	123.3	High	Med	High	\$6,000	\$0	\$0	\$6,000
44	HANLEY & REAR	0	3	0	56.5	65.0	121.5	High	Med	High	\$6,000	\$0	\$0	\$6,000
121	HANLEY & SHIRLEY	0	3	0	53.5	66.7	120.2	High	Med	High	\$6,000	\$0	\$0	\$6,000
294	ST RITA & PRIVATE	0	1	0	59.5	60.0	119.5	High	Med	High	\$2,000	\$0	\$0	\$2,000
176	WESTWOOD & BYRON	0	4	0	52	67.5	119.5	High	Med	High	\$8,000	\$0	\$0	\$8,000
71	BRENTWOOD & WESTMORELAND	0	3	0	52.5	65.0	117.5	High	Med	High	\$6,000	\$0	\$0	\$6,000
166	WYDOWN & WESTWOOD	0	1	0	62	55.0	117.0	High	Med	High	\$2,000	\$0	\$0	\$2,000
45	HANLEY & COUNTRY CLUB	0	2	0	46.5	70.0	116.5	Med	Med	High	\$4,000	\$0	\$0	\$4,000
114	CARONDELET, LYLE & COLORADO	0	6	1	36	80.3	116.3	Med	Med	High	\$12,000	\$0	\$5,000	\$17,000
54	CENTRAL & MARYLAND	0	4	0	75	41.0	116.0	High	Low	High	\$8,000	\$14,000	\$0	\$22,000
175	WESTWOOD & PARKDALE	0	4	0	49	66.3	115.3	Med	Med	High	\$8,000	\$0	\$0	\$8,000
279	CARONDELET & MERAMEC	0	4	0	76	38.8	114.8	High	Low	High	\$8,000	\$0	\$0	\$8,000
55	MARYLAND & MERAMEC	0	5	1	63	51.5	114.5	High	Med	High	\$10,000	\$14,000	\$5,000	\$29,000
245	ALAMO & DE MUN	0	7	0	49.5	63.6	113.1	Med	Med	High	\$14,000	\$0	\$0	\$14,000
118	EDGEWOOD & CARSWOLD	0	2	0	12.5	100.0	112.5	Low	High	High	\$4,000	\$0	\$0	\$4,000
80	MARYLAND & FORSYTH	4	7	0	64	46.6	110.6	High	Med	High	\$11,250	\$1,100	\$0	\$12,350
142	FOREST & WYDOWN	0	6	0	37	73.3	110.3	Med	Med	High	\$8,900	\$0	\$0	\$8,900
136	BRENTWOOD, CORPORATE PK & ORLANDO	2	7	0	68.5	41.6	110.1	High	Med	High	\$14,000	\$4,750	\$0	\$18,750
216	SAN BONITA & DE MUN	0	2	0	52.5	57.5	110.0	High	Med	High	\$4,000	\$0	\$0	\$4,000
102	ALAMO & PRIVATE	0	3	0	49.5	60.0	109.5	Med	Med	High	\$6,000	\$0	\$0	\$6,000
190	WESTWOOD & OXFORD	0	4	0	37	72.5	109.5	Med	Med	High	\$8,000	\$0	\$0	\$8,000
63	SUDBURY & ROXBURGH	0	2	0	31.5	77.5	109.0	Med	Med	High	\$4,000	\$0	\$0	\$4,000
301	SHAW PARK & MERAMEC	0	4	0	76	33.0	109.0	High	Low	High	\$8,000	\$0	\$0	\$8,000



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1	2	3	4	5	6	7	8	9	10	11	12	13	14	15
9	BRENTWOOD & WALINCA	3	7	0	57	51.5	108.5	High	Med	High	\$14,000	\$0	\$0	\$14,000
98	CONCORDIA & PRIVATE	0	3	0	41.5	66.7	108.2	Med	Med	High	\$6,000	\$0	\$0	\$6,000
43	ELLENWOOD & UNIVERSITY	0	4	0	34	73.8	107.8	Med	Med	High	\$8,000	\$0	\$0	\$8,000
246	SAN BONITA & DE MUN	0	5	0	52.5	55.0	107.5	High	Med	High	\$10,000	\$0	\$0	\$10,000
199	WESTWOOD & YORK	0	4	0	36	71.3	107.3	Med	Med	High	\$8,000	\$0	\$0	\$8,000
61	GLEN RIDGE & YORK	0	8	0	36	69.6	105.6	Med	Med	High	\$16,000	\$0	\$0	\$16,000
300	CORPORATE PK & SHAW PARK	0	2	0	42	62.5	104.5	Med	Med	High	\$4,000	\$0	\$0	\$4,000
3	CROMWELL & GLEN RIDGE	0	8	0	36	68.1	104.1	Med	Med	High	\$16,000	\$0	\$0	\$16,000
171	BYRON & GLEN RIDGE	0	8	0	37	66.9	103.9	Med	Med	High	\$16,000	\$0	\$0	\$16,000
40	BONHOMME & MERAMEC	0	6	0	66	36.7	102.7	High	Low	Med	\$12,000	\$12,000	\$0	\$24,000
87	SAN BONITA & ST RITA	0	2	0	41.5	61.0	102.5	Med	Med	Med	\$4,000	\$0	\$0	\$4,000
311	RITZ CARLTON & COLORADO	0	3	1	30	72.5	102.5	Med	Med	Med	\$6,000	\$0	\$1,200	\$7,200
109	CENTRAL & FORSYTH	0	8	0	76	26.5	102.5	High	Low	Med	\$16,000	\$14,000	\$0	\$30,000
137	POLO & POLO	0	1	0	31.5	70.0	101.5	Med	Med	Med	\$2,000	\$0	\$0	\$2,000
244	CARONDELET & CENTRAL	0	6	0	73	27.5	100.5	High	Low	Med	\$12,000	\$0	\$0	\$12,000
181	GLEN RIDGE & BUCKINGHAM	0	8	0	30	69.4	99.4	Med	Med	Med	\$16,000	\$0	\$0	\$16,000
74	TOPTON & MARK TWAIN	0	2	0	52.5	45.0	97.5	High	Med	Med	\$4,000	\$0	\$0	\$4,000
113	BRENTWOOD & CARONDELET	3	4	0	69	28.5	97.5	High	Low	Med	\$8,000	\$400	\$0	\$8,400
222	CLAYTON & CLAYTONIA	0	1	0	41	55.0	96.0	Med	Med	Med	\$2,000	\$0	\$0	\$2,000
130	HANLEY & WALINCA	0	2	0	32.5	63.5	96.0	Med	Med	Med	\$4,000	\$0	\$0	\$4,000
133	UNIVERSITY & CECIL	0	4	0	31	63.8	94.8	Med	Med	Med	\$8,000	\$0	\$0	\$8,000
39	BONHOMME & CENTRAL	0	6	0	73	21.7	94.7	High	Low	Med	\$6,000	\$14,000	\$0	\$20,000
147	HANLEY & POLO	0	1	0	29.5	65.0	94.5	Med	Med	Med	\$2,000	\$0	\$0	\$2,000
47	BRENTWOOD & PERSHING	0	2	0	49.5	45.0	94.5	Med	Med	Med	\$4,000	\$0	\$0	\$4,000
125	POLO & N POLO	0	2	0	43.5	51.0	94.5	Med	Med	Med	\$4,000	\$0	\$0	\$4,000
183	WESTWOOD & BUCKINGHAM	0	5	0	40	54.0	94.0	Med	Med	Med	\$10,000	\$0	\$0	\$10,000
108	MERAMEC & FORSYTH	0	8	0	69	25.0	94.0	High	Low	Med	\$13,050	\$8,000	\$0	\$21,050
107	MARYLAND & LEE	0	3	0	43.5	50.0	93.5	Med	Med	Med	\$6,000	\$0	\$0	\$6,000
198	AUDUBON & YORK	0	3	0	12.5	80.0	92.5	Low	Med	Med	\$6,000	\$0	\$0	\$6,000
115	CARONDELET & RITZ CARLTON	0	3	1	36	56.5	92.5	Med	Med	Med	\$6,000	\$0	\$4,200	\$10,200
210	AUDUBON & SOMERSET	0	4	0	15.5	76.8	92.3	Low	Med	Med	\$8,000	\$0	\$0	\$8,000
67	GAY & LADUE & MARYLAND	4	6	0	32	60.3	92.3	Med	Med	Med	\$12,000	\$0	\$0	\$12,000
207	CRESTWOOD & HILLVALE	0	8	0	19.5	72.8	92.3	Low	Med	Med	\$16,000	\$0	\$0	\$16,000
259	BEMISTON & SHAW PARK	0	2	0	56	36.0	92.0	High	Low	Med	\$4,000	\$0	\$0	\$4,000
247	DE MUN & SOUTHWOOD	0	3	0	35.5	55.0	90.5	Med	Med	Med	\$6,000	\$0	\$0	\$6,000
213	CRESTWOOD & CLAVERACH	0	3	0	19.5	70.7	90.2	Low	Med	Med	\$6,000	\$0	\$0	\$6,000
62	FRANCIS & ROXBURGH	0	2	0	34.5	55.0	89.5	Med	Med	Med	\$4,000	\$0	\$0	\$4,000
257	BIG BEND & SOUTHMOOR	0	4	0	19.5	70.0	89.5	Low	Med	Med	\$8,000	\$0	\$0	\$8,000
89	TOPTON & MARK TWAIN	0	2	0	46.5	42.5	89.0	Med	Med	Med	\$4,000	\$0	\$0	\$4,000
73	MARYLAND & LANCASTER	0	2	0	36	52.5	88.5	Med	Med	Med	\$4,000	\$0	\$0	\$4,000
211	BIG BEND & TUSCANY	0	2	0	32.5	56.0	88.5	Med	Med	Med	\$4,000	\$0	\$0	\$4,000
243	BEMISTON & CARONDELET	0	5	0	59	29.0	88.0	High	Low	Med	\$10,000	\$0	\$0	\$10,000
15	BRENTWOOD & VENETIAN	0	2	0	62.5	25.0	87.5	High	Low	Med	\$4,000	\$0	\$0	\$4,000
304	HANLEY & CLAYTON	0	2	0	34	53.5	87.5	Med	Med	Med	\$4,000	\$0	\$0	\$4,000
99	CONCORDIA & ALAMO	0	2	0	44.5	42.5	87.0	Med	Med	Med	\$4,000	\$0	\$0	\$4,000
201	BIG BEND & OAK KNOLL	0	2	0	29.5	57.5	87.0	Med	Med	Med	\$4,000	\$0	\$0	\$4,000



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1	2	3	4	5	6	7	8	9	10	11	12	13	14	15
214	SAN BONITA & SEMINARY	0	7	0	43.5	42.1	85.6	Med	Med	Med	\$14,000	\$0	\$0	\$14,000
274	WALLACE & FORSYTH	4	8	0	35.5	50.0	85.5	Med	Med	Med	\$3,900	\$2,600	\$0	\$6,500
72	BRENTWOOD & WESTMORELAND	0	3	1	51	34.2	85.2	High	Low	Med	\$4,400	\$0	\$3,250	\$7,650
127	HANLEY & REAR	0	2	0	32.5	52.5	85.0	Med	Med	Med	\$4,000	\$0	\$0	\$4,000
150	POLO & POLO	0	1	0	29.5	55.0	84.5	Med	Med	Med	\$2,000	\$0	\$0	\$2,000
90	POLO & MIDDLE POLO	0	4	0	29.5	55.0	84.5	Med	Med	Med	\$8,000	\$0	\$0	\$8,000
122	CARSWOLD & SHIRLEY	0	4	0	22.5	61.8	84.3	Low	Med	Med	\$8,000	\$0	\$0	\$8,000
197	WESTWOOD & WELLINGTON	0	5	0	36	47.4	83.4	Med	Med	Med	\$8,650	\$0	\$0	\$8,650
111	PARKSIDE & BRIGHTON	0	3	0	46.5	36.7	83.2	Med	Low	Med	\$6,000	\$0	\$0	\$6,000
182	AUDUBON & BUCKINGHAM	0	4	0	12.5	70.0	82.5	Low	Med	Med	\$8,000	\$0	\$0	\$8,000
91	POLO & MIDDLE POLO	0	4	0	28.5	53.5	82.0	Med	Med	Med	\$8,000	\$0	\$0	\$8,000
296	BIG BEND & NORTHMOOR	0	4	0	25.5	56.3	81.8	Low	Med	Med	\$8,000	\$0	\$0	\$8,000
172	AUDUBON & PARKDALE	0	4	0	12.5	68.8	81.3	Low	Med	Med	\$8,000	\$0	\$0	\$8,000
49	CENTRAL & PERSHING	0	4	0	51	30.0	81.0	High	Low	Med	\$6,650	\$0	\$0	\$6,650
309	JACKSON & MARYLAND	0	4	0	28.5	52.5	81.0	Med	Med	Med	\$8,000	\$0	\$0	\$8,000
33	KINGSBURY & CENTRAL	0	5	0	35	45.0	80.0	Med	Med	Med	\$8,650	\$0	\$0	\$8,650
170	AUDUBON & BYRON	0	4	0	9.5	70.0	79.5	Low	Med	Med	\$8,000	\$0	\$0	\$8,000
238	FORSYTH, JACKSON & CARONDELET	4	8	2	25	54.5	79.5	Low	Med	Med	\$16,000	\$3,500	\$6,300	\$25,800
51	PERSHING & BEMISTON	0	8	0	36	43.4	79.4	Med	Med	Med	\$16,000	\$0	\$0	\$16,000
69	MARYLAND & CRANDON	0	2	0	36	42.5	78.5	Med	Med	Med	\$4,000	\$0	\$0	\$4,000
110	FORSYTH & BEMISTON	0	8	0	69	9.4	78.4	High	Low	Med	\$13,050	\$8,000	\$0	\$21,050
248	DE MUN & SAN BONITA	0	2	0	40.5	37.5	78.0	Med	Low	Med	\$4,000	\$0	\$0	\$4,000
8	WALINCA & MERAMEC	0	2	0	49.5	27.5	77.0	Med	Low	Med	\$4,000	\$0	\$0	\$4,000
32	KINGSBURY & BEMISTON	0	6	0	39	37.5	76.5	Med	Low	Med	\$12,000	\$0	\$0	\$12,000
174	PARKDALE & GLEN RIDGE	0	8	0	40	36.5	76.5	Med	Low	Med	\$16,000	\$0	\$0	\$16,000
153	BRENTWOOD & WATKINS	0	2	0	52.5	23.5	76.0	High	Low	Med	\$4,000	\$0	\$0	\$4,000
217	BIG BEND & TUSCANY	0	2	0	38.5	37.5	76.0	Med	Low	Med	\$4,000	\$0	\$0	\$4,000
165	WYDOWN & EDGEWOOD	0	4	0	62	13.8	75.8	High	Low	Med	\$3,550	\$0	\$0	\$3,550
186	ARUNDEL & UNIVERSITY	0	5	0	19.5	56.0	75.5	Low	Med	Med	\$10,000	\$0	\$0	\$10,000
234	DE MUN & ROSEBURY	0	3	0	31.5	43.3	74.8	Med	Med	Med	\$6,000	\$0	\$0	\$6,000
276	DE MUN & NORTHWOOD	0	2	0	28.5	46.0	74.5	Med	Med	Med	\$4,000	\$0	\$0	\$4,000
124	SHIRLEY & POLO & REAR	0	1	0	43.5	30.0	73.5	Med	Low	Med	\$2,000	\$0	\$0	\$2,000
2	AUDUBON & CROMWELL	0	5	0	15.5	58.0	73.5	Low	Med	Med	\$10,000	\$0	\$0	\$10,000
184	DARTFORD & ARUNDEL	0	4	0	19.5	53.8	73.3	Low	Med	Med	\$8,000	\$0	\$0	\$8,000
6	BRENTWOOD & KINGSBURY	0	2	0	40.5	32.5	73.0	Med	Low	Med	\$4,000	\$0	\$0	\$4,000
70	MERAMEC & WESTMORELAND	0	2	0	48	25.0	73.0	Med	Low	Med	\$4,000	\$0	\$0	\$4,000
78	FORSYTH & PARKSIDE	0	2	0	63	10.0	73.0	High	Low	Med	\$4,000	\$0	\$0	\$4,000
168	UNIVERSITY & WYDOWN	0	12	0	39.5	33.5	73.0	Med	Low	Med	\$16,750	\$0	\$0	\$16,750
81	BRENTWOOD & DAVIS	0	2	0	44.5	27.5	72.0	Med	Low	Med	\$4,000	\$0	\$0	\$4,000
169	WYDOWN & WYDOWN	0	2	0	36.5	35.0	71.5	Med	Low	Med	\$4,000	\$0	\$0	\$4,000
258	BIG BEND & FOREST RIDGE	0	4	0	24	47.5	71.5	Low	Med	Med	\$8,000	\$0	\$0	\$8,000
177	UNIVERSITY & WYDOWN	0	4	0	29.5	41.3	70.8	Med	Low	Med	\$8,000	\$0	\$0	\$8,000
307	BLAND & NORTHMOOR	0	1	0	15.5	55.0	70.5	Low	Med	Med	\$2,000	\$0	\$0	\$2,000
195	GLEN RIDGE & WELLINGTON	0	8	0	33	37.3	70.3	Med	Low	Med	\$16,000	\$0	\$0	\$16,000
68	FORSYTH & WESTMORELAND	0	4	1	37.5	32.5	70.0	Med	Low	Med	\$2,700	\$0	\$2,250	\$4,950
158	POLO & POLO	0	2	0	29.5	40.0	69.5	Med	Low	Med	\$4,000	\$0	\$0	\$4,000



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1	2	3	4	5	6	7	8	9	10	11	12	13	14	15
265	DE MUN & N/A	0	2	0	29.5	40.0	69.5	Med	Low	Med	\$4,000	\$0	\$0	\$4,000
250	DE MUN & ARUNDEL	0	6	0	26.5	42.8	69.3	Low	Med	Med	\$12,000	\$0	\$0	\$12,000
226	BRENTWOOD & WHITBURN	0	2	0	24	45.0	69.0	Low	Med	Med	\$4,000	\$0	\$0	\$4,000
138	EDGEWOOD & WALINCA	0	2	0	45.5	22.5	68.0	Med	Low	Med	\$4,000	\$0	\$0	\$4,000
253	DE MUN & WYDOWN	0	2	0	25.5	42.5	68.0	Low	Med	Med	\$4,000	\$0	\$0	\$4,000
154	UNIVERSITY & ABERDEEN	0	8	0	21.5	46.3	67.8	Low	Med	Med	\$16,000	\$0	\$0	\$16,000
7	BRENTWOOD & STRATFORD	0	2	0	24.5	42.5	67.0	Low	Med	Med	\$4,000	\$0	\$0	\$4,000
291	FORSYTH & LYLE	0	2	0	42	25.0	67.0	Med	Low	Med	\$4,000	\$0	\$0	\$4,000
192	FRANCIS & WHITBURN	0	7	0	24	42.9	66.9	Low	Med	Med	\$12,000	\$0	\$0	\$12,000
41	BONHOMME & BEMISTON	0	4	0	53	13.8	66.8	High	Low	Med	\$8,000	\$10,000	\$0	\$18,000
129	MERAMEC & ROSILINE	0	2	0	46.5	20.0	66.5	Med	Low	Med	\$500	\$0	\$0	\$500
52	FORSYTH & UNIVERSITY	0	5	1	30.5	35.5	66.0	Med	Low	Med	\$8,000	\$0	\$5,250	\$13,250
86	CLAYTON & SOMERSET	0	1	0	25.5	40.0	65.5	Low	Low	Med	\$2,000	\$0	\$0	\$2,000
188	AUDUBON & OXFORD	0	5	0	12.5	53.0	65.5	Low	Med	Med	\$10,000	\$0	\$0	\$10,000
157	WYDOWN & ASBURY	0	4	0	20	43.5	63.5	Low	Med	Med	\$8,000	\$0	\$0	\$8,000
148	DARTFORD & ABERDEEN	0	4	0	21.5	41.8	63.3	Low	Med	Med	\$8,000	\$0	\$0	\$8,000
225	BRENTWOOD & FRANCIS	0	6	0	24	39.2	63.2	Low	Low	Med	\$10,650	\$0	\$0	\$10,650
27	BRENTWOOD & CLAYTON LANE	0	2	0	12.5	50.0	62.5	Low	Med	Med	\$4,000	\$0	\$0	\$4,000
233	DE MUN & ROSEBURY	0	2	0	31.5	31.0	62.5	Med	Low	Med	\$4,000	\$0	\$0	\$4,000
232	SHAW PARK & SHAW PARK	0	1	0	42	20.0	62.0	Med	Low	Med	\$2,000	\$0	\$0	\$2,000
131	BIG BEND & SHEPLEY	0	2	0	14	47.5	61.5	Low	Med	Med	\$4,000	\$0	\$0	\$4,000
251	DE MUN & ABERDEEN	0	8	0	19.5	41.9	61.4	Low	Med	Med	\$14,600	\$0	\$0	\$14,600
200	BIG BEND & SOUTHMOOR	0	2	0	18.5	42.5	61.0	Low	Med	Med	\$4,000	\$0	\$0	\$4,000
263	RITZ CARLTON & CARONDELET	0	2	0	36	25.0	61.0	Med	Low	Med	\$4,000	\$0	\$0	\$4,000
196	AUDUBON & WELLINGTON	0	5	0	12.5	48.0	60.5	Low	Med	Med	\$10,000	\$0	\$0	\$10,000
278	WYDOWN & WYDOWN TERRACE	0	4	0	28.5	31.3	59.8	Med	Low	Med	\$8,000	\$0	\$0	\$8,000
35	KINGSBURY & MERAMEC	0	3	0	39.5	20.0	59.5	Med	Low	Med	\$4,400	\$0	\$0	\$4,400
189	OXFORD & GLEN RIDGE	0	8	0	29	30.3	59.3	Med	Low	Med	\$16,000	\$0	\$0	\$16,000
292	FORSYTH & LYLE	2	2	0	42	15.5	57.5	Med	Low	Low	\$2,650	\$350	\$0	\$3,000
164	WYDOWN & GLEN RIDGE	0	2	0	47	10.0	57.0	Med	Low	Low	\$500	\$0	\$0	\$500
132	BIG BEND & BRENTMOOR	0	2	0	17	40.0	57.0	Low	Low	Low	\$4,000	\$0	\$0	\$4,000
277	DE MUN & ARUNDEL	0	3	0	28.5	28.3	56.8	Med	Low	Low	\$4,650	\$0	\$0	\$4,650
112	FORSYTH & LEE	0	2	0	36	20.0	56.0	Med	Low	Low	\$700	\$2,000	\$0	\$2,700
13	MERAMEC & ORLANDO	0	3	0	37.5	16.7	54.2	Med	Low	Low	\$1,950	\$0	\$0	\$1,950
149	WYDOWN & DARTFORD	0	7	0	31.5	20.7	52.2	Med	Low	Low	\$4,700	\$0	\$0	\$4,700
297	BIG BEND & PRIVATE	0	2	0	14	37.5	51.5	Low	Low	Low	\$4,000	\$0	\$0	\$4,000
4	KINGSBURY & BRENTWOOD	0	3	0	30	18.3	48.3	Med	Low	Low	\$2,650	\$0	\$0	\$2,650
203	CRESCENT & OLETA	0	4	0	18.5	28.8	47.3	Low	Low	Low	\$6,250	\$0	\$0	\$6,250
50	MERAMEC & PERSHING	0	2	0	42	5.0	47.0	Med	Low	Low	\$2,400	\$0	\$0	\$2,400
205	BILTMORE & BILTMORE	0	4	0	25.5	21.0	46.5	Low	Low	Low	\$3,150	\$0	\$0	\$3,150
119	EDGEWOOD & SHIRLEY	0	2	0	16.5	30.0	46.5	Low	Low	Low	\$4,000	\$0	\$0	\$4,000
23	BEMISTON & BILTMORE	0	2	0	21.5	22.5	44.0	Low	Low	Low	\$2,250	\$0	\$0	\$2,250
20	DAVIS & CENTRAL	0	2	0	21.5	22.5	44.0	Low	Low	Low	\$2,450	\$0	\$0	\$2,450
14	MERAMEC & VENETIAN	0	2	0	31.5	10.0	41.5	Med	Low	Low	\$2,200	\$0	\$0	\$2,200
204	OLETA & SEMINOLE	0	2	0	15.5	25.0	40.5	Low	Low	Low	\$2,250	\$0	\$0	\$2,250
141	MERAMEC & DAVIS	0	4	0	21.5	18.8	40.3	Low	Low	Low	\$3,550	\$0	\$0	\$3,550



City of Clayton ADA Transition Plan
Intersection Data Summary



1	2	3	4	5	6	7	8	9	10	11	12	13	14	15
194	BILTMORE & MOHAWK	0	2	0	22.5	15.0	37.5	Low	Low	Low	\$700	\$0	\$0	\$700
36	FORSYTH, BRIGHTON & PERSHING	0	6	1	16	19.2	35.2	Low	Low	Low	\$9,100	\$0	\$750	\$9,850
161	WYDOWN & BRENTMOOR	0	5	0	15.5	19.0	34.5	Low	Low	Low	\$5,750	\$0	\$0	\$5,750
5	FORSYTH, TOPTON & KINGSBURY	0	8	2	16	16.3	32.3	Low	Low	Low	\$12,700	\$0	\$1,000	\$13,700
152	MERAMEC & DAYTONA	0	2	0	21.5	10.0	31.5	Low	Low	Low	\$2,400	\$0	\$0	\$2,400
19	DAVIS & CENTRAL	0	4	0	13.5	15.0	28.5	Low	Low	Low	\$1,800	\$0	\$0	\$1,800
31	GAY & COLONIAL	0	2	0	8.5	20.0	28.5	Low	Low	Low	\$2,650	\$0	\$0	\$2,650
18	FORSYTH & OLYMPIAN	0	2	0	19.5	5.0	24.5	Low	Low	Low	\$1,450	\$0	\$0	\$1,450
159	WYDOWN & BRENTMOOR	0	2	0	21.5	0.0	21.5	Low	Low	Low	\$500	\$0	\$0	\$500
162	WYDOWN & AUDUBON	0	2	0	15.5	5.0	20.5	Low	Low	Low	\$900	\$0	\$0	\$900
160	WYDOWN & CRESTWOOD	0	2	0	15.5	5.0	20.5	Low	Low	Low	\$2,200	\$0	\$0	\$2,200
266	FORSYTH & EAST	0	3	0	5.5	8.3	13.8	Low	Low	Low	\$2,900	\$0	\$0	\$2,900
17	FORSYTH & STRATFORD	0	1	0	5.5	5.0	10.5	Low	Low	Low	\$2,000	\$0	\$0	\$2,000



City of Clayton ADA Transition Plan
Sidewalk Cost Projection Summary



Casename	From Street	To Street	Priority	Costs
MARYLAND_002873-COLE_S_SB	Meramec	Central	High	\$29,150.00
BRENTWOOD_002693-COLE_E_SB	Mid Block	Forsyth	High	\$21,420.00
CENTRAL_002891-COLE_W_SB	Maryland	Forsyth	High	\$28,340.00
MERAMEC_002877-COLE_E_SB	Maryland	Forsyth	High	\$23,830.00
BEMISTON_003503-COLE_W_SB	Bonhomme	Shaw Park	High	\$25,500.00
MERAMEC_003454-COLE_E_SB	Bonhomme	Shaw Park	High	\$49,600.00
BONHOMME_003540-COLE_N_SB	Bemiston	Hanley	High	\$55,200.00
FOREST_002406-COLE_W_SB	Wydown	Mid Block	High	\$24,290.00
MARYLAND_002872-COLE_S_SB	Central	Bemiston	High	\$28,220.00
CARONDELET_000864-COLE_S_SB	Bemiston	Hanley	High	\$54,700.00
MARYLAND_002871-COLE_S_SB	Bemiston	Hanley	High	\$37,260.00
BEMISTON_003677-COLE_W_SB	Maryland	Forsyth	High	\$37,450.00
MERAMEC_003778-COLE_E_SB	Forsyth	Carondelet	High	\$17,660.00
CARONDELET_003622-COLE_N_SB	Bemiston	Hanley	High	\$47,440.00
FORSYTH_003670-COLE_N_SB	Bemiston	Hanley	High	\$34,310.00
TOPTON2_016373_COLE_E_CW	Maryland	Mid Block	High	\$47,350.00
BONHOMME_003490-COLE_S_SB	Central	Bemiston	High	\$11,070.00
BRENTWOOD_017511_COLE_E_CW	Mid Block	Shaw Park	High	\$7,220.00
BRENTWOOD_017512_COLE_E_CW	Bonhomme	Mid Block	High	\$18,400.00
BONHOMME_003425-COLE_S_SB	Brentwood	Meramec	High	\$24,420.00
DE-MUN_019703_COLE_E_CW	Northwood	Rosebury	High	\$12,160.00
WESTMORELAND_016478_COLE_S_CW	Brentwood	Meramec	High	\$28,400.00
MARYLAND_002766-COLE_N_SB	Brentwood	Meramec	High	\$40,930.00
MARYLAND_002844-COLE_N_SB	Bemiston	Linden	High	\$23,180.00
DE-MUN_000988-COLE_W_SB	Alamo	Clayton	High	\$16,710.00
WYDOWN_002401-COLE_S_SB	Hanley	Forest	High	\$17,680.00
WYDOWN_002376-COLE_N_SB	Hanley	Mid Block	High	\$9,640.00
DE-MUN_001172-COLE_W_SB	Arundel	Mid Block	High	\$32,330.00
CARONDELET_003076-COLE_S_SB	Hanley	Lyle	High	\$7,780.00
SHAW-PARK_003410-COLE_N_SB	Brentwood	Meramec	High	\$18,980.00
DE-MUN_001171-COLE_W_SB	Mid Block	San Bonita	High	\$19,100.00
FORSYTH_002961-COLE_S_SB	Lyle	Carondelet	High	\$24,300.00
MARYLAND_002763-COLE_S_SB	Mid Block	Meramec	High	\$6,200.00
SHAW-PARK_003406-COLE_N_SB	Central	Bemiston	High	\$24,710.00
FORSYTH_003756-COLE_S_SB	Brentwood	Meramec	High	\$31,420.00
MARYLAND2_016365_COLE_S_CW	Gay	Topton	High	\$32,390.00
BRENTWOOD_002692-COLE_E_SB	Maryland	Mid Block	High	\$14,780.00
MARYLAND_004279-COLE_S_SB	Brentwood	Meramec	High	\$15,810.00
BONHOMME_003805-COLE_N_SB	Brentwood	Meramec	High	\$38,860.00
CENTRAL_003821-COLE_W_SB	Mid Block	Shaw Park	High	\$13,780.00
BRENTWOOD_002690-COLE_W_SB	Maryland	Mid Block	High	\$8,710.00
CARONDELET_003743-COLE_N_SB	Brentwood	Meramec	High	\$24,110.00
CARONDELET_003811-COLE_S_SB	Brentwood	Meramec	High	\$18,680.00
CENTRAL_003819-COLE_E_SB	Bonhomme	Shaw Park	High	\$31,530.00
SHAW-PARK_003408-COLE_N_SB	Meramec	Central	High	\$9,230.00
BRENTWOOD_017520_COLE_E_CW	Forsyth	Carondelet	High	\$23,930.00
FORSYTH_002715-COLE_N_SB	Brentwood	Meramec	High	\$23,440.00
MERAMEC_002760-COLE_W_SB	Maryland	Forsyth	High	\$35,170.00
BRENTWOOD_017516_COLE_E_CW	Carondelet	Bonhome	High	\$15,500.00
BRENTWOOD_002689-COLE_W_SB	Mid Block	Forsyth	High	\$17,950.00
MERAMEC_003452-COLE_W_SB	Bonhomme	Shaw Park	High	\$22,800.00
CENTRAL_003692-COLE_E_SB	Maryland	Forsyth	High	\$10,740.00
CENTRAL_003822-COLE_W_SB	Bonhomme	Mid Block	High	\$13,330.00
CENTRAL_017189_COLE_W_CW	Mid Block	Clayton	High	\$39,930.00
BRENTWOOD_017507_COLE_E_CW	Shaw Park	Walinca	High	\$1,450.00



City of Clayton ADA Transition Plan
Sidewalk Cost Projection Summary



Casename	From Street	To Street	Priority	Costs
CENTRAL_017195_COLE_E_CW	Mid Block	Clayton	High	\$31,410.00
DE-MUN_000914-COLE_E_SB	Alamo	Clayton	High	\$23,210.00
SHAW-PARK_003523-COLE_N_SB	Bemiston	Mid Block	High	\$32,450.00
MARYLAND_002647-COLE_S_SB	Forsyth	Brentwood	High	\$25,970.00
MERAMEC_003749-COLE_W_SB	Forsyth	Carondelet	High	\$22,660.00
FORSYTH_002680-COLE_E_SB	Ladue	Parkside	High	\$30,240.00
BEMISTON_003730-COLE_W_SB	Forsyth	Carondelet	High	\$18,200.00
WESTWOOD_002336-COLE_E_SB	Byron	Parkdale	High	\$8,100.00
BEMISTON_003635-COLE_E_SB	Forsyth	Carondelet	High	\$29,810.00
BYRON_002352-COLE_N_SB	Westwood	Mid Block	High	\$7,240.00
MERAMEC_003799-COLE_W_SB	Carondelet	Bonhome	High	\$15,250.00
WESTWOOD_000820-COLE_W_SB	Wydown	Buckingham	High	\$40,110.00
BEMISTON_003508-COLE_E_SB	Bonhomme	Shaw Park	High	\$21,770.00
CENTRAL_003556-COLE_E_SB	Carondelet	Bonhome	High	\$18,880.00
CENTRAL_003765-COLE_E_SB	Forsyth	Carondelet	High	\$18,870.00
FORSYTH_002682-COLE_N_SB	Parkside	Brentwood	High	\$20,280.00
BEMISTON_003606-COLE_W_SB	Carondelet	Bonhome	High	\$19,640.00
BYRON_019769_COLE_S_CW	Westwood	Glen Ridge	High	\$29,890.00
FORSYTH_002885-COLE_N_SB	Meramec	Central	High	\$15,610.00
WYDOWN_000828-COLE_S_SB	Westwood	Glen Ridge	High	\$29,330.00
CARONDELET_003561-COLE_S_SB	Central	Bemiston	High	\$10,610.00
CARONDELET_003734-COLE_N_SB	Central	Bemiston	High	\$10,820.00
CENTRAL_003772-COLE_W_SB	Forsyth	Carondelet	High	\$15,510.00
BEMISTON_003675-COLE_E_SB	Maryland	Forsyth	High	\$12,460.00
FORSYTH_003725-COLE_S_SB	Central	Bemiston	High	\$11,650.00
WESTWOOD_002359-COLE_E_SB	Wydown	Byron	High	\$6,860.00
BEMISTON_003609-COLE_E_SB	Carondelet	Bonhome	High	\$7,000.00
FORSYTH_003666-COLE_S_SB	Bemiston	Hanley	High	\$13,810.00
FORSYTH_003684-COLE_N_SB	Central	Bemiston	High	\$14,170.00
FORSYTH_003757-COLE_S_SB	Meramec	Central	High	\$1,690.00
BONHOMME_003460-COLE_S_SB	Meramec	Central	High	\$3,650.00
BONHOMME_003542-COLE_N_SB	Central	Bemiston	High	\$240.00
BRIGHTON_016394_COLE_E_CW	Mid Block	Parkside	High	\$30,100.00
LYLE_002901-COLE_W_SB	Mid Block	Forsyth	High	\$9,820.00
BRENTWOOD2_016485_COLE_E_CW	Pershing	Westmoreland	High	\$17,050.00
BRIGHTON_016392_COLE_W_CW	Mid Block	Mid Block	High	\$21,370.00
FRANCIS_017105_COLE_W_CW	Whitburn	Clayton	High	\$20,400.00
ROSILINE_016969_COLE_S_CW	Brentwood	Meramec	High	\$22,830.00
HANLEY_003517-COLE_W_SB	Bonhomme	Shaw Park	High	\$2,270.00
SHAW-PARK_003522-COLE_N_SB	Mid Block	Hanley	High	\$21,950.00
BONHOMME_003511-COLE_S_SB	Bemiston	Hanley	High	\$40,500.00
PERSHING_016492_COLE_N_CW	Brentwood	Meramec	High	\$16,240.00
FOREST_002408-COLE_N_SB	Mid Block	Mid Block	High	\$1,000.00
PERSHING_016490_COLE_S_CW	Brentwood	Meramec	High	\$16,860.00
BRIGHTON_016391_COLE_W_CW	Mid Block	Parkside	High	\$6,880.00
LEE_016892_COLE_E_CW	Maryland	Forsyth	High	\$20,400.00
BRENTWOOD_017001_COLE_E_CW	Orlando	Venetian	High	\$6,080.00
BRENTWOOD_017033_COLE_E_CW	Venetian	Davis	High	\$9,590.00
LEE_016885_COLE_W_CW	Maryland	Forsyth	High	\$19,730.00
MARYLAND_002780-COLE_N_SB	Meramec	Central	High	\$16,780.00
LYLE_003014-COLE_E_SB	Mid Block	Mid Block	High	\$1,790.00
BRENTWOOD_016976_COLE_E_CW	Roseline	Orlando	High	\$7,170.00
TOPTON_016379_COLE_E_CW	Topton	Brighton	High	\$6,880.00
TOPTON_016378_COLE_E_CW	Mid Block	Mark Twain	High	\$7,360.00
PARKDALE_003394-COLE_S_SB	Westwood	Mid Block	Med	\$18,930.00



City of Clayton ADA Transition Plan
Sidewalk Cost Projection Summary



Casename	From Street	To Street	Priority	Costs
WESTWOOD_002419-COLE_E_SB	Parkdale	Buckingham	Med	\$11,470.00
CENTRAL_004156-COLE_E_SB	Pershing	Mid Block	Med	\$27,540.00
PARKDALE_002329-COLE_N_SB	Westwood	Mid Block	Med	\$19,610.00
CENTRAL_004154-COLE_W_SB	Pershing	Mid Block	Med	\$26,560.00
BYRON_002351-COLE_N_SB	Mid Block	Glen Ridge	Med	\$6,470.00
MERAMEC_016626_COLE_E_CW	Mid Block	Kingsbury	Med	\$33,150.00
KINGSBURY_016565_COLE_N_CW	Brentwood	Meramec	Med	\$6,360.00
KINGSBURY_016620_COLE_N_CW	Meramec	Central	Med	\$8,250.00
DAVIS_017054_COLE_S_CW	Brentwood	Meramec	Med	\$39,030.00
DAYTONA_017067_COLE_S_CW	Brentwood	Meramec	Med	\$45,900.00
POLO_017358_COLE_N_CW	Rear	Hanley	Med	\$11,130.00
BRENTWOOD_016559_COLE_W_CW	Kingsbury	Pershing	Med	\$11,130.00
POLO_017356_COLE_E_CW	Mid Block	Hanley	Med	\$13,050.00
SHIRLEY_017388_COLE_N_CW	Hanley	Carswold	Med	\$7,210.00
WALINCA_016995_COLE_S_CW	Brentwood	Meramec	Med	\$19,980.00
WALINCA_017415_COLE_N_CW	Hanley	Carswold	Med	\$13,140.00
ROSEBURY_001206-COLE_N_SB	De Mun	Mid Block	Med	\$19,090.00
WALINCA_016915_COLE_N_CW	Brentwood	Meramec	Med	\$17,820.00
BRENTWOOD_017058_COLE_E_CW	Davis	Daytona	Med	\$9,760.00
MERAMEC_016582_COLE_W_CW	Mid Block	Kingsbury	Med	\$19,900.00
DE-MUN_001109-COLE_W_SB	San Bonita	Alamo	Med	\$14,790.00
CENTRAL_004153-COLE_W_SB	Mid Block	Maryland	Med	\$42,420.00
BEMISTON3_016846_COLE_E_CW	Pershing	Maryland	Med	\$31,050.00
CENTRAL_016727_COLE_W_CW	Kingsbury	Pershing	Med	\$12,220.00
WELLINGTON_002215-COLE_N_SB	Westwood	Glen Ridge	Med	\$43,740.00
WESTWOOD_002434-COLE_W_SB	Buckingham	Wellington	Med	\$26,570.00
WESTWOOD_002221-COLE_E_SB	Wellington	York	Med	\$4,650.00
WESTWOOD_002435-COLE_W_SB	Wellington	Clayton	Med	\$14,050.00
BRENTWOOD_017072_COLE_E_CW	Daytona	Watkins	Med	\$6,490.00
PARKSIDE_016458_COLE_E_CW	Brighton	Brentwood	Med	\$21,890.00
SHAW-PARK_017551_COLE_N_CW	Mid Block	Brentwood	Med	\$2,220.00
BRENTWOOD_017557_COLE_W_CW	Shaw Park	Mid Block	Med	\$4,500.00
BRENTWOOD_017562_COLE_W_CW	Mid Block	Orlando	Med	\$8,790.00
WESTMORELAND_016863_COLE_N_CW	Hanley	Mid Block	Med	\$38,550.00
BRENTWOOD_017531_COLE_W_CW	Forsyth	Bonhome	Med	\$29,440.00
KINGSBURY_016673_COLE_N_CW	Bemiston	Mid Block	Med	\$10,790.00
ROSILINE_016984_COLE_N_CW	Brentwood	Meramec	Med	\$21,170.00
BRENTWOOD_016277_COLE_W_CW	Westmoreland	Maryland	Med	\$7,840.00
NORTHWOOD_001192-COLE_N_SB	Bland	Mid Block	Med	\$28,600.00
ORLANDO_016948_COLE_N_CW	Brentwood	Meramec	Med	\$29,880.00
ORLANDO_016955_COLE_N_CW	Brentwood	Meramec	Med	\$24,830.00
SHAW-PARK_017552_COLE_N_CW	Shaw Park	Mid Block	Med	\$20,470.00
BRENTWOOD_017532_COLE_W_CW	Bonhomme	Shaw Park	Med	\$11,550.00
BRENTWOOD2_016087_COLE_W_CW	Unversity	Westmoreland	Med	\$4,390.00
CONCORDIA_001063-COLE_E_SB	San Bonita	Alamo	Med	\$6,750.00
DE-MUN_001170-COLE_W_SB	San Boninta	San Bonita	Med	\$7,690.00
DE-MUN_001156-COLE_E_SB	Southwood	San Bonita	Med	\$1,060.00
ROSEBURY_001223-COLE_S_SB	De Mun	Mid Block	Med	\$38,480.00
SOUTHWOOD_001150-COLE_S_SB	De Mun	Mid Block	Med	\$30,530.00
BRENTWOOD_017000_COLE_E_CW	Mid Block	Rosiline	Med	\$4,680.00
CONCORDIA_001061-COLE_W_SB	San Bonita	Clayton	Med	\$38,600.00
DE-MUN_001215-COLE_E_SB	Rosebury	Rosebury	Med	\$2,290.00
SAN-BONITA_001775-COLE_S_SB	Mid Block	De Mun	Med	\$5,390.00
SEMINARY_001003-COLE_W_SB	San Bonita	Clayton	Med	\$14,600.00
SOUTHWOOD_001148-COLE_N_SB	De Mun	Mid Block	Med	\$35,500.00



City of Clayton ADA Transition Plan
Sidewalk Cost Projection Summary



Casename	From Street	To Street	Priority	Costs
ST-RITA_001021-COLE_E_SB	San Bonita	Alamo	Med	\$15,960.00
WESTMORELAND_016480-COLE_N_CW	Brentwood	Meramec	Med	\$15,270.00
ALAMO_000995-COLE_N_SB	Seminary	De Mun	Med	\$6,580.00
DE-MUN_001167-COLE_E_SB	San Bonita	San Bonita	Med	\$1,920.00
MERAMEC_016593-COLE_E_CW	Kingsbury	Pershing	Med	\$43,200.00
ROSEBURY_001210-COLE_S_SB	De Mun	Mid Block	Med	\$20,460.00
ROSEBURY_001221-COLE_N_SB	De Mun	Mid Block	Med	\$16,030.00
SEMINARY_001098-COLE_E_SB	San Bonita	Alamo	Med	\$7,990.00
ALAMO_001029-COLE_N_SB	Concordia	St Rita	Med	\$6,930.00
CENTRAL_016645-COLE_E_CW	Mid Block	Kingsbury	Med	\$23,970.00
PERSHING_016693-COLE_S_CW	Bemiston	Mid Block	Med	\$11,060.00
ST-RITA_001068-COLE_W_SB	Mid Block	Stratford	Med	\$2,080.00
BEMISTON_004163-COLE_W_SB	Pershing	Mid Block	Med	\$23,910.00
BRENTWOOD_016469-COLE_E_CW	Mid Block	Maryland	Med	\$8,800.00
BRENTWOOD_016470-COLE_E_CW	Westmoreland	Mid Block	Med	\$14,190.00
CENTRAL_016643-COLE_W_CW	Mid Block	Kingsbury	Med	\$26,360.00
DE-MUN_001130-COLE_E_SB	Rosebury	Southwood	Med	\$2,420.00
MERAMEC_016585-COLE_W_CW	Westmoreland	Maryland	Med	\$17,560.00
MERAMEC_016592-COLE_E_CW	Mid Block	Maryland	Med	\$25,070.00
BEMISTON_004162-COLE_W_SB	Mid Block	Maryland	Med	\$26,740.00
KINGSBURY_016649-COLE_N_CW	Central	Bemiston	Med	\$11,740.00
KINGSBURY_016665-COLE_S_CW	Central	Bemiston	Med	\$9,810.00
CENTRAL_016725-COLE_E_CW	Kingsbury	Pershing	Med	\$8,460.00
PERSHING_016702-COLE_S_CW	Central	Bemiston	Med	\$14,190.00
PERSHING_016717-COLE_N_CW	Central	Bemiston	Med	\$10,480.00
OXFORD_002209-COLE_S_SB	Westwood	Mid Block	Med	\$29,840.00
OXFORD_002263-COLE_N_SB	Westwood	Mid Block	Med	\$21,660.00
WHITBURN_002598-COLE_N_SB	Sudsbury	Francis	Med	\$28,970.00
FRANCIS_017594-COLE_E_CW	Mid Block	Whitburn	Med	\$25,350.00
FRANCIS_017596-COLE_W_CW	Mid Block	Roxburgh	Med	\$6,220.00
ROXBURGH_017603-COLE_N_CW	Sudsbury	Francis	Med	\$22,530.00
WESTWOOD_002310-COLE_E_SB	Buckingham	Oxford	Med	\$8,180.00
NORTHWOOD_001737-COLE_S_SB	De Mun	Mid Block	Med	\$19,620.00
GLEN-RIDGE_019683-COLE_W_CW	Byron	Parkdale	Med	\$5,970.00
OXFORD_003347-COLE_N_SB	Mid Block	Glen Ridge	Med	\$24,720.00
ROXBURGH_002586-COLE_S_SB	Sudsbury	Francis	Med	\$21,430.00
WELLINGTON_003363-COLE_S_SB	Westwood	Mid Block	Med	\$24,470.00
GLEN-RIDGE_002255-COLE_W_SB	York	Cromwell	Med	\$6,800.00
LINDEN_016855-COLE_E_CW	Mid Block	Maryland	Med	\$35,090.00
YORK_003332-COLE_S_SB	Westwood	Mid Block	Med	\$19,240.00
YORK_004056-COLE_N_SB	Mid Block	Glen Ridge	Med	\$28,860.00
YORK_004057-COLE_N_SB	Westwood	Mid Block	Med	\$16,140.00
BUCKINGHAM_002425-COLE_N_SB	Westwood	Mid Block	Med	\$5,880.00
GLEN-RIDGE_002346-COLE_W_SB	Wydown	Byron	Med	\$5,110.00
NORTHWOOD_001735-COLE_S_SB	De Mun	Mid Block	Med	\$30,720.00
WESTWOOD_002437-COLE_E_SB	Oxford	Wellington	Med	\$5,010.00
BUCKINGHAM_000851-COLE_N_SB	Mid Block	Glen Ridge	Med	\$20,290.00
BUCKINGHAM_002426-COLE_N_SB	Mid Block	Mid Block	Med	\$12,820.00
BUCKINGHAM_003385-COLE_S_SB	Westwood	Mid Block	Med	\$28,460.00
OXFORD_002210-COLE_S_SB	Mid Block	Glen Ridge	Med	\$20,010.00
YORK_003333-COLE_S_SB	Mid Block	Glen Ridge	Med	\$19,970.00
BEMISTON_017348-COLE_W_CW	Shaw Park	Shirley	Med	\$4,780.00
CARSWOLD_017421-COLE_W_CW	Shirley	Walınca	Med	\$31,320.00
GLEN-RIDGE_002201-COLE_W_SB	Parkdale	Buckingham	Med	\$5,510.00
WESTWOOD_002237-COLE_E_SB	York	Clayton	Med	\$7,970.00



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BUCKINGHAM_003386-COLE_S_SB	Mid Block	Glen Ridge	Med	\$20,540.00
CARSWOLD_017436_COLE_E_CW	Shirley	Edgewood	Med	\$41,390.00
CECIL_001730-COLE_S_SB	University	Mid Block	Med	\$21,020.00
TOPTON_016445_COLE_E_CW	Topton	Brighton	Med	\$12,510.00
WESTMORELAND_016866_COLE_S_CW	Hanley	Mid Block	Med	\$28,020.00
GLEN-RIDGE_004062-COLE_W_SB	Buckingham	Oxford	Med	\$4,070.00
N-POLO_017334_COLE_N_CW	Mid Block	Mid Block	Med	\$10,340.00
EDGEWOOD_017410_COLE_W_CW	Hanley	Wydown	Med	\$31,150.00
MARYLAND_016871_COLE_N_CW	Hanley	Jackson	Med	\$39,410.00
MERAMEC_016937_COLE_E_CW	Mid Block	Orlando	Med	\$23,560.00
TOPTON_016438_COLE_W_CW	Ladue	Mark Twain	Med	\$36,080.00
DARTFORD_001297-COLE_W_SB	Wydown	Seminary	Med	\$66,150.00
GAY_016293_COLE_W_CW	Mid Block	Ladue	Med	\$14,290.00
MARYLAND_016879_COLE_S_CW	Hanley	Lee	Med	\$25,190.00
VENETIAN_017006_COLE_N_CW	Brentwood	Meramec	Med	\$31,930.00
MARYLAND_002811-COLE_N_SB	Central	Bemiston	Med	\$17,970.00
MERAMEC_016990_COLE_W_CW	Walinca	Rosiline	Med	\$8,180.00
VENETIAN_017030_COLE_S_CW	Brentwood	Meramec	Med	\$29,470.00
WESTMORELAND_016274_COLE_S_CW	Forsyth	Brentwood	Med	\$7,720.00
WESTMORELAND2_016100_COLE_N_CW	Forsyth	Brentwood	Med	\$13,660.00
ALAMO_001780-COLE_S_SB	Seminary	Mid Block	Med	\$4,690.00
ALAMO_001781-COLE_S_SB	Mid Block	De Mun	Med	\$6,660.00
PARKSIDE_016400_COLE_W_CW	Brighton	Forsyth	Med	\$8,510.00
SAN-BONITA_001773-COLE_N_SB	Mid Block	De Mun	Med	\$16,750.00
SAN-BONITA_001083-COLE_S_SB	Mid Block	Concordia	Med	\$6,190.00
ALAMO_003153-COLE_S_SB	Concordia	St Rita	Med	\$12,950.00
CENTRAL_004157-COLE_E_SB	Mid Block	Maryland	Med	\$13,110.00
CONCORDIA_001058-COLE_E_SB	Alamo	Clayton	Med	\$20,940.00
SAN-BONITA_001776-COLE_S_SB	Seminary	Mid Block	Med	\$7,430.00
ST-RITA_001022-COLE_E_SB	Forsyth	Brentwood	Med	\$1,480.00
SHAW-PARK_017555_COLE_W_CW	Shaw Park	Mid Block	Med	\$3,890.00
LYLE_003015-COLE_E_SB	Mid Block	Forsyth	Med	\$7,750.00
FOREST_002411-COLE_E_SB	Wydown	Mid Block	Med	\$24,250.00
BEMISTON_016654_COLE_W_CW	Mid Block	Kingsbury	Med	\$21,180.00
FORSYTH_016410_COLE_S_CW	Mid Block	Parkside	Med	\$22,530.00
KINGSBURY_016671_COLE_S_CW	Bemiston	Mid Block	Med	\$10,010.00
MARYLAND_016895_COLE_S_CW	Lee	Jackson	Med	\$7,540.00
MERAMEC_016583_COLE_W_CW	Kingsbury	Pershing	Med	\$17,050.00
WYDOWN_002416-COLE_S_SB	Forest	Westwood	Med	\$7,390.00
BRENTWOOD_017574_COLE_W_CW	Corporate Pk	Corporate Pk	Med	\$19,250.00
MERAMEC_016584_COLE_W_CW	Pershing	Westmoreland	Med	\$16,450.00
BEMISTON_016678_COLE_E_CW	Kingsbury	Pershing	Med	\$13,570.00
MARYLAND_016282_COLE_N_CW	Forsyth	Brentwood	Med	\$11,530.00
BEMISTON_016684_COLE_W_CW	Kingsbury	Pershing	Med	\$16,000.00
UNIVERSITY_001432-COLE_W_SB	Ellenwood	Wydown	Med	\$28,280.00
CROMWELL_003235-COLE_N_SB	Clayton	Mid Block	Med	\$22,960.00
CENTRAL_017250_COLE_N_CW	Mid Block	Mid Block	Med	\$250.00
CROMWELL_003234-COLE_N_SB	Mid Block	Glen Ridge	Med	\$19,200.00
SAN-BONITA_001085-COLE_N_SB	Mid Block	Concordia	Med	\$7,170.00
CROMWELL_002246-COLE_S_SB	Clayton	Glen Ridge	Med	\$17,240.00
MARYLAND_002845-COLE_N_SB	Linden	Hanley	Med	\$9,800.00
WELLINGTON_003342-COLE_S_SB	Mid Block	Glen Ridge	Med	\$22,330.00
COUNTRY-CLUB_019664_COLE_N_CW	Hanley	Hanley	Med	\$25,780.00
WHITBURN_002456-COLE_S_SB	Clayshire	Mid Block	Med	\$13,340.00
PARKDALE_002328-COLE_N_SB	Mid Block	Glen Ridge	Med	\$26,980.00



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PERSHING_016691_COLE_N_CW	Bemiston	Mid Block	Med	\$9,960.00
SUDBURY_002603-COLE_E_SB	Roxburgh	Whitburn	Med	\$11,990.00
CARSWOLD_017394_COLE_W_CW	Edgewood	Shirley	Med	\$50,320.00
DAYTONA_017064_COLE_N_CW	Brentwood	Mid Block	Med	\$21,900.00
GLEN-RIDGE_002254-COLE_W_SB	Cromwell	Clayton	Med	\$8,260.00
GLEN-RIDGE_002256-COLE_W_SB	Wellington	York	Med	\$4,800.00
GLEN-RIDGE_002257-COLE_W_SB	Oxford	Wellington	Med	\$7,920.00
MARYLAND_019603_COLE_N_CW	Topton	Brighton	Med	\$29,160.00
PARKDALE_003393-COLE_S_SB	Mid Block	Glen Ridge	Med	\$24,280.00
POLO_017330_COLE_W_CW	N Polo	Middle Polo	Med	\$24,480.00
EDGEWOOD_017440_COLE_E_CW	Shirley	Edgewood	Med	\$28,420.00
SHIRLEY_017430_COLE_S_CW	Hanley	Carswold	Med	\$8,370.00
WYDOWN_002375-COLE_N_SB	Forest	Edgewood	Med	\$14,230.00
EDGEWOOD_017403_COLE_E_CW	Oakley	Wydown	Med	\$16,280.00
FRANCIS_002593-COLE_W_SB	Roxburgh	Whitburn	Med	\$5,950.00
UNIVERSITY_001314-COLE_E_SB	Wydown	Aberdeen	Med	\$23,990.00
WALINCA_017494_COLE_N_CW	Mid Block	Hanley	Med	\$7,920.00
CARSWOLD_017463_COLE_S_CW	Edgewood	Shirley	Med	\$41,150.00
CENTRAL_017248_COLE_W_CW	Mid Block	Davis	Med	\$32,030.00
FORSYTH_016261_COLE_W_CW	Westmoreland	Mid Block	Med	\$1,310.00
LINDEN_016853_COLE_W_CW	Mid Block	Maryland	Med	\$17,940.00
MIDDLE-POLO_017380_COLE_N_CW	Polo	Polo	Med	\$16,730.00
POLO_017360_COLE_E_CW	Rear	Polo	Med	\$27,080.00
POLO_017362_COLE_N_CW	Polo	Hanley	Med	\$6,470.00
WYDOWN_001405-COLE_N_SB	Shepley	University	Med	\$12,700.00
WYDOWN_002374-COLE_N_SB	Edgewood	Mid Block	Med	\$4,140.00
BRENTWOOD_016116_COLE_W_CW	Pershing	University	Med	\$6,110.00
CECIL_001439-COLE_N_SB	University	Ellenwood	Med	\$44,560.00
FORSYTH_016033_COLE_E_CW	Unversity	Westmoreland	Med	\$5,940.00
FORSYTH_016260_COLE_W_CW	Pershing	Westmoreland	Med	\$16,840.00
MERAMEC_019637_COLE_W_CW	Rosline	Orlando	Med	\$9,150.00
POLO_017364_COLE_S_CW	Polo	Hanley	Med	\$3,900.00
POLO_017373_COLE_W_CW	Middle Polo	Polo	Med	\$19,060.00
SUDBURY_002567-COLE_S_SB	Mid Block	Clayshire	Med	\$38,240.00
UNIVERSITY_001434-COLE_E_SB	Ellenwood	Cecil	Med	\$12,910.00
WYDOWN_001347-COLE_S_SB	University	De Mun	Med	\$27,390.00
DAVIS_017038_COLE_N_CW	Brentwood	Meramec	Med	\$32,880.00
DE-MUN_001342-COLE_W_SB	Wydown	Aberdeen	Med	\$34,600.00
POLO_017345_COLE_W_CW	Mid Block	N Polo	Med	\$4,010.00
UNIVERSITY_016120_COLE_N_CW	Forsyth	Brentwood	Med	\$23,910.00
WATKINS_017079_COLE_N_CW	Brentwood	Meramec	Med	\$42,870.00
JACKSON_016910_COLE_E_CW	Mid Block	Maryland	Med	\$3,810.00
SAN-BONITA_001767-COLE_S_SB	Concordia	St Rita	Med	\$24,200.00
UNIVERSITY_001446-COLE_E_SB	Cecil	Wydown	Med	\$13,920.00
UNIVERSITY2_016041_COLE_N_CW	Forsyth	Brentwood	Med	\$17,710.00
WYDOWN_001386-COLE_N_SB	Big Bend	Shepley	Med	\$13,530.00
WYDOWN_001460-COLE_N_SB	University	Mid Block	Med	\$41,490.00
DARTFORD_001299-COLE_E_SB	Wydown	Aberdeen	Med	\$11,330.00
FORSYTH_016263_COLE_E_CW	Westmoreland	Maryland	Med	\$7,740.00
MARYLAND_016896_COLE_S_CW	Mid Block	Mid Block	Med	\$1,000.00
UNIVERSITY_001312-COLE_W_SB	Wydown	Aberdeen	Med	\$5,100.00
WYDOWN_001382-COLE_S_SB	Dartford	University	Med	\$18,100.00
SEMINARY_001095-COLE_W_SB	Mid Block	San Bonita	Med	\$9,190.00
SAN-BONITA_001770-COLE_N_SB	St Rita	Seminary	Med	\$49,680.00
KINGSBURY_016563_COLE_S_CW	Brentwood	Meramec	Med	\$11,840.00



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SAN-BONITA_001771-COLE_N_SB	Seminary	Mid Block	Med	\$22,280.00
FORSYTH_003008-COLE_N_SB	Lyle	Lee	Med	\$23,820.00
SAN-BONITA_001769-COLE_N_SB	Concordia	Mid Block	Med	\$26,240.00
FORSYTH_002928-COLE_N_SB	Hanley	Lyle	Med	\$10,030.00
SUDBURY_017620_COLE_E_CW	Mid Block	Roxburgh	Med	\$1,880.00
KINGSBURY_016614_COLE_S_CW	Meramec	Central	Med	\$16,250.00
SEMINARY_001097-COLE_E_SB	Mid Block	San Bonita	Med	\$8,280.00
CLAYSHIRE_002562-COLE_W_SB	Whitburn	Mid Block	Med	\$200.00
FORSYTH_016265_COLE_W_CW	Mid Block	Maryland	Med	\$8,530.00
JACKSON_016901_COLE_W_CW	Maryland	Forsyth	Med	\$17,820.00
MARYLAND2_016434_COLE_N_CW	Brighton	Forsyth	Med	\$5,920.00
FORSYTH_002930-COLE_S_SB	Hanley	Lyle	Med	\$30,180.00
FORSYTH_016412_COLE_W_CW	Brighton	Forsyth	Med	\$21,590.00
MARYLAND_016367_COLE_S_CW	Topton	Brighton	Med	\$7,450.00
SAN-BONITA_001006-COLE_S_SB	St Rita	Seminary	Med	\$7,630.00
CARONDELET_003020-COLE_N_SB	Lyle	Coronado	Med	\$15,720.00
WALINCA_017496_COLE_S_CW	Mid Block	Hanley	Med	\$10,540.00
GAY_016353_COLE_E_CW	Ladue	Mid Block	Med	\$21,330.00
CARONDELET_003088-COLE_S_SB	Lyle	Coronado	Med	\$15,730.00
CARONDELET_003022-COLE_N_SB	Hanley	Lyle	Med	\$3,550.00
LYLE_002937-COLE_E_SB	Forsyth	Mid Block	Med	\$7,830.00
CARONDELET_003019-COLE_N_SB	Coronado	Mid Block	Med	\$19,110.00
JACKSON_016909_COLE_E_CW	Maryland	Forsyth	Med	\$18,030.00
ARUNDEL_001184-COLE_S_SB	De Mun	Mid Block	Med	\$36,860.00
CECIL_001729-COLE_S_SB	Mid Block	Ellenwood	Med	\$30,680.00
PERSHING_019611_COLE_N_CW	Forsyth	Brentwood	Med	\$35,420.00
BRENTWOOD_017099_COLE_E_CW	Watkins	Whitburn	Med	\$20,670.00
EDGEWOOD_017400_COLE_N_CW	Mid Block	Carswold	Med	\$18,910.00
EDGEWOOD_017402_COLE_E_CW	Carswold	Oakley	Med	\$56,010.00
ARUNDEL_001254-COLE_S_SB	University	De Mun	Med	\$57,120.00
BRENTWOOD_016141_COLE_W_CW	Kingsbury	Pershing	Med	\$6,110.00
DE-MUN_001173-COLE_W_SB	Arundel	Arundel	Med	\$6,330.00
KINGSBURY_016196_COLE_N_CW	Forsyth	Brentwood	Med	\$43,100.00
POLO_017368_COLE_E_CW	Middle Polo	Mid Block	Med	\$18,740.00
RITZ-CARLTON_019563_COLE_S_CW	Coronado	Forsyth	Med	\$39,210.00
ARUNDEL_001250-COLE_N_SB	De Mun	Mid Block	Med	\$40,660.00
BILTMORE_017319_COLE_E_CW	Davis	Clayton	Med	\$22,320.00
CENTRAL_017254_COLE_N_CW	Mid Block	Mid Block	Med	\$1,140.00
DE-MUN_001178-COLE_E_SB	Arundel	Mid Block	Med	\$5,900.00
OLETA_017097_COLE_E_CW	Brentwood	Crescent	Med	\$24,500.00
PERSHING_016113_COLE_S_CW	Forsyth	Brentwood	Med	\$27,010.00
POLO_017329_COLE_W_CW	Middle Polo	Mid Block	Med	\$15,260.00
SHIRLEY_017449_COLE_S_CW	Shirley	Oakley	Med	\$10,060.00
STRATFORD_016192_COLE_S_CW	San Bonita	Private	Med	\$46,750.00
WYDOWN_001504-COLE_N_SB	De Mun	Ellenwood	Med	\$10,380.00
ARUNDEL_001320-COLE_N_SB	University	De Mun	Med	\$33,090.00
DAVIS_017259_COLE_N_CW	Central	Hanley	Med	\$52,690.00
DE-MUN_001472-COLE_E_SB	Wydown	Fauquier	Med	\$25,500.00
KINGSBURY_016145_COLE_S_CW	Forsyth	Brentwood	Med	\$34,510.00
MERAMEC_017013_COLE_E_CW	Orlando	Davis	Med	\$20,360.00
POLO_017370_COLE_S_CW	Mid Block	Polo	Med	\$14,870.00
WYDOWN_001383-COLE_S_SB	Big Bend	Dartford	Med	\$34,120.00
BRENTWOOD_016201_COLE_W_CW	Stratford	Kingsbury	Med	\$7,090.00
FORSYTH_016107_COLE_E_CW	Pershing	University	Med	\$4,200.00
MERAMEC_017011_COLE_W_CW	Orlando	Venetian	Med	\$5,500.00



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POLO_017327_COLE_S_CW	Mid Block	Polo	Med	\$29,880.00
DE-MUN_001228-COLE_E_SB	Fauquier	Aberdeen	Med	\$3,160.00
DE-MUN_001245-COLE_E_SB	Aberdeen	Arundel	Med	\$11,020.00
DE-MUN_001325-COLE_W_SB	Mid Block	Arundel	Med	\$340.00
POLO_017366_COLE_N_CW	Mid Block	Polo	Med	\$11,660.00
WYDOWN_001615-COLE_S_SB	Mid Block	Forest Ridge	Med	\$19,610.00
DE-MUN_001125-COLE_E_SB	San Bonita	Alamo	Med	\$3,900.00
ALAMO_001785-COLE_N_SB	De Mun	Mid Block	Med	\$9,840.00
SAN-BONITA_000868-COLE_S_SB	De Mun	Mid Block	Med	\$5,640.00
MARYLAND_016307_COLE_N_CW	Lancaster	Topton	Low	\$19,460.00
BRENTWOOD_016575_COLE_E_CW	Clayton Lane	Kingsbury	Low	\$9,700.00
BRENTWOOD_017583_COLE_W_CW	Corporate Pk	Mid Block	Low	\$19,910.00
FORSYTH_002986-COLE_N_SB	Lee	Jackson	Low	\$6,070.00
RITZ-CARLTON_003098-COLE_S_SB	Mid Block	Mid Block	Low	\$12,100.00
WYDOWN_001605-COLE_S_SB	Forest Ridge	Big Bend	Low	\$3,790.00
LYLE_002935-COLE_W_SB	Forsyth	Mid Block	Low	\$840.00
WYDOWN_003148-COLE_N_SB	Mid Block	De Mun	Low	\$3,400.00
EDGEWOOD_017477_COLE_W_CW	Shirley	Oakley	Low	\$28,940.00
OAKLEY_017482_COLE_E_CW	Shirley	Edgewood	Low	\$28,700.00
SAN-BONITA_001160-COLE_N_SB	De Mun	Mid Block	Low	\$22,670.00
RIDGEMOOR_001711-COLE_S_SB	Mid Block	Mid Block	Low	\$870.00
BILTMORE_017296_COLE_W_CW	Davis	Mohawk	Low	\$8,880.00
WYDOWN_004048-COLE_S_SB	De Mun	Ellenwood	Low	\$31,170.00
ABERDEEN_001232-COLE_N_SB	De Mun	Mid Block	Low	\$43,070.00
CARSWOLD_017396_COLE_N_CW	Mid Block	Mid Block	Low	\$17,210.00
MERAMEC_017181_COLE_E_CW	Watkins	Clayton	Low	\$42,230.00
SHIRLEY_017451_COLE_N_CW	Carswold	Edgewood	Low	\$20,930.00
UNIVERSITY_001315-COLE_E_SB	Aberdeen	Arundel	Low	\$12,590.00
ABERDEEN_001332-COLE_S_SB	University	De Mun	Low	\$22,280.00
ABERDEEN_001337-COLE_N_SB	University	De Mun	Low	\$55,700.00
ARUNDEL_001185-COLE_S_SB	Mid Block	Mid Block	Low	\$23,170.00
BEMISTON_017274_COLE_W_CW	Davis	Clayton	Low	\$40,900.00
DARTFORD_001300-COLE_E_SB	Aberdeen	Arundel	Low	\$5,780.00
DE-MUN_001326-COLE_W_SB	Aberdeen	Mid Block	Low	\$7,520.00
EDGEWOOD_017456_COLE_W_CW	Carswold	Shirley	Low	\$14,020.00
OLETA_017095_COLE_W_CW	Watkins	Crescent	Low	\$9,240.00
UNIVERSITY_001311-COLE_W_SB	Aberdeen	Arundel	Low	\$10,090.00
ABERDEEN_001234-COLE_S_SB	De Mun	Mid Block	Low	\$33,270.00
DAYTONA_017065_COLE_N_CW	Mid Block	Meramec	Low	\$18,410.00
ABERDEEN_001336-COLE_N_SB	Dartford	University	Low	\$14,100.00
ARUNDEL_001255-COLE_S_SB	Dartford	University	Low	\$38,780.00
BEMISTON_017280_COLE_E_CW	Mohawk	Biltmore	Low	\$10,720.00
BILTMORE_017315_COLE_N_CW	Bemiston	W Biltmore	Low	\$12,940.00
BLAND_003111-COLE_S_SB	Northmoor	Mid Block	Low	\$16,750.00
MERAMEC_017026_COLE_W_CW	Venetian	Davis	Low	\$4,150.00
WATKINS_017089_COLE_S_CW	Oleta	Meramec	Low	\$33,890.00
ARUNDEL_001305-COLE_N_SB	Dartford	University	Low	\$13,440.00
BEMISTON_017279_COLE_E_CW	Biltmore	Clayton	Low	\$7,180.00
BILTMORE_017313_COLE_W_CW	Mohawk	Biltmore	Low	\$4,600.00
BILTMORE_017317_COLE_S_CW	Bemiston	Clayton	Low	\$22,480.00
DARTFORD_001295-COLE_E_SB	Arundel	Mid Block	Low	\$4,690.00
ELLENWOOD_003142-COLE_S_SB	University	Mid Block	Low	\$25,280.00
SAN-BONITA_000869-COLE_S_SB	Mid Block	Mid Block	Low	\$23,290.00
MARYLAND_016305_COLE_N_CW	Gay	Crandon	Low	\$12,560.00
RIDGEMOOR_001708-COLE_N_SB	Broadview	Mid Block	Low	\$9,400.00



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Casename	From Street	To Street	Priority	Costs
FORSYTH_001540-COLE_S_SB	Olympian	Wallace	Low	\$10,010.00
RIDGEMOOR_001710-COLE_S_SB	Mid Block	Mid Block	Low	\$2,530.00
RIDGEMOOR_001713-COLE_W_SB	Mid Block	Mid Block	Low	\$31,140.00
ALAMO_001786-COLE_N_SB	Mid Block	Mid Block	Low	\$2,450.00
ALAMO_000767-COLE_S_SB	De Mun	Mid Block	Low	\$10,780.00
ELLENWOOD_003139-COLE_N_SB	University	Mid Block	Low	\$9,360.00
FORSYTH_001545-COLE_N_SB	Olympian	Wallace	Low	\$4,150.00
GAY_016299_COLE_E_CW	Mid Block	Ladue	Low	\$22,480.00
BLAND_003113-COLE_N_SB	Mid Block	Northmoor	Low	\$15,650.00
DAVIS_017265_COLE_S_CW	Biltmore	Hanley	Low	\$10,540.00
MARYLAND_016315_COLE_N_CW	Crandon	Lancaster	Low	\$13,000.00
FORSYTH_001518-COLE_S_SB	Wallace	Mid Block	Low	\$65,050.00
BLAND_003114-COLE_N_SB	Mid Block	Northmoor	Low	\$7,260.00
BRENTWOOD_017109_COLE_E_CW	Whitburn	Clayton	Low	\$23,390.00
BRENTWOOD_016202_COLE_W_CW	Clayton Lane	Stratford	Low	\$6,760.00
ELLENWOOD_001497-COLE_S_SB	Mid Block	University	Low	\$3,320.00
ST-RITA_001024-COLE_W_SB	Forsyth	Brentwood	Low	\$24,650.00
FORSYTH_001546-COLE_N_SB	Wallace	Mid Block	Low	\$3,680.00
MERAMEC_017048_COLE_W_CW	Daytona	Mid Block	Low	\$13,660.00
SEMINARY_001001-COLE_E_SB	Alamo	Clayton	Low	\$9,160.00
BEMISTON_017283_COLE_W_CW	Central	Bemiston	Low	\$15,670.00
MERAMEC_017046_COLE_E_CW	Daytona	Mid Block	Low	\$23,070.00
NORTHMOOR_003121-COLE_W_SB	Mid Block	Polo	Low	\$7,720.00
BEMISTON_017284_COLE_W_CW	Central	Mid Block	Low	\$350.00
CENTRAL_017209_COLE_W_CW	Davis	Mid Block	Low	\$20,000.00
FORSYTH_016259_COLE_W_CW	Kingsbury	Pershing	Low	\$4,730.00
MERAMEC2_017177_COLE_W_CW	Watkins	Clayton	Low	\$27,710.00
ABERDEEN_001333-COLE_S_SB	Aberdeen	Mid Block	Low	\$2,570.00
FORSYTH2_016129_COLE_E_CW	Kingsbury	Pershing	Low	\$5,750.00
MOHAWK_017304_COLE_N_CW	Bemiston	Biltmore	Low	\$15,750.00
ABERDEEN_001334-COLE_S_SB	Dartford	University	Low	\$10,190.00
CENTRAL_017231_COLE_W_CW	Davis	Mid Block	Low	\$14,730.00
DAVIS_017214_COLE_S_CW	Meramec	Central	Low	\$6,720.00
DAVIS_017216_COLE_N_CW	Meramec	Central	Low	\$9,010.00
DAVIS_017269_COLE_S_CW	Bemiston	Biltmore	Low	\$17,790.00
GAY_016292_COLE_W_CW	Colonial	Mid Block	Low	\$9,940.00
MOHAWK_017306_COLE_S_CW	Bemiston	Biltmore	Low	\$14,700.00
STRATFORD_016218_COLE_N_CW	Private	Clayton	Low	\$36,760.00
MERAMEC_017049_COLE_W_CW	Davis	Daytona	Low	\$6,110.00
ELLENWOOD_003143-COLE_S_SB	Mid Block	Cecil	Low	\$17,360.00
FORSYTH_001541-COLE_S_SB	Big Bend	Mid Block	Low	\$35,890.00
BUCKINGHAM_002057-COLE_N_SB	Glen Ridge	Mid Block	Low	\$23,380.00
BYRON_019767_COLE_N_CW	Glen Ridge	Audubon	Low	\$18,560.00
WYDOWN_004049-COLE_S_SB	Wydown	Mid Block	Low	\$24,410.00
ELLENWOOD_001499-COLE_S_SB	Cecil	Wydown	Low	\$15,870.00
CLAVERACH_001651-COLE_S_SB	Crestwood	Broadview	Low	\$32,460.00
BUCKINGHAM_002059-COLE_S_SB	Glen Ridge	Mid Block	Low	\$11,900.00
BYRON_003174-COLE_S_SB	Glen Ridge	Mid Block	Low	\$12,410.00
FRANCIS_017592_COLE_N_CW	Francis	Brentwood	Low	\$1,040.00
GLEN-RIDGE_002064-COLE_E_SB	Parkdale	Buckingham	Low	\$7,450.00
RIDGEMOOR_001714-COLE_W_SB	Ridgemoor	Mid Block	Low	\$48,060.00
RIDGEMOOR_001792-COLE_E_SB	Wydown	Ridgemoor	Low	\$55,710.00
WYDOWN_002194-COLE_S_SB	Glen Ridge	Audubon	Low	\$35,520.00
YORK_001984-COLE_S_SB	Glen Ridge	Mid Block	Low	\$17,200.00
FORSYTH_001543-COLE_N_SB	Big Bend	Olympian	Low	\$20,140.00



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GLEN-RIDGE_002199-COLE_E_SB	Wydown	Byron	Low	\$9,490.00
GLEN-RIDGE_002251-COLE_E_SB	York	Cromwell	Low	\$8,090.00
RIDGEMOOR_001791-COLE_E_SB	Ridgemoor	Mid Block	Low	\$44,100.00
BROADVIEW_001679-COLE_W_SB	Clavernach	Clayton	Low	\$31,030.00
BROADVIEW_001681-COLE_E_SB	Clavernach	Mid Block	Low	\$36,420.00
BYRON_003175-COLE_S_SB	Mid Block	Audubon	Low	\$18,030.00
GLEN-RIDGE_002203-COLE_E_SB	Buckingham	Oxford	Low	\$10,610.00
GLEN-RIDGE_002080-COLE_E_SB	Byron	Parkdale	Low	\$7,800.00
WHITBURN_002455-COLE_S_SB	Mid Block	Francis	Low	\$2,290.00
BRENTWOOD_017584_COLE_W_CW	Mid Block	Whitburn	Low	\$2,860.00
FRANCIS_017103_COLE_E_CW	Whitburn	Clayton	Low	\$18,780.00
BRENTWOOD_002450-COLE_W_SB	Whitburn	Clayton	Low	\$9,110.00
BRENTWOOD_016576_COLE_E_CW	Mid Block	Clayton Lane	Low	\$1,480.00
ELLENWOOD_003138-COLE_N_SB	Mid Block	Cecil	Low	\$20,430.00
CRESCENT_017125_COLE_W_CW	Seminole	Clayton	Low	\$15,350.00
ELLENWOOD_003136-COLE_N_SB	Wydown	Mid Block	Low	\$20,900.00
CLAYSHIRE_002462-COLE_E_SB	Whitburn	Mid Block	Low	\$5,570.00
CRESCENT_017123_COLE_N_CW	Oleta	Clayton	Low	\$52,220.00
ELLENWOOD_003137-COLE_N_SB	Cecil	Wydown	Low	\$23,300.00
OLETA_017137_COLE_E_CW	Seminole	Clayton	Low	\$10,140.00
SHIRLEY_017472_COLE_N_CW	Oakley	Edgewood	Low	\$9,200.00
OLETA_017115_COLE_W_CW	Oleta	Clayton	Low	\$17,680.00
SEMINOLE_017131_COLE_S_CW	Oleta	Crescent	Low	\$15,010.00
CRESCENT_017166_COLE_W_CW	Oleta	Seminole	Low	\$19,280.00
FORSYTH_016159_COLE_E_CW	Stratford	Topton	Low	\$17,520.00
FORSYTH_016246_COLE_S_CW	Crandon	Topton	Low	\$16,650.00
SEMINOLE_017143_COLE_S_CW	Oleta	Crescent	Low	\$15,830.00
BEMISTON_017281_COLE_E_CW	Davis	Mohawk	Low	\$7,260.00
GLEN-RIDGE_002204-COLE_E_SB	Oxford	Wellington	Low	\$10,270.00
KINGSBURYFORSYTH_016150_COLE_N_CW	Topton	Mid Block	Low	\$40.00
OLETA_017172_COLE_E_CW	Oleta	Seminole	Low	\$6,600.00
OXFORD_003348-COLE_N_SB	Glen Ridge	Mid Block	Low	\$26,250.00
YORK_003335-COLE_N_SB	Glen Ridge	Mid Block	Low	\$33,210.00
GLEN-RIDGE_019685_COLE_E_CW	Wellington	York	Low	\$7,850.00
SOMERSET_003202-COLE_N_SB	Mid Block	Audubon	Low	\$12,750.00
AUDUBON_003187-COLE_E_SB	Oxfor	York	Low	\$28,260.00
HILLVALE_001724-COLE_W_SB	Wydown	Mid Block	Low	\$42,910.00
YORK_001987-COLE_N_SB	Mid Block	Audubon	Low	\$19,380.00
AUDUBON_001911-COLE_E_SB	Hillvale	Clayton	Low	\$25,120.00
CRESTWOOD_001811-COLE_W_SB	Hillvale	Clayton	Low	\$32,820.00
CROMWELL_001967-COLE_N_SB	Glen Ridge	Mid Block	Low	\$26,610.00
HARCOURT_001587-COLE_N_SB	Hillvale	Wydown	Low	\$28,250.00
RIDGEMOOR_001717-COLE_S_SB	Hillvale	Clavernach	Low	\$31,640.00
WELLINGTON_001992-COLE_S_SB	Glen Ridge	Mid Block	Low	\$15,550.00
WELLINGTON_002013-COLE_N_SB	Glen Ridge	Audubon	Low	\$29,780.00
AUDUBON_001827-COLE_W_SB	Wellington	York	Low	\$4,890.00
AUDUBON_001930-COLE_W_SB	Cromwell	Somerset	Low	\$9,570.00
BROADVIEW_001646-COLE_W_SB	Hillvale	Clavernach	Low	\$10,190.00
CRESTWOOD_001805-COLE_E_SB	Mid Block	Mid Block	Low	\$32,490.00
CROMWELL_001965-COLE_S_SB	Glen Ridge	Mid Block	Low	\$9,820.00
HARCOURT_003161-COLE_N_SB	Hillvale	Hillvale	Low	\$2,960.00
OXFORD_002030-COLE_S_SB	Glen Ridge	Audubon	Low	\$29,590.00
PARKDALE_002075-COLE_N_SB	Glen Ridge	Audubon	Low	\$40,500.00
PARKDALE_003392-COLE_S_SB	Glen Ridge	Mid Block	Low	\$21,700.00
SOMERSET_001920-COLE_S_SB	Clayton	Audubon	Low	\$21,540.00



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Casename	From Street	To Street	Priority	Costs
CLAVERACH_000801-COLE_N_SB	Crestwood	Broadview	Low	\$36,040.00
CRESTWOOD_001669-COLE_E_SB	Clavernach	Clayton	Low	\$7,610.00
CRESTWOOD_001809-COLE_E_SB	Hillvale	Clavernach	Low	\$12,800.00
AUDUBON_001913-COLE_W_SB	Somerset	Clayton	Low	\$3,400.00
CRESTWOOD_001860-COLE_W_SB	Mid Block	Mid Block	Low	\$28,130.00
YORK_001983-COLE_S_SB	Mid Block	Audubon	Low	\$17,220.00
GLEN-RIDGE_002252-COLE_E_SB	Cromwell	Clayton	Low	\$7,370.00
WYDOWN_001616-COLE_S_SB	Ridgemoor	Mid Block	Low	\$20,340.00
FORSYTH_003128-COLE_S_SB	Mid Block	Chaplin	Low	\$37,170.00
FORSYTH_001548-COLE_N_SB	Mid Block	Chaplin	Low	\$4,980.00
AUDUBON_003188-COLE_E_SB	Yrok	Hillvale	Low	\$10,540.00
GAY_016289_COLE_W_CW	Mid Block	Colonial	Low	\$14,450.00
AUDUBON_003186-COLE_E_SB	Parkdale	Oxford	Low	\$31,710.00
HILLVALE_001723-COLE_W_SB	Mid Block	Broadview	Low	\$43,620.00
RIDGEMOOR_001579-COLE_W_SB	Mid Block	Mid Block	Low	\$18,970.00
OXFORD_003349-COLE_N_SB	Mid Block	Audubon	Low	\$18,920.00
WELLINGTON_003341-COLE_S_SB	Mid Block	Audubon	Low	\$23,510.00
BUCKINGHAM_002056-COLE_N_SB	Mid Block	Audubon	Low	\$14,540.00
SOMERSET_003201-COLE_N_SB	Clayton	Mid Block	Low	\$22,850.00
AUDUBON_001894-COLE_W_SB	Parkdale	Buckingham	Low	\$4,110.00
CROMWELL_001968-COLE_N_SB	Mid Block	Audubon	Low	\$24,660.00
HILLVALE_001635-COLE_N_SB	Crestwood	Broadview	Low	\$32,590.00
RIDGEMOOR_001578-COLE_W_SB	Harcourt	Mid Block	Low	\$11,820.00
WYDOWN_001617-COLE_S_SB	Hillvale	Ridgemoor	Low	\$43,100.00
WYDOWN_001798-COLE_S_SB	Crestwood	Hillvale	Low	\$16,420.00
AUDUBON_001895-COLE_W_SB	Buckingham	Oxford	Low	\$9,850.00
AUDUBON_001976-COLE_W_SB	York	Cromwell	Low	\$9,050.00
CRESTWOOD_001808-COLE_E_SB	Mid Block	Hillvale	Low	\$9,830.00
HILLVALE_003169-COLE_E_SB	Wydown	Harcourt	Low	\$11,970.00
HILLVALE_001638-COLE_S_SB	Mid Block	Broadview	Low	\$16,180.00
HILLVALE_001819-COLE_N_SB	Audubon	Crestwood	Low	\$6,100.00
AUDUBON_002018-COLE_W_SB	Oxfor	Wellington	Low	\$4,840.00
BUCKINGHAM_003387-COLE_S_SB	Mid Block	Audubon	Low	\$15,510.00
CRESTWOOD_001807-COLE_E_SB	Mid Block	Mid Block	Low	\$38,990.00
CROMWELL_004070-COLE_S_SB	Mid Block	Audubon	Low	\$26,580.00
HILLVALE_001637-COLE_S_SB	Crestwood	Mid Block	Low	\$11,320.00
HILLVALE_001817-COLE_S_SB	Audubon	Crestwood	Low	\$10,220.00
RIDGEMOOR_001716-COLE_S_SB	Broadview	Ridgemoor	Low	\$14,930.00
CRESTWOOD_001859-COLE_W_SB	Mid Block	Hillvale	Low	\$40,580.00
CRESTWOOD_001861-COLE_W_SB	Wydown	Mid Block	Low	\$12,840.00
RIDGEMOOR_001580-COLE_W_SB	Mid Block	Ridgemoor	Low	\$11,590.00
WYDOWN_001866-COLE_S_SB	Audubon	Crestwood	Low	\$6,300.00
EAST_016224_COLE_W_CW	Mid Block	Stratford	Low	\$4,680.00
FORSYTH_001550-COLE_N_SB	Hoyt	Mid Block	Low	\$9,680.00
FORSYTH_003129-COLE_S_SB	Chaplin	Mid Block	Low	\$36,970.00
FORSYTH_001549-COLE_N_SB	Chaplin	Hoyt	Low	\$5,250.00
AUDUBON_003185-COLE_E_SB	Wydown	Parkdale	Low	\$28,830.00
EAST_016222_COLE_E_CW	Mid Block	Stratford	Low	\$4,620.00
STRATFORD_016239_COLE_N_CW	Alamo	Clayton	Low	\$5,140.00
AUDUBON_001890-COLE_W_SB	Byron	Parkdale	Low	\$3,750.00
PARKDALE_003391-COLE_S_SB	Mid Block	Audubon	Low	\$15,620.00
AUDUBON_001889-COLE_W_SB	Wydown	Byron	Low	\$8,580.00
HILLVALE_001573-COLE_E_SB	Harcourt	Mid Block	Low	\$17,860.00

Sidewalk Data Reference Guide:

The following list is reference for the headers (1-26) in the sidewalk data spreadsheet.

1. Casename
2. From Street
3. To Street
4. Length (linear feet)
5. Fixed Obstructions
6. Vegetation Obstructions
7. Changes in Level: 0.25"-0.49"
8. Changes in Level: 0.50"-0.74"
9. Changes in Level: 0.75"-0.99"
10. Changes in Level: 1" or More
11. Running Slope (linear feet): 5-8.33%
12. Running Slope (linear feet): 8.33-10%
13. Running Slope (linear feet): 10-12.5%
14. Running Slope (linear feet): 12.5% or More
15. Cross Slope (linear feet): 2-4%
16. Cross Slope (linear feet): 4-6%
17. Cross Slope (linear feet): 6-8%
18. Cross Slope (linear feet): 8-10%
19. Cross Slope (linear feet): 10% or More
20. Activity Score
21. Impedance Score
22. Final Score
23. Activity Ranking
24. Impedance Ranking
25. Final Ranking
26. Costs



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1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22	23	24	25	26
MARYLAND_002873-COLE_S_SB	Meramec	Central	319	2	0	21	9	0	1	161	2	2	2	64	81	31	21	77	62	27	89	High	High	High	\$29,150
BRENTWOOD_002693-COLE_E_SB	Mid Block	Forsyth	162	1	0	8	0	0	0	0	0	0	0	44	94	8	1	10	62	26	88	High	High	High	\$21,420
CENTRAL_002891-COLE_W_SB	Maryland	Forsyth	444	1	0	11	5	0	0	0	0	0	0	148	249	2	0	0	62	26	88	High	High	High	\$28,340
MERAMEC_002877-COLE_E_SB	Maryland	Forsyth	440	1	0	19	3	0	0	0	0	0	0	207	72	12	0	0	62	26	88	High	High	High	\$23,830
BEMISTON_003503-COLE_W_SB	Bonhomme	Shaw Park	461	2	0	18	2	0	1	0	5	3	0	151	22	16	0	0	62	25	87	High	High	High	\$25,500
MERAMEC_003454-COLE_E_SB	Bonhomme	Shaw Park	654	1	0	11	4	0	1	0	0	0	0	117	94	7	0	0	62	25	87	High	High	High	\$49,600
BONHOMME_003540-COLE_N_SB	Bemiston	Hanley	719	1	0	29	9	2	4	0	0	0	0	269	207	74	45	33	59	27	86	High	High	High	\$55,200
FOREST_002406-COLE_W_SB	Wydown	Mid Block	379	1	0	9	6	1	1	48	0	0	0	125	129	119	0	0	59	27	86	High	High	High	\$24,290
MARYLAND_002872-COLE_S_SB	Central	Bemiston	319	2	0	16	2	0	1	32	6	3	0	112	104	28	1	4	59	26	85	High	High	High	\$28,220
CARONDELET_000864-COLE_S_SB	Bemiston	Hanley	712	3	1	26	3	1	0	0	0	0	0	345	230	45	9	29	59	26	85	High	High	High	\$54,700
MARYLAND_002871-COLE_S_SB	Bemiston	Hanley	716	1	1	36	8	0	1	0	0	0	0	320	163	44	7	3	59	26	85	High	High	High	\$37,260
BEMISTON_003677-COLE_W_SB	Maryland	Forsyth	435	1	0	16	3	1	2	0	3	6	3	141	59	10	0	0	59	26	85	High	High	High	\$37,450
MERAMEC_003778-COLE_E_SB	Forsyth	Carondelet	451	1	0	4	0	0	0	211	34	3	0	177	93	43	8	5	59	26	85	High	High	High	\$17,660
CARONDELET_003622-COLE_N_SB	Bemiston	Hanley	728	1	0	12	4	0	1	0	0	0	0	215	123	7	0	0	59	25	84	High	High	High	\$47,440
FORSYTH_003670-COLE_N_SB	Bemiston	Hanley	698	1	0	13	0	1	1	54	0	0	0	120	68	3	8	6	59	25	84	High	High	High	\$34,310
TOPTON2_016373_COLE_E_CW	Maryland	Mid Block	774	1	0	34	15	6	1	0	11	12	9	251	183	67	30	91	58	27	84	High	High	High	\$47,350
BONHOMME_003490-COLE_S_SB	Central	Bemiston	321	1	0	7	1	0	0	0	0	0	0	34	0	0	0	0	59	25	84	High	High	High	\$11,070
BRENTWOOD_017511_COLE_E_CW	Mid Block	Shaw Park	108	0	0	3	0	0	0	0	0	0	0	14	26	26	11	0	77	1	78	High	Low	High	\$7,220
BRENTWOOD_017512_COLE_E_CW	Bonhomme	Mid Block	551	0	1	18	2	0	1	0	9	5	2	221	19	4	0	0	77	0	77	High	Low	High	\$18,400
BONHOMME_003425-COLE_S_SB	Brentwood	Meramec	446	0	0	7	1	0	0	0	0	0	0	29	0	0	0	0	77	0	77	High	Low	High	\$24,420
DE-MUN_019703_COLE_E_CW	Northwood	Rosebury	297	1	3	16	3	0	0	0	0	0	0	48	117	58	26	6	49	27	75	Med	High	High	\$12,160
WESTMORELAND_016478_COLE_S_CW	Brentwood	Meramec	345	1	2	10	3	2	1	0	0	0	0	108	128	50	1	29	47	27	74	Med	High	High	\$28,400
MARYLAND_002766-COLE_N_SB	Brentwood	Meramec	419	1	0	38	8	1	4	0	0	0	0	201	143	24	6	0	47	26	73	Med	High	High	\$40,930
MARYLAND_002844-COLE_N_SB	Bemiston	Linden	347	1	1	26	6	0	0	0	3	5	1	122	69	41	21	18	47	26	73	Med	High	High	\$23,180
DE-MUN_000988-COLE_W_SB	Alamo	Clayton	297	1	0	5	3	1	0	0	0	0	0	34	68	77	26	25	45	27	71	Med	High	High	\$16,710
WYDOWN_002401-COLE_S_SB	Hanley	Forest	214	1	0	4	2	1	0	4	67	17	5	56	57	16	13	0	44	26	70	Med	High	High	\$17,680
WYDOWN_002376-COLE_N_SB	Hanley	Mid Block	264	1	0	5	4	2	0	0	0	0	0	96	48	7	0	0	44	26	70	Med	High	High	\$9,640
DE-MUN_001172-COLE_W_SB	Arundel	Mid Block	676	3	0	23	4	0	0	0	0	0	0	210	306	63	5	0	43	26	68	Med	High	High	\$32,330
CARONDELET_003076-COLE_S_SB	Hanley	Lyle	134	1	0	5	2	0	0	0	0	0	0	69	5	0	0	0	41	25	66	Med	High	High	\$7,780
SHAW-PARK_003410-COLE_N_SB	Brentwood	Meramec	424	0	0	8	3	1	0	140	39	9	3	213	57	14	9	36	65	1	66	High	Low	High	\$18,980
DE-MUN_001171-COLE_W_SB	Mid Block	San Bonita	305	2	0	7	2	1	1	0	0	0	0	4	99	138	62	0	39	27	66	Med	High	High	\$19,100
FORSYTH_002961-COLE_S_SB	Lyle	Carondelet	453	1	0	25	9	1	0	256	37	15	10	150	28	29	5	0	38	26	64	Med	High	High	\$24,300
MARYLAND_002763-COLE_S_SB	Mid Block	Meramec	148	0	0	19	10	1	0	0	0	0	0	31	12	33	3	0	62	2	64	High	Med	High	\$6,200
SHAW-PARK_003406-COLE_N_SB	Central	Bemiston	331	0	0	6	3	5	2	0	0	0	0	117	121	43	18	6	62	2	64	High	Med	High	\$24,710
FORSYTH_003756-COLE_S_SB	Brentwood	Meramec	429	0	0	32	15	1	0	391	29	7	2	183	65	38	28	11	62	2	64	High	Med	High	\$31,420
MARYLAND2_016365_COLE_S_CW	Gay	Topton	865	1	0	21	3	2	0	313	0	0	0	582	51	8	5	18	38	26	64	Med	High	High	\$32,390
BRENTWOOD_002692-COLE_E_SB	Maryland	Mid Block	275	0	0	12	0	0	0	68	0	0	0	28	55	151	2	0	62	2	64	High	Med	High	\$14,780
MARYLAND_004279-COLE_S_SB	Brentwood	Meramec	273	0	0	10	4	0	0	0	0	0	0	66	37	36	20	30	62	1	63	High	Med	High	\$15,810
BONHOMME_003805-COLE_N_SB	Brentwood	Meramec	435	0	0	33	6	0	0	76	8	5	4	172	128	50	16	4	62	1	63	High	Low	High	\$38,860
CENTRAL_003821-COLE_W_SB	Mid Block	Shaw Park	247	0	0	17	5	0	0	0	0	0	0	96	119	12	11	0	62	1	63	High	Low	High	\$13,780



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BRENTWOOD_002690-COLE_W_SB	Maryland	Mid Block	124	0	0	12	2	0	0	0	0	0	0	31	57	7	0	0	62	1	63	High	Low	High	\$8,710
CARONDELET_003743-COLE_N_SB	Brentwood	Meramec	435	0	0	22	2	0	0	0	8	0	0	16	99	102	20	0	62	1	63	High	Low	High	\$24,110
CARONDELET_003811-COLE_S_SB	Brentwood	Meramec	419	0	0	41	16	0	1	0	0	0	0	216	37	23	0	0	62	1	63	High	Low	High	\$18,680
CENTRAL_003819-COLE_E_SB	Bonhomme	Shaw Park	569	0	0	45	13	4	1	195	29	11	8	289	16	1	2	3	62	1	63	High	Low	High	\$31,530
SHAW-PARK_003408-COLE_N_SB	Meramec	Central	328	0	0	5	2	0	0	53	6	0	0	96	190	31	1	3	62	1	63	High	Low	High	\$9,230
BRENTWOOD_017520-COLE_E_CW	Forsyth	Carondelet	427	0	0	12	5	0	0	165	47	7	2	149	39	31	7	10	62	1	63	High	Low	High	\$23,930
FORSYTH_002715-COLE_N_SB	Brentwood	Meramec	431	0	0	26	5	0	2	52	6	3	6	192	27	0	0	0	62	1	63	High	Low	High	\$23,440
MERAMEC_002760-COLE_W_SB	Maryland	Forsyth	452	0	0	25	8	0	1	0	0	0	0	219	109	6	0	0	62	1	63	High	Low	High	\$35,170
BRENTWOOD_017516-COLE_E_CW	Carondelet	Bonhome	430	0	0	12	3	0	0	188	16	15	2	182	27	13	3	0	62	1	63	High	Low	High	\$15,500
BRENTWOOD_002689-COLE_W_SB	Mid Block	Forsyth	314	0	0	18	1	1	0	0	0	0	0	227	47	0	0	0	62	1	63	High	Low	High	\$17,950
MERAMEC_003452-COLE_W_SB	Bonhomme	Shaw Park	643	0	0	20	4	1	0	70	14	3	2	274	61	7	0	0	62	0	62	High	Low	High	\$22,800
CENTRAL_003692-COLE_E_SB	Maryland	Forsyth	455	0	0	8	2	1	0	0	0	0	0	181	10	0	0	0	62	0	62	High	Low	High	\$10,740
CENTRAL_003822-COLE_W_SB	Bonhomme	Mid Block	358	0	0	15	2	0	0	101	0	0	0	151	16	0	0	0	62	0	62	High	Low	High	\$13,330
CENTRAL_017189-COLE_W_CW	Mid Block	Clayton	863	1	1	27	5	0	2	0	9	0	0	446	211	40	7	7	37	26	62	Low	High	High	\$39,930
BRENTWOOD_017507-COLE_E_CW	Shaw Park	Walinca	152	0	0	5	0	0	0	0	0	0	0	34	4	0	0	0	62	0	62	High	Low	High	\$1,450
CENTRAL_017195-COLE_E_CW	Mid Block	Clayton	902	1	2	20	6	3	0	0	1	3	4	298	35	5	2	9	37	25	62	Low	High	High	\$31,410
DE-MUN_000914-COLE_E_SB	Alamo	Clayton	299	1	0	9	0	0	0	261	10	5	1	86	71	53	10	19	36	26	62	Low	High	High	\$23,210
SHAW-PARK_003523-COLE_N_SB	Bemiston	Mid Block	472	0	0	15	5	0	0	0	3	2	3	137	105	19	27	132	59	2	61	High	Med	High	\$32,450
MARYLAND_002647-COLE_S_SB	Forsyth	Brentwood	568	0	0	35	16	2	0	0	0	0	0	160	187	82	56	0	59	2	61	High	Med	High	\$25,970
MERAMEC_003749-COLE_W_SB	Forsyth	Carondelet	410	0	0	23	9	2	0	0	0	0	0	178	83	43	34	0	59	1	60	High	Med	High	\$22,660
FORSYTH_002680-COLE_E_SB	Ladue	Parkside	503	0	0	31	5	0	0	82	22	21	4	207	91	51	41	0	59	1	60	High	Low	High	\$30,240
BEMISTON_003730-COLE_W_SB	Forsyth	Carondelet	405	0	0	7	2	1	1	0	0	0	0	190	129	20	35	0	59	1	60	High	Low	High	\$18,200
WESTWOOD_002336-COLE_E_SB	Byron	Parkdale	238	0	0	7	2	1	0	0	0	0	0	92	81	39	0	0	59	1	60	High	Low	High	\$8,100
BEMISTON_003635-COLE_E_SB	Forsyth	Carondelet	401	0	0	8	1	0	0	0	0	0	0	184	36	27	42	0	59	1	60	High	Low	High	\$29,810
BYRON_002352-COLE_N_SB	Westwood	Mid Block	438	0	0	15	4	2	0	99	8	5	0	128	134	13	0	0	59	1	60	High	Low	High	\$7,240
MERAMEC_003799-COLE_W_SB	Carondelet	Bonhome	394	0	0	30	9	1	0	28	7	3	2	144	65	0	0	0	59	1	60	High	Low	High	\$15,250
WESTWOOD_000820-COLE_W_SB	Wydown	Buckingham	838	0	0	33	12	4	4	0	0	0	0	383	75	27	7	0	59	1	60	High	Low	High	\$40,110
BEMISTON_003508-COLE_E_SB	Bonhomme	Shaw Park	425	0	0	9	4	0	0	0	0	0	0	176	150	12	0	0	59	1	60	High	Low	High	\$21,770
CENTRAL_003556-COLE_E_SB	Carondelet	Bonhome	390	0	0	8	2	2	1	0	0	0	0	110	5	16	8	0	59	1	60	High	Low	High	\$18,880
CENTRAL_003765-COLE_E_SB	Forsyth	Carondelet	415	0	0	26	2	0	0	81	0	0	0	210	84	8	0	0	59	1	60	High	Low	High	\$18,870
FORSYTH_002682-COLE_N_SB	Parkside	Brentwood	426	0	0	18	4	0	0	199	0	0	0	209	74	18	0	0	59	1	60	High	Low	High	\$20,280
BEMISTON_003606-COLE_W_SB	Carondelet	Bonhome	389	0	0	8	3	0	0	0	0	0	0	101	75	30	0	0	59	1	60	High	Low	High	\$19,640
BYRON_019769-COLE_S_CW	Westwood	Glen Ridge	950	0	1	25	13	3	1	0	0	0	0	488	84	0	0	0	59	1	60	High	Low	High	\$29,890
FORSYTH_002885-COLE_N_SB	Meramec	Central	332	0	0	4	4	1	0	0	3	2	3	189	42	0	0	0	59	1	60	High	Low	High	\$15,610
WYDOWN_000828-COLE_S_SB	Westwood	Glen Ridge	917	0	0	21	10	6	0	0	0	0	0	344	11	0	0	0	59	1	60	High	Low	High	\$29,330
CARONDELET_003561-COLE_S_SB	Central	Bemiston	328	0	0	4	0	0	0	0	0	0	0	151	84	0	0	0	59	0	59	High	Low	High	\$10,610
CARONDELET_003734-COLE_N_SB	Central	Bemiston	327	0	0	12	1	0	0	0	4	4	0	105	79	0	0	0	59	0	59	High	Low	High	\$10,820
CENTRAL_003772-COLE_W_SB	Forsyth	Carondelet	409	0	0	21	3	0	0	0	3	2	4	200	45	0	0	0	59	0	59	High	Low	High	\$15,510
BEMISTON_003675-COLE_E_SB	Maryland	Forsyth	422	0	0	9	0	0	0	0	4	2	3	166	47	0	0	0	59	0	59	High	Low	High	\$12,460
FORSYTH_003725-COLE_S_SB	Central	Bemiston	335	0	0	9	1	0	0	57	0	0	0	119	38	3	0	0	59	0	59	High	Low	High	\$11,650



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WESTWOOD_002359-COLE_E_SB	Wydown	Byron	240	0	0	11	3	0	0	0	0	0	0	147	0	0	0	0	59	0	59	High	Low	High	\$6,860
BEMISTON_003609-COLE_E_SB	Carondelet	Bonhome	409	0	0	4	0	1	1	0	0	0	0	46	8	0	0	0	59	0	59	High	Low	High	\$7,000
FORSYTH_003666-COLE_S_SB	Bemiston	Hanley	728	0	0	10	4	0	0	0	0	0	0	145	12	29	0	0	59	0	59	High	Low	High	\$13,810
FORSYTH_003684-COLE_N_SB	Central	Bemiston	336	0	0	9	4	0	0	0	0	0	0	94	0	0	0	0	59	0	59	High	Low	High	\$14,170
FORSYTH_003757-COLE_S_SB	Meramec	Central	324	0	0	15	1	0	0	0	0	0	0	17	4	3	0	0	59	0	59	High	Low	High	\$1,690
BONHOMME_003460-COLE_S_SB	Meramec	Central	321	0	0	4	1	0	0	0	0	0	0	12	0	0	0	0	59	0	59	High	Low	High	\$3,650
BONHOMME_003542-COLE_N_SB	Central	Bemiston	319	0	0	5	1	0	0	0	0	0	0	0	0	0	0	0	59	0	59	High	Low	High	\$240
BRIGHTON_016394_COLE_E_CW	Mid Block	Parkside	745	0	4	23	5	2	1	266	6	6	0	311	98	57	22	44	58	1	59	High	Low	High	\$30,100
LYLE_002901-COLE_W_SB	Mid Block	Forsyth	183	0	0	12	6	0	0	63	0	0	0	76	37	8	0	0	58	1	58	High	Low	High	\$9,820
BRENTWOOD2_016485_COLE_E_CW	Pershing	Westmoreland	427	0	2	7	3	0	1	0	0	0	0	246	79	29	0	0	58	1	58	High	Low	High	\$17,050
BRIGHTON_016392_COLE_W_CW	Mid Block	Mid Block	645	0	2	22	8	3	0	135	0	0	1	333	69	10	0	0	58	1	58	High	Low	High	\$21,370
FRANCIS_017105_COLE_W_CW	Whitburn	Clayton	381	1	1	6	8	1	0	139	0	0	0	154	81	9	25	0	32	26	58	Low	High	High	\$20,400
ROSLINE_016969_COLE_S_CW	Brentwood	Meramec	603	0	1	11	5	1	0	0	0	0	0	250	8	8	15	4	58	0	58	High	Low	High	\$22,830
HANLEY_003517-COLE_W_SB	Bonhomme	Shaw Park	59	0	0	7	0	1	0	44	8	4	3	12	12	7	0	0	56	2	58	High	Med	High	\$2,270
SHAW-PARK_003522-COLE_N_SB	Mid Block	Hanley	315	0	0	9	4	2	3	41	5	6	0	104	28	18	14	4	56	1	57	High	Med	High	\$21,950
BONHOMME_003511-COLE_S_SB	Bemiston	Hanley	711	0	0	18	1	1	1	0	0	0	0	283	90	64	76	6	56	1	57	High	Low	High	\$40,500
PERSHING_016492_COLE_N_CW	Brentwood	Meramec	351	0	0	17	4	0	0	0	2	2	2	75	96	49	34	79	55	2	57	High	Med	High	\$16,240
FOREST_002408-COLE_N_SB	Mid Block	Mid Block	36	0	0	0	0	0	0	12	24	0	0	0	0	0	0	0	56	0	56	High	Low	High	\$1,000
PERSHING_016490_COLE_S_CW	Brentwood	Meramec	357	0	0	15	4	0	2	102	9	0	0	81	17	8	21	56	55	2	56	High	Med	High	\$16,860
BRIGHTON_016391_COLE_W_CW	Mid Block	Parkside	120	0	0	2	0	0	0	83	0	0	0	47	39	15	5	7	55	1	56	High	Low	High	\$6,880
LEE_016892_COLE_E_CW	Maryland	Forsyth	489	0	0	17	2	1	1	197	12	7	7	183	56	45	6	3	55	1	55	High	Low	High	\$20,400
BRENTWOOD_017001_COLE_E_CW	Orlando	Venetian	229	0	0	7	3	2	0	0	0	0	0	65	16	9	5	0	55	1	55	High	Low	High	\$6,080
BRENTWOOD_017033_COLE_E_CW	Venetian	Davis	249	0	1	9	1	1	1	0	0	0	0	82	44	4	0	0	55	1	55	High	Low	High	\$9,590
LEE_016885_COLE_W_CW	Maryland	Forsyth	479	0	3	7	5	0	1	0	4	6	0	284	48	19	17	0	55	1	55	High	Low	High	\$19,730
MARYLAND_002780-COLE_N_SB	Meramec	Central	310	0	0	33	8	1	0	281	15	0	0	64	125	49	41	11	53	2	55	High	Med	High	\$16,780
LYLE_003014-COLE_E_SB	Mid Block	Mid Block	89	0	1	5	1	0	0	1	58	0	0	35	0	0	0	0	55	1	55	High	Low	High	\$1,790
BRENTWOOD_016976_COLE_E_CW	Roseline	Orlando	230	0	0	6	1	1	0	0	0	0	0	43	13	10	0	0	55	0	55	High	Low	High	\$7,170
TOPTON_016379_COLE_E_CW	Topton	Brighton	366	0	0	6	1	0	0	0	2	1	5	165	32	0	0	0	55	0	55	High	Low	High	\$6,880
TOPTON_016378_COLE_E_CW	Mid Block	Mark Twain	400	0	3	6	2	1	0	0	0	0	0	145	0	0	0	0	55	0	55	High	Low	High	\$7,360
PARKDALE_003394-COLE_S_SB	Westwood	Mid Block	441	0	0	12	9	1	1	22	2	2	2	221	113	43	12	0	53	1	54	High	Low	Med	\$18,930
WESTWOOD_002419-COLE_E_SB	Parkdale	Buckingham	238	0	0	6	6	1	0	0	0	0	0	93	82	3	0	0	53	1	54	High	Low	Med	\$11,470
CENTRAL_004156-COLE_E_SB	Pershing	Mid Block	464	0	0	11	4	1	1	58	4	2	2	239	86	6	0	0	53	1	54	High	Low	Med	\$27,540
PARKDALE_002329-COLE_N_SB	Westwood	Mid Block	455	0	0	17	4	3	0	0	0	0	0	227	58	15	0	0	53	1	54	High	Low	Med	\$19,610
CENTRAL_004154-COLE_W_SB	Pershing	Mid Block	434	0	0	10	3	1	0	0	0	0	0	284	77	0	0	0	53	1	54	High	Low	Med	\$26,560
BYRON_002351-COLE_N_SB	Mid Block	Glen Ridge	506	0	0	14	3	1	0	0	0	0	0	208	46	4	0	0	53	0	53	High	Low	Med	\$6,470
MERAMEC_016626_COLE_E_CW	Mid Block	Kingsbury	625	0	12	14	1	0	1	43	12	0	0	157	220	55	37	78	52	2	53	High	Med	Med	\$33,150
KINGSBURY_016565_COLE_N_CW	Brentwood	Meramec	334	0	7	9	3	2	0	0	0	0	0	117	34	16	15	0	52	1	52	High	Low	Med	\$6,360
KINGSBURY_016620_COLE_N_CW	Meramec	Central	337	0	5	8	0	2	0	46	9	6	8	212	64	0	0	0	52	1	52	High	Low	Med	\$8,250
DAVIS_017054_COLE_S_CW	Brentwood	Meramec	865	0	5	11	5	1	3	0	0	0	0	510	73	0	0	0	52	1	52	High	Low	Med	\$39,030
DAYTONA_017067_COLE_S_CW	Brentwood	Meramec	958	0	3	22	7	1	1	0	0	0	0	602	136	7	0	0	52	1	52	High	Low	Med	\$45,900



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POLO_017358_COLE_N_CW	Rear	Hanley	326	0	0	4	3	0	0	0	0	0	0	215	68	0	0	0	52	1	52	High	Low	Med	\$11,130
BRENTWOOD_016559_COLE_W_CW	Kingsbury	Pershing	375	0	0	11	0	1	0	0	0	0	0	219	35	0	0	0	52	0	52	High	Low	Med	\$11,130
POLO_017356_COLE_E_CW	Mid Block	Hanley	833	0	9	22	7	1	1	0	0	0	0	370	48	0	0	0	52	0	52	High	Low	Med	\$13,050
SHIRLEY_017388_COLE_N_CW	Hanley	Carswold	254	0	0	12	1	1	0	0	0	0	0	115	13	3	0	0	52	0	52	High	Low	Med	\$7,210
WALINCA_016995_COLE_S_CW	Brentwood	Meramec	579	0	0	15	5	2	0	0	0	0	0	264	27	0	0	0	52	0	52	High	Low	Med	\$19,980
WALINCA_017415_COLE_N_CW	Hanley	Carswold	245	0	3	9	2	0	0	0	0	0	0	112	22	7	0	0	52	0	52	High	Low	Med	\$13,140
ROSEBURY_001206-COLE_N_SB	De Mun	Mid Block	728	0	0	23	2	1	1	0	0	0	0	238	5	0	0	0	52	0	52	High	Low	Med	\$19,090
WALINCA_016915_COLE_N_CW	Brentwood	Meramec	707	0	0	18	0	0	0	0	0	0	0	271	37	5	6	9	52	0	52	High	Low	Med	\$17,820
BRENTWOOD_017058_COLE_E_CW	Davis	Daytona	251	0	0	9	0	0	0	0	0	0	0	91	6	2	4	0	52	0	52	High	Low	Med	\$9,760
MERAMEC_016582_COLE_W_CW	Mid Block	Kingsbury	692	0	16	11	4	0	1	11	7	0	0	166	5	7	0	0	52	0	52	High	Low	Med	\$19,900
DE-MUN_001109-COLE_W_SB	San Bonita	Alamo	300	0	0	5	6	0	0	120	6	2	2	96	9	3	0	0	51	1	51	Med	Low	Med	\$14,790
CENTRAL_004153-COLE_W_SB	Mid Block	Maryland	505	0	0	11	4	3	0	0	0	0	0	164	163	58	0	0	50	1	51	Med	Low	Med	\$42,420
BEMISTON3_016846_COLE_E_CW	Pershing	Maryland	900	0	8	27	10	3	2	102	10	0	0	326	7	0	0	0	50	1	51	Med	Low	Med	\$31,050
CENTRAL_016727_COLE_W_CW	Kingsbury	Pershing	430	0	0	13	4	1	0	0	0	0	0	224	24	0	0	0	50	0	50	Med	Low	Med	\$12,220
WELLINGTON_002215-COLE_N_SB	Westwood	Glen Ridge	957	0	0	35	12	4	5	288	23	4	3	314	44	3	0	0	49	1	50	Med	Low	Med	\$43,740
WESTWOOD_002434-COLE_W_SB	Buckingham	Wellington	545	0	0	14	7	2	1	0	0	0	0	248	58	10	6	8	49	1	50	Med	Low	Med	\$26,570
WESTWOOD_002221-COLE_E_SB	Wellington	York	227	0	0	7	3	0	2	0	0	0	0	30	0	0	0	0	49	1	50	Med	Low	Med	\$4,650
WESTWOOD_002435-COLE_W_SB	Wellington	Clayton	547	0	0	18	2	0	1	0	0	0	0	202	95	17	0	0	49	1	50	Med	Low	Med	\$14,050
BRENTWOOD_017072_COLE_E_CW	Daytona	Watkins	263	0	0	5	0	1	0	0	0	0	0	67	126	32	0	0	49	1	49	Med	Low	Med	\$6,490
PARKSIDE_016458_COLE_E_CW	Brighton	Brentwood	597	0	1	21	4	1	1	276	3	3	2	295	62	2	5	0	49	1	49	Med	Low	Med	\$21,890
SHAW-PARK_017551_COLE_N_CW	Mid Block	Brentwood	189	0	0	3	0	0	0	15	4	13	14	29	9	0	0	0	49	1	49	Med	Low	Med	\$2,220
BRENTWOOD_017557_COLE_W_CW	Shaw Park	Mid Block	173	0	0	4	1	0	0	0	0	0	0	109	9	6	6	0	49	1	49	Med	Low	Med	\$4,500
BRENTWOOD_017562_COLE_W_CW	Mid Block	Orlando	512	0	0	11	4	1	1	0	0	0	0	219	1	3	7	6	49	1	49	Med	Low	Med	\$8,790
WESTMORELAND_016863_COLE_N_CW	Hanley	Mid Block	1101	0	7	26	8	6	1	0	0	0	0	304	42	5	6	0	49	1	49	Med	Low	Med	\$38,550
BRENTWOOD_017531_COLE_W_CW	Forsyth	Bonhome	1036	0	2	31	3	2	0	0	0	2	1	561	129	10	0	0	49	0	49	Med	Low	Med	\$29,440
KINGSBURY_016673_COLE_N_CW	Bemiston	Mid Block	457	0	10	10	1	0	1	0	3	2	7	169	27	5	4	0	49	0	49	Med	Low	Med	\$10,790
ROSILINE_016984_COLE_N_CW	Brentwood	Meramec	595	0	0	12	3	3	0	0	0	0	0	186	43	4	0	0	49	0	49	Med	Low	Med	\$21,170
BRENTWOOD_016277_COLE_W_CW	Westmoreland	Maryland	353	0	0	12	4	0	0	0	0	0	0	92	7	0	0	0	49	0	49	Med	Low	Med	\$7,840
NORTHWOOD_001192-COLE_N_SB	Bland	Mid Block	741	0	0	18	2	0	1	112	0	0	0	360	14	1	3	6	49	0	49	Med	Low	Med	\$28,600
ORLANDO_016948_COLE_N_CW	Brentwood	Meramec	694	0	3	9	2	1	0	0	0	0	0	455	72	0	0	0	49	0	49	Med	Low	Med	\$29,880
ORLANDO_016955_COLE_N_CW	Brentwood	Meramec	652	0	2	7	3	2	1	0	7	0	0	202	32	0	0	0	49	0	49	Med	Low	Med	\$24,830
SHAW-PARK_017552_COLE_N_CW	Shaw Park	Mid Block	866	0	5	20	3	0	1	0	0	0	0	323	44	0	0	0	49	0	49	Med	Low	Med	\$20,470
BRENTWOOD_017532_COLE_W_CW	Bonhomme	Shaw Park	638	0	0	14	1	0	0	0	0	0	0	376	15	5	0	0	49	0	49	Med	Low	Med	\$11,550
BRENTWOOD2_016087_COLE_W_CW	Unversity	Westmoreland	244	0	0	5	1	0	0	0	0	0	0	52	0	0	0	0	49	0	49	Med	Low	Med	\$4,390
CONCORDIA_001063-COLE_E_SB	San Bonita	Alamo	282	0	0	5	4	1	1	0	0	0	0	62	21	15	12	0	48	1	48	Med	Low	Med	\$6,750
DE-MUN_001170-COLE_W_SB	San Boninta	San Bonita	161	0	0	4	1	1	0	109	38	0	2	38	27	0	0	0	48	1	48	Med	Low	Med	\$7,690
DE-MUN_001156-COLE_E_SB	Southwood	San Bonita	127	0	0	1	3	0	0	0	0	0	0	11	15	7	0	0	48	1	48	Med	Low	Med	\$1,060
ROSEBURY_001223-COLE_S_SB	De Mun	Mid Block	757	0	0	25	11	1	0	0	0	0	0	306	65	24	0	0	48	1	48	Med	Low	Med	\$38,480
SOUTHWOOD_001150-COLE_S_SB	De Mun	Mid Block	763	0	0	14	6	2	2	217	0	1	1	216	37	12	3	0	48	1	48	Med	Low	Med	\$30,530
BRENTWOOD_017000_COLE_E_CW	Mid Block	Rosiline	167	0	0	2	0	0	0	0	0	0	0	57	89	20	0	0	47	1	48	Med	Low	Med	\$4,680



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CONCORDIA_001061-COLE_W_SB	San Bonita	Clayton	643	0	0	21	8	1	0	0	0	0	0	164	43	18	5	0	48	0	48	Med	Low	Med	\$38,600
DE-MUN_001215-COLE_E_SB	Rosebury	Rosebury	117	0	0	5	1	0	0	0	0	0	0	62	11	0	0	0	48	0	48	Med	Low	Med	\$2,290
SAN-BONITA_001775-COLE_S_SB	Mid Block	De Mun	417	0	0	40	6	0	0	60	0	0	0	49	14	6	0	0	48	0	48	Med	Low	Med	\$5,390
SEMINARY_001003-COLE_W_SB	San Bonita	Clayton	672	0	0	6	8	1	0	0	0	0	0	261	63	0	0	0	48	0	48	Med	Low	Med	\$14,600
SOUTHWOOD_001148-COLE_N_SB	De Mun	Mid Block	791	0	0	26	8	1	1	148	4	4	3	253	7	0	0	0	48	0	48	Med	Low	Med	\$35,500
ST-RITA_001021-COLE_E_SB	San Bonita	Alamo	441	0	0	11	2	1	0	316	0	0	0	119	12	0	0	0	48	0	48	Med	Low	Med	\$15,960
WESTMORELAND_016480_COLE_N_CW	Brentwood	Meramec	347	0	2	9	3	1	0	0	0	0	0	84	171	27	0	0	47	1	48	Med	Low	Med	\$15,270
ALAMO_000995-COLE_N_SB	Seminary	De Mun	667	0	0	27	4	1	0	0	0	0	0	187	4	0	0	0	48	0	48	Med	Low	Med	\$6,580
DE-MUN_001167-COLE_E_SB	San Bonita	San Bonita	171	0	0	6	2	0	0	0	0	0	0	43	15	0	0	0	48	0	48	Med	Low	Med	\$1,920
MERAMEC_016593_COLE_E_CW	Kingsbury	Pershing	730	0	2	19	5	1	2	0	0	0	0	354	96	87	6	0	47	1	48	Med	Low	Med	\$43,200
ROSEBURY_001210-COLE_S_SB	De Mun	Mid Block	662	0	0	21	7	1	0	0	0	0	0	213	20	4	0	0	48	0	48	Med	Low	Med	\$20,460
ROSEBURY_001221-COLE_N_SB	De Mun	Mid Block	623	0	0	21	6	0	1	0	0	0	0	234	5	0	0	0	48	0	48	Med	Low	Med	\$16,030
SEMINARY_001098-COLE_E_SB	San Bonita	Alamo	304	0	0	3	3	0	0	0	0	0	0	159	13	0	0	0	48	0	48	Med	Low	Med	\$7,990
ALAMO_001029-COLE_N_SB	Concordia	St Rita	531	0	0	13	3	1	0	0	0	0	0	112	4	0	0	0	48	0	48	Med	Low	Med	\$6,930
CENTRAL_016645_COLE_E_CW	Mid Block	Kingsbury	556	0	5	11	1	1	0	0	2	11	7	271	147	36	0	0	47	1	48	Med	Low	Med	\$23,970
PERSHING_016693_COLE_S_CW	Bemiston	Mid Block	360	0	5	12	6	1	1	204	0	0	0	149	14	5	3	0	47	1	48	Med	Low	Med	\$11,060
ST-RITA_001068-COLE_W_SB	Mid Block	Stratford	290	0	0	9	3	0	0	0	0	0	0	58	0	0	0	0	48	0	48	Med	Low	Med	\$2,080
BEMISTON_004163-COLE_W_SB	Pershing	Mid Block	376	0	0	11	3	0	0	376	0	0	0	265	29	0	0	0	47	1	48	Med	Low	Med	\$23,910
BRENTWOOD_016469_COLE_E_CW	Mid Block	Maryland	112	0	0	5	0	0	0	0	0	0	0	68	32	8	0	0	47	1	48	Med	Low	Med	\$8,800
BRENTWOOD_016470_COLE_E_CW	Westmoreland	Mid Block	299	0	0	8	3	0	1	0	0	0	0	135	49	12	0	0	47	1	48	Med	Low	Med	\$14,190
CENTRAL_016643_COLE_W_CW	Mid Block	Kingsbury	577	0	5	11	2	1	1	0	0	0	0	248	136	32	0	0	47	1	48	Med	Low	Med	\$26,360
DE-MUN_001130-COLE_E_SB	Rosebury	Southwood	270	0	1	7	1	0	0	0	0	0	0	67	0	0	0	0	48	0	48	Med	Low	Med	\$2,420
MERAMEC_016585_COLE_W_CW	Westmoreland	Maryland	400	0	1	7	1	1	0	0	10	9	0	181	42	15	9	0	47	1	48	Med	Low	Med	\$17,560
MERAMEC_016592_COLE_E_CW	Mid Block	Maryland	551	0	0	15	2	1	0	0	0	0	0	319	76	21	6	10	47	1	48	Med	Low	Med	\$25,070
BEMISTON_004162-COLE_W_SB	Mid Block	Maryland	516	0	0	16	8	2	0	0	0	0	0	322	24	0	0	0	47	1	48	Med	Low	Med	\$26,740
KINGSBURY_016649_COLE_N_CW	Central	Bemiston	350	0	4	11	4	1	0	0	0	0	0	141	37	8	0	0	47	1	48	Med	Low	Med	\$11,740
KINGSBURY_016665_COLE_S_CW	Central	Bemiston	370	0	0	7	2	0	0	335	0	0	0	115	30	4	7	0	47	1	48	Med	Low	Med	\$9,810
CENTRAL_016725_COLE_E_CW	Kingsbury	Pershing	377	0	1	9	4	1	0	0	0	0	0	180	19	10	0	0	47	0	47	Med	Low	Med	\$8,460
PERSHING_016702_COLE_S_CW	Central	Bemiston	345	0	5	5	1	0	0	272	0	0	0	121	21	5	0	0	47	0	47	Med	Low	Med	\$14,190
PERSHING_016717_COLE_N_CW	Central	Bemiston	355	0	3	11	1	0	0	0	0	0	0	135	12	0	0	0	47	0	47	Med	Low	Med	\$10,480
OXFORD_002209-COLE_S_SB	Westwood	Mid Block	578	0	0	14	5	2	0	0	0	0	0	154	254	75	13	0	46	1	47	Med	Low	Med	\$29,840
OXFORD_002263-COLE_N_SB	Westwood	Mid Block	462	0	0	15	7	2	3	0	2	4	4	208	112	30	0	0	46	1	47	Med	Low	Med	\$21,660
WHITBURN_002598-COLE_N_SB	Sudsbury	Francis	642	0	1	15	5	3	0	36	86	6	1	242	181	65	25	61	46	2	47	Med	Med	Med	\$28,970
FRANCIS_017594_COLE_E_CW	Mid Block	Whitburn	582	0	1	17	3	0	4	0	0	0	0	248	173	55	3	65	46	1	47	Med	Med	Med	\$25,350
FRANCIS_017596_COLE_W_CW	Mid Block	Roxburgh	152	0	0	3	0	0	1	0	0	0	0	54	27	25	3	17	46	1	47	Med	Med	Med	\$6,220
ROXBURGH_017603_COLE_N_CW	Sudsbury	Francis	601	0	2	21	5	1	0	0	3	4	0	158	237	54	12	75	46	1	47	Med	Med	Med	\$22,530
WESTWOOD_002310-COLE_E_SB	Buckingham	Oxford	236	0	0	5	2	0	0	0	0	0	0	84	115	20	4	0	46	1	47	Med	Low	Med	\$8,180
NORTHWOOD_001737-COLE_S_SB	De Mun	Mid Block	258	0	0	27	8	3	0	0	0	0	0	144	64	13	0	0	46	1	47	Med	Med	Med	\$19,620
GLEN-RIDGE_019683_COLE_W_CW	Byron	Parkdale	241	0	1	18	9	1	0	0	0	0	0	64	8	0	0	0	46	1	47	Med	Low	Med	\$5,970
OXFORD_003347-COLE_N_SB	Mid Block	Glen Ridge	529	0	0	13	6	4	1	0	0	0	0	326	57	0	0	0	46	1	47	Med	Low	Med	\$24,720



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ROXBURGH_002586-COLE_S_SB	Sudbury	Francis	606	0	0	9	2	1	1	37	10	3	2	262	120	60	19	51	46	1	47	Med	Low	Med	\$21,430
WELLINGTON_003363-COLE_S_SB	Westwood	Mid Block	448	0	0	14	3	1	1	61	28	0	0	204	92	22	0	0	46	1	47	Med	Low	Med	\$24,470
GLEN-RIDGE_002255-COLE_W_SB	York	Cromwell	228	0	0	7	1	2	0	63	0	0	0	110	8	0	0	0	46	1	47	Med	Low	Med	\$6,800
LINDEN_016855_COLE_E_CW	Mid Block	Maryland	828	0	4	36	13	7	6	345	13	9	4	316	1	2	2	7	46	1	47	Med	Low	Med	\$35,090
YORK_003332-COLE_S_SB	Westwood	Mid Block	464	0	0	22	4	0	2	125	25	11	1	136	10	0	0	0	46	1	47	Med	Low	Med	\$19,240
YORK_004056-COLE_N_SB	Mid Block	Glen Ridge	455	0	0	13	6	0	0	444	4	7	0	182	13	7	0	0	46	1	47	Med	Low	Med	\$28,860
YORK_004057-COLE_N_SB	Westwood	Mid Block	473	0	0	15	4	2	1	98	22	6	1	155	27	3	0	0	46	1	47	Med	Low	Med	\$16,140
BUCKINGHAM_002425-COLE_N_SB	Westwood	Mid Block	170	0	0	5	2	0	0	0	0	0	0	81	36	0	0	0	46	1	47	Med	Low	Med	\$5,880
GLEN-RIDGE_002346-COLE_W_SB	Wydown	Byron	237	0	0	8	1	0	0	188	5	2	2	79	13	0	0	0	46	1	47	Med	Low	Med	\$5,110
NORTHWOOD_001735-COLE_S_SB	De Mun	Mid Block	445	0	0	36	15	2	2	0	12	0	0	248	33	0	0	0	46	1	47	Med	Low	Med	\$30,720
WESTWOOD_002437-COLE_E_SB	Oxford	Wellington	227	0	0	6	3	0	1	0	7	0	1	46	9	0	0	0	46	1	47	Med	Low	Med	\$5,010
BUCKINGHAM_000851-COLE_N_SB	Mid Block	Glen Ridge	490	0	0	15	4	2	0	70	0	0	0	123	15	3	0	0	46	0	46	Med	Low	Med	\$20,290
BUCKINGHAM_002426-COLE_N_SB	Mid Block	Mid Block	313	0	0	15	3	0	0	51	0	0	0	108	32	0	0	0	46	0	46	Med	Low	Med	\$12,820
BUCKINGHAM_003385-COLE_S_SB	Westwood	Mid Block	543	0	0	14	5	0	1	0	0	0	0	238	45	5	0	0	46	0	46	Med	Low	Med	\$28,460
OXFORD_002210-COLE_S_SB	Mid Block	Glen Ridge	401	0	0	9	4	1	0	0	0	0	0	162	39	0	0	0	46	0	46	Med	Low	Med	\$20,010
YORK_003333-COLE_S_SB	Mid Block	Glen Ridge	478	0	0	13	10	0	0	0	0	1	2	156	20	0	0	0	46	0	46	Med	Low	Med	\$19,970
BEMISTON_017348_COLE_W_CW	Shaw Park	Shirley	180	0	0	2	0	0	1	63	12	8	0	106	41	0	0	0	46	1	46	Med	Low	Med	\$4,780
CARSWOLD_017421_COLE_W_CW	Shirley	Walınca	620	0	2	18	8	2	1	395	0	0	0	259	102	20	4	0	46	1	46	Med	Low	Med	\$31,320
GLEN-RIDGE_002201-COLE_W_SB	Parkdale	Buckingham	236	0	0	3	0	1	0	0	0	0	0	105	0	0	0	0	46	0	46	Med	Low	Med	\$5,510
WESTWOOD_002237-COLE_E_SB	York	Clayton	263	0	0	11	2	0	0	0	0	0	0	55	8	4	0	0	46	0	46	Med	Low	Med	\$7,970
BUCKINGHAM_003386-COLE_S_SB	Mid Block	Glen Ridge	467	0	0	10	6	0	0	0	0	0	0	85	0	0	0	0	46	0	46	Med	Low	Med	\$20,540
CARSWOLD_017436_COLE_E_CW	Shirley	Edgewood	987	0	3	29	15	2	1	198	17	0	0	318	118	30	7	28	46	1	46	Med	Low	Med	\$41,390
CECIL_001730-COLE_S_SB	University	Mid Block	492	0	0	45	5	3	1	0	0	0	0	210	28	0	0	0	46	1	46	Med	Low	Med	\$21,020
TOPTON_016445_COLE_E_CW	Topton	Brighton	482	0	0	21	3	0	2	0	0	0	0	202	110	16	0	0	46	1	46	Med	Low	Med	\$12,510
WESTMORELAND_016866_COLE_S_CW	Hanley	Mid Block	1100	0	7	17	4	2	1	120	9	4	0	443	345	51	0	0	46	1	46	Med	Low	Med	\$28,020
GLEN-RIDGE_004062-COLE_W_SB	Buckingham	Oxford	237	0	0	7	1	0	0	0	0	0	0	33	0	0	0	0	46	0	46	Med	Low	Med	\$4,070
N-POLO_017334_COLE_N_CW	Mid Block	Mid Block	328	0	1	4	2	0	0	211	0	0	0	186	71	11	0	0	46	1	46	Med	Low	Med	\$10,340
EDGEWOOD_017410_COLE_W_CW	Hanley	Wydown	782	0	5	15	8	2	1	126	6	6	2	298	36	0	0	0	46	1	46	Med	Low	Med	\$31,150
MARYLAND_016871_COLE_N_CW	Hanley	Jackson	1280	1	9	44	14	1	2	87	0	0	0	717	157	6	1	4	46	1	46	Med	Low	Med	\$39,410
MERAMEC_016937_COLE_E_CW	Mid Block	Orlando	489	0	1	10	3	1	1	0	2	8	0	329	49	0	0	0	46	1	46	Med	Low	Med	\$23,560
TOPTON_016438_COLE_W_CW	Ladue	Mark Twain	1203	0	3	33	9	5	0	97	16	6	3	475	119	3	0	0	46	1	46	Med	Low	Med	\$36,080
DARTFORD_001297-COLE_W_SB	Wydown	Seminary	1081	0	0	16	7	2	0	97	0	0	0	666	99	5	0	0	46	0	46	Med	Low	Med	\$66,150
GAY_016293_COLE_W_CW	Mid Block	Ladue	439	0	0	14	1	1	0	0	0	0	0	117	81	5	0	0	46	0	46	Med	Low	Med	\$14,290
MARYLAND_016879_COLE_S_CW	Hanley	Lee	686	0	1	23	8	0	1	0	0	0	0	247	36	1	3	0	46	0	46	Med	Low	Med	\$25,190
VENETIAN_017006_COLE_N_CW	Brentwood	Meramec	761	0	5	11	5	1	0	0	0	0	0	433	34	0	0	0	46	0	46	Med	Low	Med	\$31,930
MARYLAND_002811-COLE_N_SB	Central	Bemiston	317	0	0	18	5	0	1	82	23	0	0	53	126	90	18	0	44	2	46	Med	Med	Med	\$17,970
MERAMEC_016990_COLE_W_CW	Walınca	Rosiline	252	0	4	7	0	0	0	0	0	0	0	123	18	0	0	0	46	0	46	Med	Low	Med	\$8,180
VENETIAN_017030_COLE_S_CW	Brentwood	Meramec	770	0	8	9	4	0	0	0	0	0	0	428	40	0	0	0	46	0	46	Med	Low	Med	\$29,470
WESTMORELAND_016274_COLE_S_CW	Forsyth	Brentwood	642	0	0	12	5	1	0	0	0	0	0	181	0	0	0	0	46	0	46	Med	Low	Med	\$7,720
WESTMORELAND2_016100_COLE_N_CW	Forsyth	Brentwood	655	0	2	19	0	0	0	0	0	0	0	153	0	0	0	0	46	0	46	Med	Low	Med	\$13,660



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ALAMO_001780-COLE_S_SB	Seminary	Mid Block	272	0	0	21	7	3	0	0	0	2	2	98	22	0	0	0	45	1	46	Med	Low	Med	\$4,690
ALAMO_001781-COLE_S_SB	Mid Block	De Mun	384	0	0	43	9	2	2	0	3	3	4	108	28	3	0	0	45	1	46	Med	Low	Med	\$6,660
PARKSIDE_016400_COLE_W_CW	Brighton	Forsyth	272	0	1	12	4	3	0	47	11	0	0	80	36	43	15	7	44	2	46	Med	Med	Med	\$8,510
SAN-BONITA_001773-COLE_N_SB	Mid Block	De Mun	417	0	0	42	15	3	0	243	0	0	1	146	0	0	0	0	45	1	46	Med	Low	Med	\$16,750
SAN-BONITA_001083-COLE_S_SB	Mid Block	Concordia	92	0	2	10	1	0	0	0	0	0	0	43	38	0	0	0	45	1	45	Med	Low	Med	\$6,190
ALAMO_003153-COLE_S_SB	Concordia	St Rita	528	0	0	41	12	2	0	0	0	0	0	271	7	0	0	0	45	1	45	Med	Low	Med	\$12,950
CENTRAL_004157-COLE_E_SB	Mid Block	Maryland	426	0	0	8	9	2	0	335	19	10	4	128	137	15	0	0	44	1	45	Med	Low	Med	\$13,110
CONCORDIA_001058-COLE_E_SB	Alamo	Clayton	297	0	0	9	2	0	0	0	0	0	0	107	72	19	0	0	45	1	45	Med	Low	Med	\$20,940
SAN-BONITA_001776-COLE_S_SB	Seminary	Mid Block	376	0	0	38	14	0	0	0	0	0	0	71	13	0	0	0	45	1	45	Med	Low	Med	\$7,430
ST-RITA_001022-COLE_E_SB	Forsyth	Brentwood	157	0	0	6	0	1	1	0	0	0	0	32	0	0	0	0	45	1	45	Med	Low	Med	\$1,480
SHAW-PARK_017555_COLE_W_CW	Shaw Park	Mid Block	183	0	0	8	2	1	0	0	2	2	6	90	18	6	7	5	44	1	45	Med	Low	Med	\$3,890
LYLE_003015-COLE_E_SB	Mid Block	Forsyth	127	0	2	11	4	0	0	0	0	0	0	51	23	0	0	0	44	1	45	Med	Low	Med	\$7,750
FOREST_002411-COLE_E_SB	Wydown	Mid Block	383	0	0	8	4	3	2	0	0	0	0	130	4	12	0	0	44	1	45	Med	Low	Med	\$24,250
BEMISTON_016654_COLE_W_CW	Mid Block	Kingsbury	531	0	5	18	1	3	0	97	0	0	0	234	29	29	5	0	44	1	45	Med	Low	Med	\$21,180
FORSYTH_016410_COLE_S_CW	Mid Block	Parkside	522	0	0	9	3	0	0	0	3	8	0	201	65	51	10	0	44	1	45	Med	Low	Med	\$22,530
KINGSBURY_016671_COLE_S_CW	Bemiston	Mid Block	427	0	1	10	2	2	0	0	0	0	0	205	66	2	5	0	44	1	45	Med	Low	Med	\$10,010
MARYLAND_016895_COLE_S_CW	Lee	Jackson	405	0	0	10	4	2	2	72	0	0	0	131	3	0	0	0	44	1	45	Med	Low	Med	\$7,540
MERAMEC_016583_COLE_W_CW	Kingsbury	Pershing	380	0	0	6	3	1	0	0	0	0	0	114	114	11	0	0	44	1	45	Med	Low	Med	\$17,050
WYDOWN_002416-COLE_S_SB	Forest	Westwood	226	0	0	10	0	1	0	0	0	0	0	29	17	17	7	0	44	1	45	Med	Low	Med	\$7,390
BRENTWOOD_017574_COLE_W_CW	Corporate Pk	Corporate Pk	818	0	3	20	4	1	1	0	0	0	0	513	82	24	0	0	44	1	45	Med	Low	Med	\$19,250
MERAMEC_016584_COLE_W_CW	Pershing	Westmoreland	425	0	3	9	5	1	0	0	3	4	0	110	66	7	0	0	44	1	45	Med	Low	Med	\$16,450
BEMISTON_016678_COLE_E_CW	Kingsbury	Pershing	488	0	0	12	6	0	0	119	3	1	5	216	33	0	0	0	44	0	44	Med	Low	Med	\$13,570
MARYLAND_016282_COLE_N_CW	Forsyth	Brentwood	589	0	4	10	2	0	0	0	0	3	0	359	87	7	0	0	44	0	44	Med	Low	Med	\$11,530
BEMISTON_016684_COLE_W_CW	Kingsbury	Pershing	468	0	6	12	3	0	0	195	7	0	0	256	15	0	0	0	44	0	44	Med	Low	Med	\$16,000
UNIVERSITY_001432-COLE_W_SB	Ellenwood	Wydown	894	0	0	24	5	3	0	0	0	0	0	411	20	0	0	0	44	0	44	Med	Low	Med	\$28,280
CROMWELL_003235-COLE_N_SB	Clayton	Mid Block	480	0	0	63	18	3	2	0	0	0	0	125	51	2	0	0	43	1	44	Med	Low	Med	\$22,960
CENTRAL_017250_COLE_N_CW	Mid Block	Mid Block	81	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	44	0	44	Med	Low	Med	\$250
CROMWELL_003234-COLE_N_SB	Mid Block	Glen Ridge	328	0	0	17	8	0	0	222	0	0	0	131	59	21	0	0	43	1	44	Med	Low	Med	\$19,200
SAN-BONITA_001085-COLE_N_SB	Mid Block	Concordia	140	0	0	4	2	2	0	0	7	3	2	3	77	55	4	0	42	2	44	Med	Med	Med	\$7,170
CROMWELL_002246-COLE_S_SB	Clayton	Glen Ridge	751	0	2	18	4	4	0	244	20	8	0	264	63	17	0	0	43	1	44	Med	Low	Med	\$17,240
MARYLAND_002845-COLE_N_SB	Linden	Hanley	317	0	1	17	3	0	0	0	0	0	0	88	173	54	0	0	43	1	44	Med	Low	Med	\$9,800
WELLINGTON_003342-COLE_S_SB	Mid Block	Glen Ridge	508	0	0	11	3	3	0	338	25	6	1	221	26	7	0	0	43	1	44	Med	Low	Med	\$22,330
COUNTRY-CLUB_019664_COLE_N_CW	Hanley	Hanley	832	0	5	79	26	2	0	386	14	0	0	492	93	14	7	4	43	1	44	Med	Low	Med	\$25,780
WHITBURN_002456-COLE_S_SB	Clayshire	Mid Block	341	0	0	11	0	0	0	0	0	0	0	114	160	14	17	5	43	1	44	Med	Low	Med	\$13,340
PARKDALE_002328-COLE_N_SB	Mid Block	Glen Ridge	514	0	0	21	6	0	0	0	0	0	0	332	23	0	0	0	43	0	43	Med	Low	Med	\$26,980
PERSHING_016691_COLE_N_CW	Bemiston	Mid Block	358	0	0	8	3	3	2	0	0	0	0	216	45	0	0	0	43	1	43	Med	Low	Med	\$9,960
SUDBURY_002603-COLE_E_SB	Roxburgh	Whitburn	268	0	0	6	0	0	0	0	0	2	3	32	97	72	22	36	42	2	43	Med	Med	Med	\$11,990
CARSWOLD_017394_COLE_W_CW	Edgewood	Shirley	841	0	4	25	11	2	1	64	35	3	0	402	121	33	10	8	43	1	43	Med	Low	Med	\$50,320
DAYTONA_017064_COLE_N_CW	Brentwood	Mid Block	443	0	0	13	5	2	2	153	9	7	5	301	5	0	0	0	43	1	43	Med	Low	Med	\$21,900
GLEN-RIDGE_002254-COLE_W_SB	Cromwell	Clayton	220	0	0	8	1	0	0	0	1	6	0	82	4	0	0	0	43	0	43	Med	Low	Med	\$8,260



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GLEN-RIDGE_002256-COLE_W_SB	Wellington	York	228	0	0	5	5	0	0	0	0	0	0	63	0	0	0	0	43	0	43	Med	Low	Med	\$4,800
GLEN-RIDGE_002257-COLE_W_SB	Oxford	Wellington	228	0	0	7	1	0	0	208	0	0	0	50	0	0	0	0	43	0	43	Med	Low	Med	\$7,920
MARYLAND_019603_COLE_N_CW	Topton	Brighton	590	0	3	16	3	0	1	291	13	0	0	221	108	14	8	30	43	1	43	Med	Low	Med	\$29,160
PARKDALE_003393-COLE_S_SB	Mid Block	Glen Ridge	533	0	0	10	2	0	0	0	0	0	0	330	67	0	0	0	43	0	43	Med	Low	Med	\$24,280
POLO_017330_COLE_W_CW	N Polo	Middle Polo	580	0	4	12	3	2	1	273	3	4	2	184	72	52	5	0	43	1	43	Med	Low	Med	\$24,480
EDGEWOOD_017440_COLE_E_CW	Shirley	Edgewood	774	0	1	23	13	2	0	274	34	7	0	209	25	20	6	28	43	1	43	Med	Low	Med	\$28,420
SHIRLEY_017430_COLE_S_CW	Hanley	Carswold	253	0	2	6	6	1	0	0	0	0	0	93	65	0	0	0	43	1	43	Med	Low	Med	\$8,370
WYDOWN_002375-COLE_N_SB	Forest	Edgewood	342	0	0	10	2	0	2	0	4	4	3	122	46	12	5	0	43	1	43	Med	Low	Med	\$14,230
EDGEWOOD_017403_COLE_E_CW	Oakley	Wydown	438	0	2	12	3	2	1	0	0	0	0	155	31	17	6	0	43	1	43	Med	Low	Med	\$16,280
FRANCIS_002593-COLE_W_SB	Roxburgh	Whitburn	231	0	0	0	0	0	1	0	3	2	2	78	28	5	9	0	43	1	43	Med	Low	Med	\$5,950
UNIVERSITY_001314-COLE_E_SB	Wydown	Aberdeen	463	0	0	15	3	3	0	168	0	0	0	132	18	13	4	0	43	1	43	Med	Low	Med	\$23,990
WALINCA_017494_COLE_N_CW	Mid Block	Hanley	230	0	3	8	4	1	0	0	0	0	0	147	17	0	0	0	43	1	43	Med	Low	Med	\$7,920
CARSWOLD_017463_COLE_S_CW	Edgewood	Shirley	800	0	5	33	5	2	2	0	0	0	0	332	54	0	0	0	43	1	43	Med	Low	Med	\$41,150
CENTRAL_017248_COLE_W_CW	Mid Block	Davis	1026	0	2	12	5	5	2	42	9	3	0	584	21	0	0	0	43	1	43	Med	Low	Med	\$32,030
FORSYTH_016261_COLE_W_CW	Westmoreland	Mid Block	73	0	0	4	0	0	0	0	0	0	0	29	23	0	0	0	43	1	43	Med	Low	Med	\$1,310
LINDEN_016853_COLE_W_CW	Mid Block	Maryland	625	0	3	12	6	3	0	53	81	6	0	227	16	2	0	0	43	1	43	Med	Low	Med	\$17,940
MIDDLE-POLO_017380_COLE_N_CW	Polo	Polo	666	0	5	10	2	0	0	148	3	6	11	319	106	14	0	0	43	1	43	Med	Low	Med	\$16,730
POLO_017360_COLE_E_CW	Rear	Polo	1093	0	14	17	8	2	1	0	16	0	0	480	80	20	7	8	43	1	43	Med	Low	Med	\$27,080
POLO_017362_COLE_N_CW	Polo	Hanley	184	0	3	3	0	0	1	0	0	0	0	73	29	2	0	0	43	1	43	Med	Low	Med	\$6,470
WYDOWN_001405-COLE_N_SB	Shepley	University	331	0	0	11	2	2	0	102	7	0	0	131	0	0	0	0	43	1	43	Med	Low	Med	\$12,700
WYDOWN_002374-COLE_N_SB	Edgewood	Mid Block	59	0	0	6	0	0	0	0	0	0	0	22	11	0	0	0	43	1	43	Med	Low	Med	\$4,140
BRENTWOOD_016116_COLE_W_CW	Pershing	University	249	0	0	4	4	1	0	0	0	0	0	123	0	0	0	0	43	0	43	Med	Low	Med	\$6,110
CECIL_001439-COLE_N_SB	University	Ellenwood	1028	0	1	27	10	2	0	0	0	1	2	718	50	16	0	0	43	0	43	Med	Low	Med	\$44,560
FORSYTH_016033_COLE_E_CW	University	Westmoreland	254	0	2	9	2	0	1	0	0	0	0	73	3	0	0	0	43	0	43	Med	Low	Med	\$5,940
FORSYTH_016260_COLE_W_CW	Pershing	Westmoreland	648	0	0	14	4	3	0	0	0	0	0	304	12	0	0	0	43	0	43	Med	Low	Med	\$16,840
MERAMEC_019637_COLE_W_CW	Rosline	Orlando	401	0	0	15	4	1	0	0	0	0	0	188	6	0	0	0	43	0	43	Med	Low	Med	\$9,150
POLO_017364_COLE_S_CW	Polo	Hanley	155	0	3	0	0	0	0	0	0	0	0	102	33	7	0	0	43	0	43	Med	Low	Med	\$3,900
POLO_017373_COLE_W_CW	Middle Polo	Polo	602	0	3	11	3	0	0	0	0	0	0	288	101	5	0	0	43	0	43	Med	Low	Med	\$19,060
SUDBURY_002567-COLE_S_SB	Mid Block	Clayshire	674	0	1	16	4	1	1	0	0	7	4	218	185	80	48	44	42	1	43	Med	Med	Med	\$38,240
UNIVERSITY_001434-COLE_E_SB	Ellenwood	Cecil	457	0	0	8	1	0	0	385	0	0	0	338	16	0	0	0	43	0	43	Med	Low	Med	\$12,910
WYDOWN_001347-COLE_S_SB	University	De Mun	1126	0	0	29	4	3	2	0	0	0	0	353	38	10	0	0	43	0	43	Med	Low	Med	\$27,390
DAVIS_017038_COLE_N_CW	Brentwood	Meramec	852	0	5	12	3	2	0	0	9	5	0	473	18	0	0	0	43	0	43	Med	Low	Med	\$32,880
DE-MUN_001342-COLE_W_SB	Wydown	Aberdeen	851	0	0	7	2	1	0	0	0	0	0	405	70	0	0	0	43	0	43	Med	Low	Med	\$34,600
POLO_017345_COLE_W_CW	Mid Block	N Polo	326	0	4	2	1	1	0	0	0	0	0	101	25	3	0	0	43	0	43	Med	Low	Med	\$4,010
UNIVERSITY_016120_COLE_N_CW	Forsyth	Brentwood	660	0	2	24	5	0	0	0	0	0	0	307	18	0	0	0	43	0	43	Med	Low	Med	\$23,910
WATKINS_017079_COLE_N_CW	Brentwood	Meramec	1193	0	3	17	0	1	1	0	0	0	0	816	124	0	0	0	43	0	43	Med	Low	Med	\$42,870
JACKSON_016910_COLE_E_CW	Mid Block	Maryland	279	0	0	6	3	0	0	0	0	0	0	103	0	0	0	0	43	0	43	Med	Low	Med	\$3,810
SAN-BONITA_001767-COLE_S_SB	Concordia	St Rita	506	0	0	47	21	2	0	224	18	5	4	203	79	10	5	5	42	1	43	Med	Low	Med	\$24,200
UNIVERSITY_001446-COLE_E_SB	Cecil	Wydown	371	0	0	8	0	0	0	0	0	0	0	274	0	0	0	0	43	0	43	Med	Low	Med	\$13,920
UNIVERSITY2_016041_COLE_N_CW	Forsyth	Brentwood	671	0	4	19	3	2	0	0	0	0	0	169	0	0	0	0	43	0	43	Med	Low	Med	\$17,710



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WYDOWN_001386-COLE_N_SB	Big Bend	Shepley	1176	0	0	48	2	2	0	0	0	0	0	303	36	4	0	0	43	0	43	Med	Low	Med	\$13,530
WYDOWN_001460-COLE_N_SB	University	Mid Block	1096	0	0	24	2	0	1	0	0	0	1	393	0	0	0	0	43	0	43	Med	Low	Med	\$41,490
DARTFORD_001299-COLE_E_SB	Wydown	Aberdeen	449	0	0	7	0	0	0	0	0	0	0	182	0	0	0	0	43	0	43	Med	Low	Med	\$11,330
FORSYTH_016263-COLE_E_CW	Westmoreland	Maryland	270	0	0	6	0	0	0	0	0	0	0	113	0	0	0	0	43	0	43	Med	Low	Med	\$7,740
MARYLAND_016896-COLE_S_CW	Mid Block	Mid Block	27	0	1	0	0	0	0	0	0	0	0	0	9	18	0	0	41	2	43	Med	Med	Med	\$1,000
UNIVERSITY_001312-COLE_W_SB	Wydown	Aberdeen	454	0	0	4	0	1	0	0	0	0	0	69	0	0	0	0	43	0	43	Med	Low	Med	\$5,100
WYDOWN_001382-COLE_S_SB	Dartford	University	765	0	0	5	0	0	0	113	0	0	0	203	3	0	0	0	43	0	43	Med	Low	Med	\$18,100
SEMINARY_001095-COLE_W_SB	Mid Block	San Bonita	132	0	0	4	2	0	1	109	10	6	7	0	0	0	0	0	42	1	43	Med	Low	Med	\$9,190
SAN-BONITA_001770-COLE_N_SB	St Rita	Seminary	635	0	0	66	19	2	2	466	0	0	0	210	0	0	0	0	42	1	42	Med	Low	Med	\$49,680
KINGSBURY_016563-COLE_S_CW	Brentwood	Meramec	364	0	0	9	2	0	0	0	0	0	0	102	63	59	40	23	41	1	42	Med	Med	Med	\$11,840
SAN-BONITA_001771-COLE_N_SB	Seminary	Mid Block	397	0	0	40	17	0	0	0	0	0	0	142	32	5	0	0	42	1	42	Med	Low	Med	\$22,280
FORSYTH_003008-COLE_N_SB	Lyle	Lee	322	0	0	39	4	0	0	79	0	6	2	76	28	18	38	0	41	1	42	Med	Low	Med	\$23,820
SAN-BONITA_001769-COLE_N_SB	Concordia	Mid Block	435	0	0	29	16	1	0	0	0	0	0	224	5	0	0	0	42	1	42	Med	Low	Med	\$26,240
FORSYTH_002928-COLE_N_SB	Hanley	Lyle	271	0	0	18	7	2	0	0	0	0	0	147	6	9	16	0	41	1	42	Med	Low	Med	\$10,030
SUDBURY_017620-COLE_E_CW	Mid Block	Roxburgh	107	0	0	2	0	0	0	0	0	2	1	63	12	1	0	5	42	1	42	Med	Low	Med	\$1,880
KINGSBURY_016614-COLE_S_CW	Meramec	Central	354	0	6	10	4	1	0	0	0	0	0	127	175	18	5	5	41	1	42	Med	Low	Med	\$16,250
SEMINARY_001097-COLE_E_SB	Mid Block	San Bonita	190	0	0	14	3	0	0	0	0	0	0	88	7	0	0	0	42	0	42	Med	Low	Med	\$8,280
CLAYSHIRE_002562-COLE_W_SB	Whitburn	Mid Block	15	0	0	0	0	0	0	0	0	0	0	7	3	0	0	0	42	0	42	Med	Low	Med	\$200
FORSYTH_016265-COLE_W_CW	Mid Block	Maryland	148	0	1	1	1	0	1	0	0	0	0	87	42	0	0	0	41	1	42	Med	Low	Med	\$8,530
JACKSON_016901-COLE_W_CW	Maryland	Forsyth	511	0	1	11	2	0	0	0	6	7	4	198	184	14	18	8	41	1	42	Med	Low	Med	\$17,820
MARYLAND2_016434-COLE_N_CW	Brighton	Forsyth	250	0	1	8	5	0	0	0	0	0	0	130	32	9	15	0	41	1	42	Med	Low	Med	\$5,920
FORSYTH_002930-COLE_S_SB	Hanley	Lyle	520	0	0	28	4	0	0	166	3	4	0	158	98	37	9	0	41	1	42	Med	Low	Med	\$30,180
FORSYTH_016412-COLE_W_CW	Brighton	Forsyth	421	0	4	15	4	2	0	208	13	6	6	109	46	6	0	0	41	1	42	Med	Low	Med	\$21,590
MARYLAND_016367-COLE_S_CW	Topton	Brighton	297	0	0	10	0	0	1	147	13	4	3	88	8	24	2	6	41	1	42	Med	Low	Med	\$7,450
SAN-BONITA_001006-COLE_S_SB	St Rita	Seminary	517	0	0	17	5	0	0	0	0	0	0	64	0	0	0	0	42	0	42	Med	Low	Med	\$7,630
CARONDELET_003020-COLE_N_SB	Lyle	Coronado	476	0	0	13	0	0	0	291	67	16	0	301	88	5	0	0	41	1	42	Med	Low	Med	\$15,720
WALINCA_017496-COLE_S_CW	Mid Block	Hanley	197	0	0	7	3	1	0	0	0	0	0	68	22	6	0	0	41	1	42	Med	Low	Med	\$10,540
GAY_016353-COLE_E_CW	Ladue	Mid Block	674	0	2	17	0	0	0	47	16	3	2	327	53	8	16	27	41	1	42	Med	Low	Med	\$21,330
CARONDELET_003088-COLE_S_SB	Lyle	Coronado	269	0	0	12	0	0	0	0	0	0	0	167	38	0	0	0	41	0	41	Med	Low	Med	\$15,730
CARONDELET_003022-COLE_N_SB	Hanley	Lyle	121	0	0	5	0	0	0	0	0	0	0	39	5	0	0	0	41	0	41	Med	Low	Med	\$3,550
LYLE_002937-COLE_E_SB	Forsyth	Mid Block	127	0	0	10	6	1	0	0	0	2	2	27	7	5	51	7	38	3	41	Med	Med	Med	\$7,830
CARONDELET_003019-COLE_N_SB	Coronado	Mid Block	447	0	2	87	16	6	3	38	8	0	1	162	129	53	21	8	38	2	40	Med	Med	Med	\$19,110
JACKSON_016909-COLE_E_CW	Maryland	Forsyth	420	0	1	12	5	0	2	66	7	0	0	175	96	10	0	0	40	1	40	Med	Low	Med	\$18,030
ARUNDEL_001184-COLE_S_SB	De Mun	Mid Block	463	0	0	11	7	1	1	57	3	3	2	318	63	15	0	0	40	1	40	Med	Low	Med	\$36,860
CECIL_001729-COLE_S_SB	Mid Block	Ellenwood	578	0	0	51	14	2	0	0	0	0	0	250	38	0	0	0	40	1	40	Med	Low	Med	\$30,680
PERSHING_019611-COLE_N_CW	Forsyth	Brentwood	711	0	4	60	13	0	0	124	0	0	0	460	111	12	0	0	40	1	40	Med	Low	Med	\$35,420
BRENTWOOD_017099-COLE_E_CW	Watkins	Whitburn	592	0	5	11	1	1	0	0	0	0	0	200	144	37	11	0	40	1	40	Med	Low	Med	\$20,670
EDGEWOOD_017400-COLE_N_CW	Mid Block	Carswold	619	0	0	3	0	1	1	309	77	53	3	205	15	0	0	0	40	1	40	Med	Low	Med	\$18,910
EDGEWOOD_017402-COLE_E_CW	Carswold	Oakley	1083	0	4	35	10	4	0	0	0	0	0	524	172	26	0	0	40	1	40	Med	Low	Med	\$56,010
ARUNDEL_001254-COLE_S_SB	University	De Mun	1007	0	0	15	13	5	0	70	0	0	0	391	45	0	0	0	40	1	40	Med	Low	Med	\$57,120



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BRENTWOOD_016141_COLE_W_CW	Kingsbury	Pershing	247	0	0	5	2	2	0	0	0	0	0	119	6	0	0	0	40	1	40	Med	Low	Med	\$6,110
DE-MUN_001173-COLE_W_SB	Arundel	Arundel	214	0	0	5	0	2	0	0	0	0	0	94	17	0	0	0	40	1	40	Med	Low	Med	\$6,330
KINGSBURY_016196_COLE_N_CW	Forsyth	Brentwood	881	0	3	12	3	0	0	447	7	3	0	552	158	0	0	0	40	1	40	Med	Low	Med	\$43,100
POLO_017368_COLE_E_CW	Middle Polo	Mid Block	745	0	6	15	4	2	1	26	8	0	0	357	94	3	0	0	40	1	40	Med	Low	Med	\$18,740
RITZ-CARLTON_019563_COLE_S_CW	Coronado	Forsyth	778	0	5	175	35	4	8	91	27	14	9	262	62	9	17	33	38	2	40	Med	Med	Med	\$39,210
ARUNDEL_001250-COLE_N_SB	De Mun	Mid Block	728	0	0	13	10	1	1	59	0	0	0	333	0	0	0	0	40	0	40	Med	Low	Med	\$40,660
BILTMORE_017319_COLE_E_CW	Davis	Clayton	816	0	6	17	5	1	2	0	0	0	0	206	43	9	2	4	40	0	40	Med	Low	Med	\$22,320
CENTRAL_017254_COLE_N_CW	Mid Block	Mid Block	65	0	0	1	0	0	0	0	0	0	0	31	18	0	0	0	40	0	40	Med	Low	Med	\$1,140
DE-MUN_001178-COLE_E_SB	Arundel	Mid Block	149	0	2	3	2	0	0	0	0	1	2	65	11	0	0	0	40	0	40	Med	Low	Med	\$5,900
OLETA_017097_COLE_E_CW	Brentwood	Crescent	601	0	1	16	3	1	0	45	32	0	0	371	67	0	0	0	40	0	40	Med	Low	Med	\$24,500
PERSHING_016113_COLE_S_CW	Forsyth	Brentwood	702	0	1	14	3	2	1	101	4	5	1	250	34	0	0	0	40	0	40	Med	Low	Med	\$27,010
POLO_017329_COLE_W_CW	Middle Polo	Mid Block	632	0	3	9	5	0	1	0	0	0	0	279	55	10	0	0	40	0	40	Med	Low	Med	\$15,260
SHIRLEY_017449_COLE_S_CW	Shirley	Oakley	292	0	0	6	3	0	1	32	0	0	0	91	0	0	0	0	40	0	40	Med	Low	Med	\$10,060
STRATFORD_016192_COLE_S_CW	San Bonita	Private	1116	0	2	27	7	1	2	0	0	0	0	568	104	15	0	0	40	0	40	Med	Low	Med	\$46,750
WYDOWN_001504-COLE_N_SB	De Mun	Ellenwood	614	0	0	39	13	0	0	0	0	0	0	83	0	0	0	0	40	0	40	Med	Low	Med	\$10,380
ARUNDEL_001320-COLE_N_SB	University	De Mun	997	0	0	21	6	4	0	106	6	5	5	158	6	0	0	0	40	0	40	Med	Low	Med	\$33,090
DAVIS_017259_COLE_N_CW	Central	Hanley	1493	0	6	39	5	2	0	57	0	0	0	752	134	5	0	0	40	0	40	Med	Low	Med	\$52,690
DE-MUN_001472-COLE_E_SB	Wydown	Fauquier	593	0	0	19	5	1	0	0	0	0	0	210	18	6	0	0	40	0	40	Med	Low	Med	\$25,500
KINGSBURY_016145_COLE_S_CW	Forsyth	Brentwood	812	0	0	17	1	1	0	0	11	0	0	519	51	0	0	0	40	0	40	Med	Low	Med	\$34,510
MERAMEC_017013_COLE_E_CW	Orlando	Davis	554	0	1	9	5	0	0	0	0	0	0	347	32	0	0	0	40	0	40	Med	Low	Med	\$20,360
POLO_017370_COLE_S_CW	Mid Block	Polo	523	0	2	8	0	0	1	0	0	0	0	300	48	0	0	0	40	0	40	Med	Low	Med	\$14,870
WYDOWN_001383-COLE_S_SB	Big Bend	Dartford	765	0	0	13	0	0	1	0	0	0	0	498	50	0	0	0	40	0	40	Med	Low	Med	\$34,120
BRENTWOOD_016201_COLE_W_CW	Stratford	Kingsbury	247	0	4	6	0	0	0	0	0	0	0	112	16	0	0	0	40	0	40	Med	Low	Med	\$7,090
FORSYTH_016107_COLE_E_CW	Pershing	University	243	0	0	3	1	1	0	0	0	0	0	45	0	0	0	0	40	0	40	Med	Low	Med	\$4,200
MERAMEC_017011_COLE_W_CW	Orlando	Venetian	248	0	0	4	1	0	0	0	0	0	0	69	14	0	0	0	40	0	40	Med	Low	Med	\$5,500
POLO_017327_COLE_S_CW	Mid Block	Polo	995	0	6	17	5	0	0	62	0	0	0	212	29	7	0	0	40	0	40	Med	Low	Med	\$29,880
DE-MUN_001228-COLE_E_SB	Fauquier	Aberdeen	331	0	0	2	2	0	0	0	0	0	0	0	0	0	0	0	40	0	40	Med	Low	Med	\$3,160
DE-MUN_001245-COLE_E_SB	Aberdeen	Arundel	347	0	0	2	1	0	0	0	0	0	0	173	3	0	0	0	40	0	40	Med	Low	Med	\$11,020
DE-MUN_001325-COLE_W_SB	Mid Block	Arundel	55	0	0	1	0	0	0	0	0	0	0	12	0	0	0	0	40	0	40	Med	Low	Med	\$340
POLO_017366_COLE_N_CW	Mid Block	Polo	987	0	3	17	7	0	0	0	0	0	0	178	0	0	0	0	40	0	40	Med	Low	Med	\$11,660
WYDOWN_001615-COLE_S_SB	Mid Block	Forest Ridge	502	0	0	44	14	1	1	420	69	10	2	140	125	16	6	0	38	1	39	Med	Med	Med	\$19,610
DE-MUN_001125-COLE_E_SB	San Bonita	Alamo	201	0	0	4	1	0	0	40	11	5	3	94	25	14	0	0	39	1	39	Med	Low	Med	\$3,900
ALAMO_001785-COLE_N_SB	De Mun	Mid Block	543	0	0	26	9	1	0	0	0	0	0	164	22	8	0	0	39	1	39	Med	Low	Med	\$9,840
SAN-BONITA_000868-COLE_S_SB	De Mun	Mid Block	496	0	0	11	2	3	0	89	0	0	0	102	11	0	0	0	39	0	39	Med	Low	Med	\$5,640
MARYLAND_016307_COLE_N_CW	Lancaster	Topton	509	0	0	11	2	1	1	81	0	0	0	195	79	4	1	5	38	1	39	Med	Low	Low	\$19,460
BRENTWOOD_016575_COLE_E_CW	Clayton Lane	Kingsbury	458	0	0	9	5	1	3	0	0	0	0	144	0	0	0	0	38	1	39	Med	Low	Low	\$9,700
BRENTWOOD_017583_COLE_W_CW	Corporate Pk	Mid Block	557	0	4	19	0	0	0	0	4	5	4	313	151	4	0	3	38	1	39	Med	Low	Low	\$19,910
FORSYTH_002986-COLE_N_SB	Lee	Jackson	277	0	0	18	5	0	0	0	0	0	0	88	28	0	0	0	38	1	39	Med	Low	Low	\$6,070
RITZ-CARLTON_003098-COLE_S_SB	Mid Block	Mid Block	193	0	0	12	3	0	0	0	0	0	0	110	7	0	0	0	38	0	38	Med	Low	Low	\$12,100
WYDOWN_001605-COLE_S_SB	Forest Ridge	Big Bend	156	0	0	5	1	0	0	0	4	6	0	96	3	0	0	0	38	0	38	Med	Low	Low	\$3,790



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LYLE_002935-COLE_W_SB	Forsyth	Mid Block	121	0	0	9	2	0	0	0	0	0	0	18	0	0	0	0	38	0	38	Med	Low	Low	\$840
WYDOWN_003148-COLE_N_SB	Mid Block	De Mun	71	0	0	3	7	0	0	0	0	0	0	0	0	0	0	0	37	1	38	Low	Low	Low	\$3,400
EDGEWOOD_017477_COLE_W_CW	Shirley	Oakley	716	0	4	27	5	4	2	0	0	0	0	317	147	15	10	13	37	1	37	Low	Low	Low	\$28,940
OAKLEY_017482_COLE_E_CW	Shirley	Edgewood	578	0	5	12	10	3	1	217	59	13	0	178	79	18	0	0	37	1	37	Low	Low	Low	\$28,700
SAN-BONITA_001160-COLE_N_SB	De Mun	Mid Block	805	0	0	25	6	2	0	0	0	0	0	102	18	0	0	0	37	0	37	Low	Low	Low	\$22,670
RIDGEMOOR_001711-COLE_S_SB	Mid Block	Mid Block	40	0	0	5	2	1	0	0	0	0	0	24	0	0	0	0	36	2	37	Low	Med	Low	\$870
BILTMORE_017296_COLE_W_CW	Davis	Mohawk	198	0	2	1	1	0	1	0	0	0	0	143	35	3	0	0	37	1	37	Low	Low	Low	\$8,880
WYDOWN_004048-COLE_S_SB	De Mun	Ellenwood	590	0	0	18	12	3	2	0	0	0	3	147	8	0	0	0	37	1	37	Low	Low	Low	\$31,170
ABERDEEN_001232-COLE_N_SB	De Mun	Mid Block	777	0	0	10	4	4	2	65	0	0	0	380	27	4	0	0	37	1	37	Low	Low	Low	\$43,070
CARSWOLD_017396_COLE_N_CW	Mid Block	Mid Block	306	0	0	9	4	1	0	0	0	0	0	117	50	0	0	0	37	1	37	Low	Low	Low	\$17,210
MERAMEC_017181_COLE_E_CW	Watkins	Clayton	912	0	3	17	7	3	1	0	0	0	0	635	89	0	0	0	37	1	37	Low	Low	Low	\$42,230
SHIRLEY_017451_COLE_N_CW	Carswold	Edgewood	643	0	4	24	8	0	2	0	0	0	0	269	50	7	0	0	37	1	37	Low	Low	Low	\$20,930
UNIVERSITY_001315-COLE_E_SB	Aberdeen	Arundel	361	0	0	4	2	0	1	231	0	0	0	264	22	0	0	0	37	1	37	Low	Low	Low	\$12,590
ABERDEEN_001332-COLE_S_SB	University	De Mun	839	0	0	15	8	4	0	177	3	2	6	223	9	0	0	0	37	0	37	Low	Low	Low	\$22,280
ABERDEEN_001337-COLE_N_SB	University	De Mun	1012	0	0	22	7	1	2	223	4	4	4	393	28	0	0	0	37	0	37	Low	Low	Low	\$55,700
ARUNDEL_001185-COLE_S_SB	Mid Block	Mid Block	243	0	0	9	4	0	0	0	0	0	0	165	3	0	0	0	37	0	37	Low	Low	Low	\$23,170
BEMISTON_017274_COLE_W_CW	Davis	Clayton	944	0	2	17	2	1	0	0	0	0	0	561	140	4	0	0	37	0	37	Low	Low	Low	\$40,900
DARTFORD_001300-COLE_E_SB	Aberdeen	Arundel	351	0	0	8	3	1	1	0	0	0	0	59	13	0	0	0	37	0	37	Low	Low	Low	\$5,780
DE-MUN_001326-COLE_W_SB	Aberdeen	Mid Block	256	0	0	7	1	0	1	0	0	0	2	139	0	0	0	0	37	0	37	Low	Low	Low	\$7,520
EDGEWOOD_017456_COLE_W_CW	Carswold	Shirley	417	0	5	9	2	2	0	29	8	0	0	181	17	0	0	0	37	0	37	Low	Low	Low	\$14,020
OLETA_017095_COLE_W_CW	Watkins	Crescent	263	0	3	6	0	0	1	0	0	0	0	162	17	0	0	0	37	0	37	Low	Low	Low	\$9,240
UNIVERSITY_001311-COLE_W_SB	Aberdeen	Arundel	340	0	0	10	1	0	1	0	0	0	0	218	10	0	0	0	37	0	37	Low	Low	Low	\$10,090
ABERDEEN_001234-COLE_S_SB	De Mun	Mid Block	677	0	0	21	6	1	0	199	0	0	0	155	0	0	0	0	37	0	37	Low	Low	Low	\$33,270
DAYTONA_017065_COLE_N_CW	Mid Block	Meramec	494	0	2	7	2	0	0	0	0	0	0	273	35	0	0	0	37	0	37	Low	Low	Low	\$18,410
ABERDEEN_001336-COLE_N_SB	Dartford	University	755	0	0	9	5	1	0	0	0	0	0	162	0	0	0	0	37	0	37	Low	Low	Low	\$14,100
ARUNDEL_001255-COLE_S_SB	Dartford	University	809	0	0	16	6	0	0	0	0	0	0	494	6	0	0	0	37	0	37	Low	Low	Low	\$38,780
BEMISTON_017280_COLE_E_CW	Mohawk	Biltmore	303	0	0	6	2	0	0	0	0	0	0	159	0	0	0	0	37	0	37	Low	Low	Low	\$10,720
BILTMORE_017315_COLE_N_CW	Bemiston	W Biltmore	357	0	0	6	0	0	0	0	0	0	0	228	13	0	0	0	37	0	37	Low	Low	Low	\$12,940
BLAND_003111-COLE_S_SB	Northmoor	Mid Block	292	0	0	43	9	3	1	44	13	68	92	91	26	13	6	0	34	3	37	Low	Med	Low	\$16,750
MERAMEC_017026_COLE_W_CW	Venetian	Davis	249	0	1	4	0	1	0	0	0	0	0	25	0	0	0	0	37	0	37	Low	Low	Low	\$4,150
WATKINS_017089_COLE_S_CW	Oleta	Meramec	780	0	4	9	2	0	0	0	0	0	0	591	37	0	0	0	37	0	37	Low	Low	Low	\$33,890
ARUNDEL_001305-COLE_N_SB	Dartford	University	760	0	0	20	1	0	0	0	0	0	0	130	0	0	0	0	37	0	37	Low	Low	Low	\$13,440
BEMISTON_017279_COLE_E_CW	Biltmore	Clayton	254	0	0	1	1	0	0	0	0	0	0	68	7	0	0	0	37	0	37	Low	Low	Low	\$7,180
BILTMORE_017313_COLE_W_CW	Mohawk	Biltmore	243	0	0	4	1	0	0	0	0	0	0	50	0	0	0	0	37	0	37	Low	Low	Low	\$4,600
BILTMORE_017317_COLE_S_CW	Bemiston	Clayton	694	0	2	11	1	0	0	1	10	0	0	251	9	0	0	0	37	0	37	Low	Low	Low	\$22,480
DARTFORD_001295-COLE_E_SB	Arundel	Mid Block	159	0	0	1	0	0	0	60	0	0	0	0	0	0	0	0	37	0	37	Low	Low	Low	\$4,690
ELLENWOOD_003142-COLE_S_SB	University	Mid Block	504	0	0	38	14	0	1	207	0	0	0	157	11	0	0	0	36	1	36	Low	Low	Low	\$25,280
SAN-BONITA_000869-COLE_S_SB	Mid Block	Mid Block	457	0	0	10	8	3	0	338	5	3	4	178	4	0	0	0	36	1	36	Low	Low	Low	\$23,290
MARYLAND_016305_COLE_N_CW	Gay	Crandon	339	0	0	10	2	2	0	0	0	0	0	131	63	34	25	0	35	1	36	Low	Low	Low	\$12,560
RIDGEMOOR_001708-COLE_N_SB	Broadview	Mid Block	516	0	0	20	9	1	0	178	0	0	0	239	44	0	0	0	36	1	36	Low	Low	Low	\$9,400



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FORSYTH_001540-COLE_S_SB	Olympian	Wallace	447	0	0	19	4	1	0	0	0	0	0	209	56	6	0	0	36	1	36	Low	Low	Low	\$10,010
RIDGEMOOR_001710-COLE_S_SB	Mid Block	Mid Block	110	0	0	2	0	0	0	0	0	0	0	80	30	0	0	0	36	0	36	Low	Low	Low	\$2,530
RIDGEMOOR_001713-COLE_W_SB	Mid Block	Mid Block	593	0	0	27	3	1	1	0	0	0	0	294	16	0	0	0	36	0	36	Low	Low	Low	\$31,140
ALAMO_001786-COLE_N_SB	Mid Block	Mid Block	411	0	0	23	7	0	0	0	0	0	0	45	0	0	0	0	36	0	36	Low	Low	Low	\$2,450
ALAMO_000767-COLE_S_SB	De Mun	Mid Block	1033	0	0	19	13	0	0	0	0	0	0	316	26	0	0	0	36	0	36	Low	Low	Low	\$10,780
ELLENWOOD_003139-COLE_N_SB	University	Mid Block	609	0	0	15	4	0	0	0	0	0	0	93	0	0	0	0	36	0	36	Low	Low	Low	\$9,360
FORSYTH_001545-COLE_N_SB	Olympian	Wallace	408	0	0	5	0	0	0	0	0	0	0	71	0	0	0	0	36	0	36	Low	Low	Low	\$4,150
GAY_016299_COLE_E_CW	Mid Block	Ladue	393	0	4	8	4	0	1	0	4	4	0	161	40	4	0	0	35	1	36	Low	Low	Low	\$22,480
BLAND_003113-COLE_N_SB	Mid Block	Northmoor	357	0	0	9	1	0	0	0	0	0	0	225	58	0	0	0	35	0	35	Low	Low	Low	\$15,650
DAVIS_017265_COLE_S_CW	Biltmore	Hanley	334	0	0	3	1	2	0	0	0	0	0	173	19	0	0	0	35	0	35	Low	Low	Low	\$10,540
MARYLAND_016315_COLE_N_CW	Crandon	Lancaster	535	0	3	17	1	2	0	0	0	0	0	215	18	0	0	0	35	0	35	Low	Low	Low	\$13,000
FORSYTH_001518-COLE_S_SB	Wallace	Mid Block	1040	0	0	79	22	4	1	932	0	0	1	770	175	32	0	0	34	1	35	Low	Low	Low	\$65,050
BLAND_003114-COLE_N_SB	Mid Block	Northmoor	161	0	0	10	3	1	1	0	0	0	0	82	60	11	0	0	34	1	35	Low	Med	Low	\$7,260
BRENTWOOD_017109_COLE_E_CW	Whitburn	Clayton	363	0	0	12	2	2	0	45	14	8	7	84	72	57	86	47	32	3	35	Low	Med	Low	\$23,390
BRENTWOOD_016202_COLE_W_CW	Clayton Lane	Stratford	221	0	1	8	3	3	0	3	0	0	0	98	32	0	0	0	34	1	34	Low	Low	Low	\$6,760
ELLENWOOD_001497-COLE_S_SB	Mid Block	University	131	0	0	4	4	0	0	0	0	0	0	0	0	0	0	0	34	0	34	Low	Low	Low	\$3,320
ST-RITA_001024-COLE_W_SB	Forsyth	Brentwood	289	0	0	2	3	0	1	0	0	0	0	149	0	0	0	0	34	0	34	Low	Low	Low	\$24,650
FORSYTH_001546-COLE_N_SB	Wallace	Mid Block	736	0	0	18	9	0	0	0	0	0	0	78	0	0	0	0	34	0	34	Low	Low	Low	\$3,680
MERAMEC_017048_COLE_W_CW	Daytona	Mid Block	281	0	0	3	1	0	0	77	15	0	0	136	119	0	0	0	34	1	34	Low	Low	Low	\$13,660
SEMINARY_001001-COLE_E_SB	Alamo	Clayton	301	0	0	4	5	0	0	0	0	0	0	30	0	0	0	0	34	0	34	Low	Low	Low	\$9,160
BEMISTON_017283_COLE_W_CW	Central	Bemiston	514	0	0	5	6	2	2	156	37	0	0	156	33	0	0	0	34	1	34	Low	Low	Low	\$15,670
MERAMEC_017046_COLE_E_CW	Daytona	Mid Block	562	0	4	10	2	1	0	0	0	0	0	341	98	14	0	0	34	1	34	Low	Low	Low	\$23,070
NORTHMOOR_003121-COLE_W_SB	Mid Block	Polo	99	0	0	12	1	0	0	0	0	0	0	55	0	0	0	0	34	1	34	Low	Low	Low	\$7,720
BEMISTON_017284_COLE_W_CW	Central	Mid Block	16	0	0	0	0	0	0	0	10	0	0	12	0	0	0	0	34	0	34	Low	Low	Low	\$350
CENTRAL_017209_COLE_W_CW	Davis	Mid Block	561	0	8	10	0	0	0	0	0	0	0	365	158	0	0	0	34	0	34	Low	Low	Low	\$20,000
FORSYTH_016259_COLE_W_CW	Kingsbury	Pershing	275	0	0	4	2	1	1	0	0	0	0	52	0	0	0	0	34	0	34	Low	Low	Low	\$4,730
MERAMEC2_017177_COLE_W_CW	Watkins	Clayton	636	0	1	13	1	0	0	0	0	0	0	285	172	7	0	0	34	0	34	Low	Low	Low	\$27,710
ABERDEEN_001333-COLE_S_SB	Aberdeen	Mid Block	182	0	0	6	2	0	0	0	0	0	0	67	14	0	0	0	34	0	34	Low	Low	Low	\$2,570
FORSYTH2_016129_COLE_E_CW	Kingsbury	Pershing	238	0	0	5	0	0	0	0	0	0	0	80	35	0	0	0	34	0	34	Low	Low	Low	\$5,750
MOHAWK_017304_COLE_N_CW	Bemiston	Biltmore	506	0	2	8	1	1	0	0	0	0	0	298	21	0	0	0	34	0	34	Low	Low	Low	\$15,750
ABERDEEN_001334-COLE_S_SB	Dartford	University	760	0	0	22	4	0	0	0	0	0	0	222	0	0	0	0	34	0	34	Low	Low	Low	\$10,190
CENTRAL_017231_COLE_W_CW	Davis	Mid Block	488	0	1	15	2	0	0	0	0	0	0	190	26	0	0	0	34	0	34	Low	Low	Low	\$14,730
DAVIS_017214_COLE_S_CW	Meramec	Central	244	0	1	2	1	0	0	0	16	3	0	101	0	0	0	0	34	0	34	Low	Low	Low	\$6,720
DAVIS_017216_COLE_N_CW	Meramec	Central	235	0	2	4	0	0	0	0	0	0	0	180	13	0	0	0	34	0	34	Low	Low	Low	\$9,010
DAVIS_017269_COLE_S_CW	Bemiston	Biltmore	676	0	0	10	0	1	1	0	0	0	0	290	0	0	0	0	34	0	34	Low	Low	Low	\$17,790
GAY_016292_COLE_W_CW	Colonial	Mid Block	502	0	2	10	1	0	0	0	4	5	0	193	4	6	4	0	34	0	34	Low	Low	Low	\$9,940
MOHAWK_017306_COLE_S_CW	Bemiston	Biltmore	448	0	3	9	0	1	0	61	0	0	0	211	0	0	0	0	34	0	34	Low	Low	Low	\$14,700
STRATFORD_016218_COLE_N_CW	Private	Clayton	1130	0	1	22	6	1	0	0	4	4	2	373	33	3	0	0	34	0	34	Low	Low	Low	\$36,760
MERAMEC_017049_COLE_W_CW	Davis	Daytona	249	0	3	4	0	0	0	0	0	0	0	80	0	0	0	0	34	0	34	Low	Low	Low	\$6,110
ELLENWOOD_003143-COLE_S_SB	Mid Block	Cecil	400	0	0	39	14	1	0	229	0	0	0	158	39	8	0	0	33	1	34	Low	Low	Low	\$17,360



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FORSYTH_001541-COLE_S_SB	Big Bend	Mid Block	862	0	1	53	12	1	0	0	0	0	0	314	487	40	0	0	33	1	34	Low	Low	Low	\$35,890
BUCKINGHAM_002057-COLE_N_SB	Glen Ridge	Mid Block	516	0	0	14	5	3	0	184	0	0	0	171	135	15	5	0	33	1	33	Low	Low	Low	\$23,380
BYRON_019767_COLE_N_CW	Glen Ridge	Audubon	749	0	1	23	6	10	2	58	0	0	0	133	9	2	4	6	33	1	33	Low	Low	Low	\$18,560
WYDOWN_004049-COLE_S_SB	Wydown	Mid Block	502	0	0	28	4	2	3	0	0	0	0	215	80	8	0	0	33	1	33	Low	Low	Low	\$24,410
ELLENWOOD_001499-COLE_S_SB	Cecil	Wydown	586	0	0	48	18	2	1	0	0	0	0	135	4	0	0	0	33	1	33	Low	Low	Low	\$15,870
CLAVERACH_001651-COLE_S_SB	Crestwood	Broadview	656	0	0	28	10	1	0	305	43	7	2	351	46	0	0	0	33	1	33	Low	Low	Low	\$32,460
BUCKINGHAM_002059-COLE_S_SB	Glen Ridge	Mid Block	344	0	0	5	10	0	1	0	0	0	0	87	3	0	0	0	33	1	33	Low	Low	Low	\$11,900
BYRON_003174-COLE_S_SB	Glen Ridge	Mid Block	362	0	0	26	7	1	0	0	0	0	0	92	0	0	0	0	33	1	33	Low	Low	Low	\$12,410
FRANCIS_017592_COLE_N_CW	Francis	Brentwood	59	0	0	1	0	0	0	0	2	4	2	10	16	9	0	0	32	1	33	Low	Low	Low	\$1,040
GLEN-RIDGE_002064-COLE_E_SB	Parkdale	Buckingham	236	0	0	7	2	1	0	92	0	0	0	154	0	0	0	0	33	1	33	Low	Low	Low	\$7,450
RIDGEMOOR_001714-COLE_W_SB	Ridgemoor	Mid Block	713	0	0	28	5	1	1	0	0	2	2	536	54	0	0	0	33	1	33	Low	Low	Low	\$48,060
RIDGEMOOR_001792-COLE_E_SB	Wydown	Ridgemoor	941	0	0	50	7	2	0	0	0	0	0	725	117	0	0	0	33	1	33	Low	Low	Low	\$55,710
WYDOWN_002194-COLE_S_SB	Glen Ridge	Audubon	674	0	0	20	7	1	1	0	0	0	2	427	45	0	0	0	33	1	33	Low	Low	Low	\$35,520
YORK_001984-COLE_S_SB	Glen Ridge	Mid Block	459	0	0	7	7	1	1	0	0	0	0	170	22	0	0	0	33	1	33	Low	Low	Low	\$17,200
FORSYTH_001543-COLE_N_SB	Big Bend	Olympian	858	0	0	49	7	0	0	0	0	0	0	570	64	9	0	0	33	0	33	Low	Low	Low	\$20,140
GLEN-RIDGE_002199-COLE_E_SB	Wydown	Byron	235	0	0	8	2	1	0	0	0	0	0	129	9	0	0	0	33	0	33	Low	Low	Low	\$9,490
GLEN-RIDGE_002251-COLE_E_SB	York	Cromwell	226	0	0	6	0	0	0	0	0	0	0	172	46	0	0	0	33	0	33	Low	Low	Low	\$8,090
RIDGEMOOR_001791-COLE_E_SB	Ridgemoor	Mid Block	865	0	0	44	5	1	0	0	0	0	0	459	78	7	0	0	33	0	33	Low	Low	Low	\$44,100
BROADVIEW_001679-COLE_W_SB	Clavernach	Clayton	693	0	0	23	4	0	0	89	0	0	0	358	6	0	0	0	33	0	33	Low	Low	Low	\$31,030
BROADVIEW_001681-COLE_E_SB	Clavernach	Mid Block	755	0	0	27	6	0	1	0	0	0	0	269	11	0	0	0	33	0	33	Low	Low	Low	\$36,420
BYRON_003175-COLE_S_SB	Mid Block	Audubon	397	0	0	28	3	1	0	0	0	0	0	78	0	0	0	0	33	0	33	Low	Low	Low	\$18,030
GLEN-RIDGE_002203-COLE_E_SB	Buckingham	Oxford	236	0	0	3	1	0	0	0	0	0	0	188	13	0	0	0	33	0	33	Low	Low	Low	\$10,610
GLEN-RIDGE_002080-COLE_E_SB	Byron	Parkdale	235	0	0	8	2	0	0	0	0	0	0	54	9	0	0	0	33	0	33	Low	Low	Low	\$7,800
WHITBURN_002455-COLE_S_SB	Mid Block	Francis	187	0	0	3	3	0	1	72	0	0	0	42	28	7	0	0	32	1	33	Low	Low	Low	\$2,290
BRENTWOOD_017584_COLE_W_CW	Mid Block	Whitburn	143	0	0	7	2	0	0	0	0	0	0	82	9	0	0	0	32	0	32	Low	Low	Low	\$2,860
FRANCIS_017103_COLE_E_CW	Whitburn	Clayton	531	0	0	18	3	1	0	0	0	0	0	261	71	0	0	0	32	0	32	Low	Low	Low	\$18,780
BRENTWOOD_002450-COLE_W_SB	Whitburn	Clayton	479	0	0	11	2	1	0	87	11	8	2	69	3	0	0	0	32	0	32	Low	Low	Low	\$9,110
BRENTWOOD_016576_COLE_E_CW	Mid Block	Clayton Lane	99	0	2	2	0	0	0	0	0	0	0	41	0	0	0	0	32	0	32	Low	Low	Low	\$1,480
ELLENWOOD_003138-COLE_N_SB	Mid Block	Cecil	583	0	0	17	9	1	1	0	0	0	0	293	63	6	0	0	31	1	32	Low	Low	Low	\$20,430
CRESCENT_017125_COLE_W_CW	Seminole	Clayton	259	0	1	5	3	2	2	0	0	0	0	213	8	0	0	0	31	1	31	Low	Low	Low	\$15,350
ELLENWOOD_003136-COLE_N_SB	Wydown	Mid Block	512	0	0	8	2	0	0	0	0	0	0	252	55	0	0	0	31	0	31	Low	Low	Low	\$20,900
CLAYSHIRE_002462-COLE_E_SB	Whitburn	Mid Block	263	0	0	3	0	0	0	0	0	0	0	235	10	0	0	0	31	0	31	Low	Low	Low	\$5,570
CRESCENT_017123_COLE_N_CW	Oleta	Clayton	1011	0	4	17	6	0	0	0	0	0	0	508	459	16	0	0	31	1	31	Low	Low	Low	\$52,220
ELLENWOOD_003137-COLE_N_SB	Cecil	Wydown	604	0	0	8	1	1	0	0	0	0	0	176	0	0	0	0	31	0	31	Low	Low	Low	\$23,300
OLETA_017137_COLE_E_CW	Seminole	Clayton	253	0	1	6	4	1	1	64	0	0	0	117	0	0	0	0	31	1	31	Low	Low	Low	\$10,140
SHIRLEY_017472_COLE_N_CW	Oakley	Edgewood	259	0	0	6	4	0	1	61	0	0	0	157	8	10	0	0	31	1	31	Low	Low	Low	\$9,200
OLETA_017115_COLE_W_CW	Oleta	Clayton	586	0	3	10	2	0	0	0	0	0	0	313	156	6	3	6	31	1	31	Low	Low	Low	\$17,680
SEMINOLE_017131_COLE_S_CW	Oleta	Crescent	481	0	2	9	3	2	1	0	0	0	0	275	0	0	0	0	31	0	31	Low	Low	Low	\$15,010
CRESCENT_017166_COLE_W_CW	Oleta	Seminole	647	0	2	5	2	0	0	0	0	0	0	326	16	0	0	0	31	0	31	Low	Low	Low	\$19,280
FORSYTH_016159_COLE_E_CW	Stratford	Topton	371	0	0	5	3	0	0	0	0	0	0	183	6	0	0	0	31	0	31	Low	Low	Low	\$17,520



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FORSYTH_016246-COLE_S_CW	Crandon	Topton	812	0	0	17	7	1	0	0	0	0	0	165	0	0	0	0	31	0	31	Low	Low	Low	\$16,650
SEMINOLE_017143-COLE_S_CW	Oleta	Crescent	488	0	0	7	0	0	0	0	0	0	0	309	35	0	0	0	31	0	31	Low	Low	Low	\$15,830
BEMISTON_017281-COLE_E_CW	Davis	Mohawk	236	0	0	3	1	0	0	0	0	0	0	39	0	0	0	0	31	0	31	Low	Low	Low	\$7,260
GLEN-RIDGE_002204-COLE_E_SB	Oxford	Wellington	227	0	0	5	2	1	0	138	0	0	0	123	8	34	6	10	30	1	31	Low	Low	Low	\$10,270
KINGSBURYFORSYTH_016150-COLE_N_CW	Topton	Mid Block	30	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	31	0	31	Low	Low	Low	\$40
OLETA_017172-COLE_E_CW	Oleta	Seminole	276	0	2	4	1	0	0	0	0	0	0	104	0	0	0	0	31	0	31	Low	Low	Low	\$6,600
OXFORD_003348-COLE_N_SB	Glen Ridge	Mid Block	439	0	0	39	10	1	1	252	5	2	2	220	20	3	0	0	30	1	30	Low	Low	Low	\$26,250
YORK_003335-COLE_N_SB	Glen Ridge	Mid Block	522	0	0	42	10	2	1	0	0	0	0	316	113	10	0	0	30	1	30	Low	Low	Low	\$33,210
GLEN-RIDGE_019685-COLE_E_CW	Wellington	York	233	0	0	21	4	0	1	0	0	0	0	135	26	0	0	0	30	1	30	Low	Low	Low	\$7,850
SOMERSET_003202-COLE_N_SB	Mid Block	Audubon	415	0	0	27	12	1	1	0	0	0	0	200	19	0	0	0	30	1	30	Low	Low	Low	\$12,750
AUDUBON_003187-COLE_E_SB	Oxfor	York	541	0	0	50	9	0	0	54	13	8	2	279	46	0	0	0	30	1	30	Low	Low	Low	\$28,260
HILLVALE_001724-COLE_W_SB	Wydown	Mid Block	893	0	0	62	10	2	2	12	0	0	0	610	38	7	0	0	30	1	30	Low	Low	Low	\$42,910
YORK_001987-COLE_N_SB	Mid Block	Audubon	473	0	0	11	8	3	0	65	0	0	0	232	36	0	0	0	30	1	30	Low	Low	Low	\$19,380
AUDUBON_001911-COLE_E_SB	Hillvale	Clayton	651	0	0	14	5	4	2	0	0	0	0	116	0	0	0	0	30	1	30	Low	Low	Low	\$25,120
CRESTWOOD_001811-COLE_W_SB	Hillvale	Clayton	605	0	0	34	8	1	0	0	0	0	0	444	29	0	0	0	30	1	30	Low	Low	Low	\$32,820
CROMWELL_001967-COLE_N_SB	Glen Ridge	Mid Block	509	0	0	14	4	1	1	0	0	0	0	262	72	5	0	0	30	1	30	Low	Low	Low	\$26,610
HARCOURT_001587-COLE_N_SB	Hillvale	Wydown	737	0	0	36	9	0	0	406	51	17	1	311	3	0	0	0	30	1	30	Low	Low	Low	\$28,250
RIDGEMOOR_001717-COLE_S_SB	Hillvale	Clavernach	569	0	0	28	12	1	0	0	0	0	0	409	23	0	0	0	30	1	30	Low	Low	Low	\$31,640
WELLINGTON_001992-COLE_S_SB	Glen Ridge	Mid Block	382	0	0	14	3	3	0	56	0	0	0	70	11	0	0	0	30	1	30	Low	Low	Low	\$15,550
WELLINGTON_002013-COLE_N_SB	Glen Ridge	Audubon	880	0	0	20	5	2	1	51	18	0	0	485	114	0	0	0	30	1	30	Low	Low	Low	\$29,780
AUDUBON_001827-COLE_W_SB	Wellington	York	247	0	0	11	4	1	0	0	0	0	0	52	4	0	0	0	30	0	30	Low	Low	Low	\$4,890
AUDUBON_001930-COLE_W_SB	Cromwell	Somerset	228	0	0	7	0	1	0	0	0	0	0	138	8	0	0	0	30	0	30	Low	Low	Low	\$9,570
BROADVIEW_001646-COLE_W_SB	Hillvale	Clavernach	419	0	0	26	4	1	0	71	18	0	0	126	0	0	0	0	30	0	30	Low	Low	Low	\$10,190
CRESTWOOD_001805-COLE_E_SB	Mid Block	Mid Block	783	0	0	36	5	0	0	56	6	13	1	518	25	0	0	0	30	0	30	Low	Low	Low	\$32,490
CROMWELL_001965-COLE_S_SB	Glen Ridge	Mid Block	241	0	0	5	3	0	0	0	0	0	0	124	29	3	0	0	30	0	30	Low	Low	Low	\$9,820
HARCOURT_003161-COLE_N_SB	Hillvale	Hillvale	257	0	0	15	3	1	0	0	0	0	0	79	0	0	0	0	30	0	30	Low	Low	Low	\$2,960
OXFORD_002030-COLE_S_SB	Glen Ridge	Audubon	813	0	0	26	10	0	1	332	0	0	0	212	50	5	0	0	30	0	30	Low	Low	Low	\$29,590
PARKDALE_002075-COLE_N_SB	Glen Ridge	Audubon	806	0	0	24	9	2	0	175	0	0	1	261	17	0	0	0	30	0	30	Low	Low	Low	\$40,500
PARKDALE_003392-COLE_S_SB	Glen Ridge	Mid Block	430	0	0	11	3	1	0	0	0	0	0	220	54	0	0	0	30	0	30	Low	Low	Low	\$21,700
SOMERSET_001920-COLE_S_SB	Clayton	Audubon	763	0	0	13	12	1	1	0	0	0	1	178	57	0	0	0	30	0	30	Low	Low	Low	\$21,540
CLAVERACH_000801-COLE_N_SB	Crestwood	Broadview	733	0	0	12	2	2	0	0	0	0	0	492	27	0	0	0	30	0	30	Low	Low	Low	\$36,040
CRESTWOOD_001669-COLE_E_SB	Clavernach	Clayton	237	0	0	10	4	0	0	0	0	0	0	36	2	0	0	0	30	0	30	Low	Low	Low	\$7,610
CRESTWOOD_001809-COLE_E_SB	Hillvale	Clavernach	335	0	0	14	1	0	0	0	0	0	0	211	13	0	0	0	30	0	30	Low	Low	Low	\$12,800
AUDUBON_001913-COLE_W_SB	Somerset	Clayton	217	0	0	6	4	0	0	0	0	0	0	0	0	0	0	0	30	0	30	Low	Low	Low	\$3,400
CRESTWOOD_001860-COLE_W_SB	Mid Block	Mid Block	727	0	0	9	3	0	0	0	0	0	0	347	0	0	0	0	30	0	30	Low	Low	Low	\$28,130
YORK_001983-COLE_S_SB	Mid Block	Audubon	539	0	0	16	1	1	0	0	0	0	0	62	6	0	0	0	30	0	30	Low	Low	Low	\$17,220
GLEN-RIDGE_002252-COLE_E_SB	Cromwell	Clayton	213	0	0	8	0	0	0	0	0	0	0	47	0	0	0	0	30	0	30	Low	Low	Low	\$7,370
WYDOWN_001616-COLE_S_SB	Ridgemoor	Mid Block	633	0	0	68	29	4	0	0	0	2	2	343	123	11	0	0	28	1	29	Low	Low	Low	\$20,340
FORSYTH_003128-COLE_S_SB	Mid Block	Chaplin	576	0	0	18	5	0	0	174	33	11	2	286	12	0	0	0	28	0	28	Low	Low	Low	\$37,170
FORSYTH_001548-COLE_N_SB	Mid Block	Chaplin	822	0	0	24	3	0	0	0	0	0	0	117	0	0	0	0	28	0	28	Low	Low	Low	\$4,980



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AUDUBON_003188-COLE_E_SB	Yrok	Hillvale	173	0	0	16	9	1	0	76	0	0	0	127	25	0	0	0	27	1	28	Low	Low	Low	\$10,540
GAY_016289_COLE_W_CW	Mid Block	Colonial	625	0	5	8	2	0	0	0	0	0	0	287	56	6	3	0	28	0	28	Low	Low	Low	\$14,450
AUDUBON_003186-COLE_E_SB	Parkdale	Oxford	552	0	0	47	21	1	2	358	19	3	3	212	52	11	0	0	27	1	28	Low	Low	Low	\$31,710
HILLVALE_001723-COLE_W_SB	Mid Block	Broadview	710	0	0	59	23	6	0	0	0	0	0	387	50	31	3	0	27	1	28	Low	Low	Low	\$43,620
RIDGEMOOR_001579-COLE_W_SB	Mid Block	Mid Block	345	0	0	25	10	3	0	0	0	0	0	179	18	0	0	0	27	1	27	Low	Low	Low	\$18,970
OXFORD_003349-COLE_N_SB	Mid Block	Audubon	367	0	0	29	13	1	0	129	0	0	0	151	33	0	0	0	27	1	27	Low	Low	Low	\$18,920
WELLINGTON_003341-COLE_S_SB	Mid Block	Audubon	493	0	0	39	14	1	0	91	50	22	1	184	27	0	0	0	27	1	27	Low	Low	Low	\$23,510
BUCKINGHAM_002056-COLE_N_SB	Mid Block	Audubon	258	0	0	8	3	0	0	0	0	0	0	134	81	14	0	0	27	1	27	Low	Low	Low	\$14,540
SOMERSET_003201-COLE_N_SB	Clayton	Mid Block	435	0	0	31	13	1	0	101	0	0	0	146	39	0	0	0	27	1	27	Low	Low	Low	\$22,850
AUDUBON_001894-COLE_W_SB	Parkdale	Buckingham	238	0	0	4	3	2	1	0	0	0	2	19	0	0	0	0	27	1	27	Low	Low	Low	\$4,110
CROMWELL_001968-COLE_N_SB	Mid Block	Audubon	491	0	0	11	7	1	1	343	0	0	0	214	7	0	0	0	27	1	27	Low	Low	Low	\$24,660
HILLVALE_001635-COLE_N_SB	Crestwood	Broadview	561	0	0	39	12	0	1	339	19	0	2	196	0	0	0	0	27	1	27	Low	Low	Low	\$32,590
RIDGEMOOR_001578-COLE_W_SB	Harcourt	Mid Block	403	0	0	28	8	2	0	0	0	0	0	155	0	0	0	0	27	1	27	Low	Low	Low	\$11,820
WYDOWN_001617-COLE_S_SB	Hillvale	Ridgemoor	734	0	0	40	10	0	0	0	0	0	0	483	132	0	0	0	27	1	27	Low	Low	Low	\$43,100
WYDOWN_001798-COLE_S_SB	Crestwood	Hillvale	390	0	0	30	3	0	0	47	12	0	0	246	61	0	0	0	27	1	27	Low	Low	Low	\$16,420
AUDUBON_001895-COLE_W_SB	Buckingham	Oxford	236	0	0	5	4	1	0	75	0	0	0	103	0	0	0	0	27	1	27	Low	Low	Low	\$9,850
AUDUBON_001976-COLE_W_SB	York	Cromwell	228	0	0	3	2	0	1	0	0	0	0	86	26	4	0	0	27	1	27	Low	Low	Low	\$9,050
CRESTWOOD_001808-COLE_E_SB	Mid Block	Hillvale	386	0	0	20	5	2	0	0	0	0	0	98	0	0	0	0	27	1	27	Low	Low	Low	\$9,830
HILLVALE_003169-COLE_E_SB	Wydown	Harcourt	409	0	0	24	8	1	0	0	0	0	0	167	0	0	0	0	27	1	27	Low	Low	Low	\$11,970
HILLVALE_001638-COLE_S_SB	Mid Block	Broadview	384	0	0	23	4	0	0	0	44	11	0	167	6	0	0	0	27	0	27	Low	Low	Low	\$16,180
HILLVALE_001819-COLE_N_SB	Audubon	Crestwood	335	0	1	24	6	0	0	0	0	0	0	57	3	0	0	0	27	0	27	Low	Low	Low	\$6,100
AUDUBON_002018-COLE_W_SB	Oxfor	Wellington	238	0	0	7	4	0	0	0	0	0	0	63	0	0	0	0	27	0	27	Low	Low	Low	\$4,840
BUCKINGHAM_003387-COLE_S_SB	Mid Block	Audubon	490	0	0	14	4	1	0	0	0	0	0	123	0	0	0	0	27	0	27	Low	Low	Low	\$15,510
CRESTWOOD_001807-COLE_E_SB	Mid Block	Mid Block	808	0	0	31	4	1	0	0	0	0	0	373	6	0	0	0	27	0	27	Low	Low	Low	\$38,990
CROMWELL_004070-COLE_S_SB	Mid Block	Audubon	597	0	0	14	7	1	0	0	0	0	0	202	10	0	0	0	27	0	27	Low	Low	Low	\$26,580
HILLVALE_001637-COLE_S_SB	Crestwood	Mid Block	303	0	0	13	0	0	0	0	0	0	0	254	27	0	0	0	27	0	27	Low	Low	Low	\$11,320
HILLVALE_001817-COLE_S_SB	Audubon	Crestwood	332	0	0	17	1	0	0	0	0	0	0	212	21	0	0	0	27	0	27	Low	Low	Low	\$10,220
RIDGEMOOR_001716-COLE_S_SB	Broadview	Ridgemoor	400	0	0	17	5	0	0	96	0	0	0	97	16	0	0	0	27	0	27	Low	Low	Low	\$14,930
CRESTWOOD_001859-COLE_W_SB	Mid Block	Hillvale	823	0	0	12	4	1	0	0	0	0	0	245	4	0	0	0	27	0	27	Low	Low	Low	\$40,580
CRESTWOOD_001861-COLE_W_SB	Wydown	Mid Block	359	0	0	4	2	0	0	66	0	0	0	106	4	0	0	0	27	0	27	Low	Low	Low	\$12,840
RIDGEMOOR_001580-COLE_W_SB	Mid Block	Ridgemoor	526	0	0	21	5	0	0	0	0	0	0	56	0	0	0	0	27	0	27	Low	Low	Low	\$11,590
WYDOWN_001866-COLE_S_SB	Audubon	Crestwood	328	0	0	8	2	0	0	0	0	0	0	99	5	0	0	0	27	0	27	Low	Low	Low	\$6,300
EAST_016224_COLE_W_CW	Mid Block	Stratford	110	0	0	8	1	3	0	0	0	0	0	53	0	0	0	0	25	1	26	Low	Low	Low	\$4,680
FORSYTH_001550-COLE_N_SB	Hoyt	Mid Block	316	0	0	8	3	1	0	0	0	0	0	263	13	0	0	0	25	1	26	Low	Low	Low	\$9,680
FORSYTH_003129-COLE_S_SB	Chaplin	Mid Block	604	0	0	27	5	1	0	0	0	0	0	222	67	0	0	0	25	0	25	Low	Low	Low	\$36,970
FORSYTH_001549-COLE_N_SB	Chaplin	Hoyt	265	0	0	9	1	0	0	0	0	0	0	145	0	0	0	0	25	0	25	Low	Low	Low	\$5,250
AUDUBON_003185-COLE_E_SB	Wydown	Parkdale	502	0	0	40	27	5	1	400	52	15	5	222	11	0	0	0	24	2	25	Low	Med	Low	\$28,830
EAST_016222_COLE_E_CW	Mid Block	Stratford	112	0	1	2	1	0	0	0	0	0	0	32	25	0	0	0	25	0	25	Low	Low	Low	\$4,620
STRATFORD_016239_COLE_N_CW	Alamo	Clayton	302	0	1	4	1	1	0	37	8	2	8	49	9	0	0	0	25	0	25	Low	Low	Low	\$5,140
AUDUBON_001890-COLE_W_SB	Byron	Parkdale	247	0	0	5	3	2	1	0	0	1	3	117	17	0	0	0	24	1	24	Low	Low	Low	\$3,750



City of Clayton ADA Transition Plan
Sidewalk Data Summary



1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22	23	24	25	26
PARKDALE_003391-COLE_S_SB	Mid Block	Audubon	395	0	0	8	7	3	0	0	0	0	0	114	16	0	0	0	24	1	24	Low	Low	Low	\$15,620
AUDUBON_001889-COLE_W_SB	Wydown	Byron	243	0	0	5	6	1	0	0	0	0	0	59	17	0	0	0	24	1	24	Low	Low	Low	\$8,580
HILLVALE_001573-COLE_E_SB	Harcourt	Mid Block	496	0	0	18	5	1	0	163	0	0	0	152	0	0	0	0	24	0	24	Low	Low	Low	\$17,860



City of Clayton ADA Transition Plan Curb Ramp Cost Projection Summary



ADA ID	Location/Intersection	Priority	Costs
2005300575	Brentwood & University	High	\$2,000.00
2005300856	Brentwood & University	High	\$2,000.00
2005300857	Brentwood & University	High	\$2,000.00
2005300858	Brentwood & University	High	\$2,000.00
2021800514	Seminary & Alamo	High	\$2,000.00
2021800515	Seminary & Alamo	High	\$2,000.00
2007900689	Maryland & Brighton	High	\$2,000.00
2007100582	Brentwood & Westmoreland	High	\$2,000.00
2017600172	Westwood & Byron	High	\$2,000.00
2017600173	Westwood & Byron	High	\$2,000.00
2010200534	Alamo & Private	High	\$2,000.00
2024500509	Alamo & De Mun	High	\$2,000.00
2024500510	Alamo & De Mun	High	\$2,000.00
2000000153	Forest	High	\$2,000.00
2017500174	Westwood & Parkdale	High	\$2,000.00
2017500177	Westwood & Parkdale	High	\$2,000.00
2010600646	Hanley & Westmoreland	High	\$2,000.00
2022300531	Concordia & Private	High	\$2,000.00
2009800530	Concordia & Private	High	\$2,000.00
2018300181	Westwood & Buckingham	High	\$2,000.00
2014200120	Forest & Wydown	High	\$2,000.00
2014200121	Forest & Wydown	High	\$2,000.00
2014200124	Forest & Wydown	High	\$2,000.00
2014200125	Forest & Wydown	High	\$2,000.00
2017100157	Byron & Glen Ridge	High	\$2,000.00
2017100159	Byron & Glen Ridge	High	\$2,000.00
2017100160	Byron & Glen Ridge	High	\$2,000.00
2017100163	Byron & Glen Ridge	High	\$2,000.00
2019000183	Westwood & Oxford	High	\$2,000.00
2019000185	Westwood & Oxford	High	\$2,000.00
2000300228	Cromwell & Glen Ridge	High	\$2,000.00
2000300229	Cromwell & Glen Ridge	High	\$2,000.00
2000300230	Cromwell & Glen Ridge	High	\$2,000.00
2000300231	Cromwell & Glen Ridge	High	\$2,000.00
2006100220	Glen Ridge & York	High	\$2,000.00
2006100221	Glen Ridge & York	High	\$2,000.00
2006100224	Glen Ridge & York	High	\$2,000.00
2006100225	Glen Ridge & York	High	\$2,000.00
2019700190	Westwood & Wellington	High	\$2,000.00
2019900192	Westwood & York	High	\$2,000.00
2019900195	Westwood & York	High	\$2,000.00
2013400542	Carswold & Walinca	High	\$2,000.00
2004300399	Ellenwood & University	High	\$2,000.00
2004300402	Ellenwood & University	High	\$2,000.00
2005600722	Maryland & Bemiston	High	\$2,000.00
2014900442	Wydown & Dartford	High	\$2,000.00



**City of Clayton ADA Transition Plan
Curb Ramp Cost Projection Summary**



ADA ID	Location/Intersection	Priority	Costs
2013300403	University & Cecil	High	\$2,000.00
2013300404	University & Cecil	High	\$2,000.00
2027900770	Carondelet & Meramec	High	\$2,000.00
2005200593	Forsyth & University	High	\$2,000.00
2005400719	Central & Maryland	High	\$2,000.00
2018100196	Glen Ridge & Buckingham	High	\$2,000.00
2018100197	Glen Ridge & Buckingham	High	\$2,000.00
2018100200	Glen Ridge & Buckingham	High	\$2,000.00
2018100201	Glen Ridge & Buckingham	High	\$2,000.00
2000000649	Westmoreland	High	\$2,000.00
2005700726	Maryland & Linden	High	\$2,000.00
2022000286	Clayton & Ridgemoor	High	\$2,000.00
2022000287	Clayton & Ridgemoor	High	\$2,000.00
2007600694	Brentwood & Forsyth	High	\$2,000.00
2012300353	Brentwood & Rosiline	High	\$2,000.00
2023100793	Brentwood & Shaw Park	High	\$2,000.00
2007700708	Brentwood & Maryland	High	\$2,000.00
2004400552	Hanley & Rear	High	\$2,000.00
2027900772	Carondelet & Meramec	High	\$2,000.00
2030100796	Shaw Park & Meramec	High	\$2,000.00
2007900691	Maryland & Brighton	High	\$2,000.00
2023100795	Brentwood & Shaw Park	High	\$2,000.00
2012100558	Hanley & Shirley	High	\$2,000.00
2021600500	San Bonita & De Mun	High	\$2,000.00
2005600721	Maryland & Bemiston	High	\$2,000.00
2004400557	Hanley & Rear	High	\$2,000.00
2004500556	Hanley & Country Club	High	\$2,000.00
2011600789	Brentwood & Bonhomme	High	\$2,000.00
2011600792	Brentwood & Bonhomme	High	\$2,000.00
2004000785	Bonhomme & Meramec	High	\$2,000.00
2021800517	Seminary & Alamo	High	\$2,000.00
2005700727	Maryland & Linden	High	\$2,000.00
2018400449	Dartford & Arundel	High	\$2,000.00
2018600455	Arundel & University	High	\$2,000.00
2020700275	Crestwood & Hillvale	High	\$2,000.00
2020700276	Crestwood & Hillvale	High	\$2,000.00
2020700277	Crestwood & Hillvale	High	\$2,000.00
2020700278	Crestwood & Hillvale	High	\$2,000.00
2021300284	Crestwood & Claverach	High	\$2,000.00
2025700298	Big Bend & Southmoor	High	\$2,000.00
2025700299	Big Bend & Southmoor	High	\$2,000.00
2029400536	St Rita & Private	High	\$2,000.00
2007600697	Brentwood & Forsyth	High	\$2,000.00
2012300354	Brentwood & Rosiline	High	\$2,000.00
2023100794	Brentwood & Shaw Park	High	\$2,000.00
2000000069	Central	High	\$2,000.00



**City of Clayton ADA Transition Plan
Curb Ramp Cost Projection Summary**



ADA ID	Location/Intersection	Priority	Costs
2000000071	Central	High	\$2,000.00
2012100553	Hanley & Shirley	High	\$2,000.00
2012100554	Hanley & Shirley	High	\$2,000.00
2013600351	Brentwood, Corporate Pk & Orlando	High	\$2,000.00
2005500712	Maryland & Meramec	High	\$2,000.00
2024600505	San Bonita & De Mun	High	\$2,000.00
2005400717	Central & Maryland	High	\$2,000.00
2005400718	Central & Maryland	High	\$2,000.00
2005600724	Maryland & Bemiston	High	\$2,000.00
2005600725	Maryland & Bemiston	High	\$2,000.00
2016600129	Wydown & Westwood	High	\$2,000.00
2004400551	Hanley & Rear	High	\$2,000.00
2006300032	Sudbury & Roxburgh	High	\$2,000.00
2010600645	Hanley & Westmoreland	High	\$2,000.00
2011600790	Brentwood & Bonhomme	High	\$2,000.00
2011600791	Brentwood & Bonhomme	High	\$2,000.00
2000200240	Audubon & Cromwell	High	\$2,000.00
2000200243	Audubon & Cromwell	High	\$2,000.00
2021000236	Audubon & Somerset	High	\$2,000.00
2021000239	Audubon & Somerset	High	\$2,000.00
2016800433	University & Wydown	High	\$2,000.00
2024500508	Alamo & De Mun	High	\$2,000.00
2007600698	Brentwood & Forsyth	High	\$2,000.00
2024300761	Bemiston & Carondelet	High	\$2,000.00
2027000801	Shaw Park & Central	High	\$2,000.00
2010700560	Maryland & Lee	High	\$2,000.00
2003900783	Bonhomme & Central	High	\$2,000.00
2007700709	Brentwood & Maryland	High	\$2,000.00
2007700710	Brentwood & Maryland	High	\$2,000.00
2024400751	Carondelet & Central	High	\$2,000.00
2000000248	Audubon & York	High	\$2,000.00
2011800545	Edgewood & Carswold	High	\$2,000.00
2011800546	Edgewood & Carswold	High	\$2,000.00
2017200267	Audubon & Parkdale	High	\$2,000.00
2017200270	Audubon & Parkdale	High	\$2,000.00
2018200263	Audubon & Buckingham	High	\$2,000.00
2018200266	Audubon & Buckingham	High	\$2,000.00
2018800258	Audubon & Oxford	High	\$2,000.00
2019600256	Audubon & Wellington	High	\$2,000.00
2019800244	Audubon & York	High	\$2,000.00
2019800247	Audubon & York	High	\$2,000.00
2024600502	San Bonita & De Mun	High	\$2,000.00
2024600503	San Bonita & De Mun	High	\$2,000.00
2000900361	Brentwood & Walinca	High	\$2,000.00
2005600720	Maryland & Bemiston	High	\$2,000.00
2005600723	Maryland & Bemiston	High	\$2,000.00



**City of Clayton ADA Transition Plan
Curb Ramp Cost Projection Summary**



ADA ID	Location/Intersection	Priority	Costs
203000847	Corporate Pk & Shaw Park	High	\$2,000.00
2004500555	Hanley & Country Club	High	\$2,000.00
2011100671	Parkside & Brighton	High	\$2,000.00
2004000845	Bonhomme & Meramec	High	\$2,000.00
2010900747	Central & Forsyth	High	\$2,000.00
2021800513	Seminary & Alamo	High	\$2,000.00
2007500654	Topton & Maryland	High	\$2,000.00
2018300182	Westwood & Buckingham	High	\$2,000.00
2017000271	Audubon & Byron	High	\$2,000.00
2017000274	Audubon & Byron	High	\$2,000.00
2007600695	Brentwood & Forsyth	High	\$2,000.00
2010800763	Meramec & Forsyth	High	\$2,000.00
2027000800	Shaw Park & Central	High	\$2,000.00
2013600349	Brentwood, Corporate Pk & Orlando	High	\$2,000.00
2019300034	Whitburn & Clayshire	High	\$2,000.00
2010900744	Central & Forsyth	High	\$2,000.00
2030100804	Shaw Park & Meramec	High	\$2,000.00
2004000787	Bonhomme & Meramec	High	\$2,000.00
2010900743	Central & Forsyth	High	\$2,000.00
2010900746	Central & Forsyth	High	\$2,000.00
2010900749	Central & Forsyth	High	\$2,000.00
2027900771	Carondelet & Meramec	High	\$2,000.00
2004700572	Brentwood & Pershing	High	\$2,000.00
2023100850	Brentwood & Shaw Park	High	\$200.00
2010800766	Meramec & Forsyth	High	\$2,000.00
2022500009	Brentwood & Francis	High	\$2,000.00
2008200049	Brentwood, Corporate Pk & Daytona	High	\$2,000.00
2008700526	San Bonita & St Rita	High	\$2,000.00
2013600346	Brentwood, Corporate Pk & Orlando	High	\$2,000.00
2021400521	San Bonita & Seminary	High	\$2,000.00
2030900820	Jackson & Maryland	High	\$2,000.00
2000000658	Topton	High	\$2,000.00
2007700707	Brentwood & Maryland	High	\$2,000.00
2024400752	Carondelet & Central	High	\$2,000.00
2024400753	Carondelet & Central	High	\$2,000.00
2007100581	Brentwood & Westmoreland	High	\$2,000.00
2000900360	Brentwood & Walinca	High	\$2,000.00
2006300031	Sudbury & Roxburgh	High	\$2,000.00
2008700525	San Bonita & St Rita	High	\$2,000.00
2008900668	Topton & Mark Twain	High	\$2,000.00
2009800527	Concordia & Private	High	\$2,000.00
2013700114	Polo & Polo	High	\$2,000.00
2019300035	Whitburn & Clayshire	High	\$2,000.00
2005100629	Pershing & Bemiston	High	\$2,000.00
2010900745	Central & Forsyth	High	\$2,000.00
2011400810	Carondelet, Lyle & Colorado	High	\$2,000.00



**City of Clayton ADA Transition Plan
Curb Ramp Cost Projection Summary**



ADA ID	Location/Intersection	Priority	Costs
2030100802	Shaw Park & Meramec	High	\$2,000.00
2030100803	Shaw Park & Meramec	High	\$2,000.00
2004800570	Brentwood & Pershing	High	\$2,000.00
2021800516	Seminary & Alamo	High	\$2,000.00
2024700498	De Mun & Southwood	High	\$2,000.00
2005400716	Central & Maryland	High	\$2,000.00
2007500653	Topton & Maryland	High	\$2,000.00
2007500686	Topton & Maryland	High	\$2,000.00
2007500687	Topton & Maryland	High	\$2,000.00
2000000662	Topton	High	\$2,000.00
2010200535	Alamo & Private	High	\$2,000.00
2013000540	Hanley & Walinca	High	\$2,000.00
2016800432	University & Wydown	High	\$2,000.00
2024500506	Alamo & De Mun	High	\$2,000.00
2024500511	Alamo & De Mun	High	\$2,000.00
2004300401	Ellenwood & University	High	\$2,000.00
2010800765	Meramec & Forsyth	High	\$2,000.00
2010800768	Meramec & Forsyth	High	\$2,000.00
2011300777	Brentwood & Carondelet	High	\$2,000.00
2024300758	Bemiston & Carondelet	High	\$2,000.00
2008200339	Brentwood, Corporate Pk & Daytona	High	\$2,000.00
2009100112	Polo & Middle Polo	High	\$2,000.00
2010700561	Maryland & Lee	High	\$2,000.00
2012500116	Polo & N Polo	High	\$2,000.00
2013600347	Brentwood, Corporate Pk & Orlando	High	\$2,000.00
2024400754	Carondelet & Central	Med	\$2,000.00
2024400755	Carondelet & Central	Med	\$2,000.00
2025900798	Bemiston & Shaw Park	Med	\$2,000.00
2007100580	Brentwood & Westmoreland	Med	\$2,000.00
2007400665	Topton & Mark Twain	Med	\$2,000.00
2007400666	Topton & Mark Twain	Med	\$2,000.00
2012700539	Hanley & Rear	Med	\$2,000.00
2021600504	San Bonita & De Mun	Med	\$2,000.00
2024600497	San Bonita & De Mun	Med	\$2,000.00
2024600501	San Bonita & De Mun	Med	\$2,000.00
2000900356	Brentwood & Walinca	Med	\$2,000.00
2030000848	Corporate Pk & Shaw Park	Med	\$2,000.00
2000000664	Topton	Med	\$2,000.00
2000000655	Topton	Med	\$2,000.00
2004000846	Bonhomme & Meramec	Med	\$2,000.00
2004900624	Central & Pershing	Med	\$2,000.00
2005100630	Pershing & Bemiston	Med	\$2,000.00
2007300681	Maryland & Lancaster	Med	\$2,000.00
2022200289	Clayton & Claytonia	Med	\$2,000.00
2027900773	Carondelet & Meramec	Med	\$2,000.00
2004800568	Brentwood & Pershing	Med	\$2,000.00



**City of Clayton ADA Transition Plan
Curb Ramp Cost Projection Summary**



ADA ID	Location/Intersection	Priority	Costs
2024700499	De Mun & Southwood	Med	\$2,000.00
2029600373	Big Bend & Northmoor	Med	\$2,000.00
2003300644	Kingsbury & Central	Med	\$2,000.00
2007500683	Topton & Maryland	Med	\$2,000.00
2031100814	Ritz Carlton & Colorado	Med	\$2,000.00
2000000661	Topton	Med	\$2,000.00
2006200015	Francis & Roxburgh	Med	\$2,000.00
2014700330	Hanley & Polo	Med	\$2,000.00
2016800427	University & Wydown	Med	\$2,000.00
2020100294	Big Bend & Oak Knoll	Med	\$2,000.00
2019200011	Francis & Whitburn	Med	\$2,000.00
2013600350	Brentwood, Corporate Pk & Orlando	Med	\$2,000.00
2013600352	Brentwood, Corporate Pk & Orlando	Med	\$2,000.00
2021400524	San Bonita & Seminary	Med	\$2,000.00
2003900841	Bonhomme & Central	Med	\$450.00
2003900842	Bonhomme & Central	Med	\$450.00
2003900843	Bonhomme & Central	Med	\$450.00
2003900782	Bonhomme & Central	Med	\$650.00
2007000612	Meramec & Westmoreland	Med	\$2,000.00
2001500344	Brentwood & Venetian	Med	\$2,000.00
2012200550	Carswold & Shirley	Med	\$2,000.00
2013000541	Hanley & Walinca	Med	\$2,000.00
2021100292	Big Bend & Tuscany	Med	\$2,000.00
2017600155	Westwood & Byron	Med	\$2,000.00
2000000118	Shirley	Med	\$2,000.00
2004000786	Bonhomme & Meramec	Med	\$2,000.00
2004000788	Bonhomme & Meramec	Med	\$2,000.00
2006100222	Glen Ridge & York	Med	\$2,000.00
2007200584	Brentwood & Westmoreland	Med	\$2,000.00
2010900748	Central & Forsyth	Med	\$2,000.00
2010900750	Central & Forsyth	Med	\$2,000.00
2005300574	Brentwood & University	Med	\$2,000.00
2012500115	Polo & N Polo	Med	\$2,000.00
2003300642	Kingsbury & Central	Med	\$2,000.00
2007900690	Maryland & Brighton	Med	\$2,000.00
2000000288	Ridgemoor	Med	\$2,000.00
2009000106	Polo & Middle Polo	Med	\$2,000.00
2009000109	Polo & Middle Polo	Med	\$2,000.00
2009900529	Concordia & Alamo	Med	\$2,000.00
2024500507	Alamo & De Mun	Med	\$2,000.00
2024500512	Alamo & De Mun	Med	\$2,000.00
2010800762	Meramec & Forsyth	Med	\$450.00
2010800767	Meramec & Forsyth	Med	\$600.00
2008000702	Maryland & Forsyth	Med	\$2,000.00
2008000705	Maryland & Forsyth	Med	\$2,000.00
2010800764	Meramec & Forsyth	Med	\$2,000.00



City of Clayton ADA Transition Plan Curb Ramp Cost Projection Summary



ADA ID	Location/Intersection	Priority	Costs
2011000738	Forsyth & Bemiston	Med	\$2,000.00
2011300774	Brentwood & Carondelet	Med	\$2,000.00
2011300775	Brentwood & Carondelet	Med	\$2,000.00
2011300776	Brentwood & Carondelet	Med	\$2,000.00
2030400363	Hanley & Clayton	Med	\$2,000.00
2000000119	Shirley	Med	\$2,000.00
2008200341	Brentwood, Corporate Pk & Daytona	Med	\$2,000.00
2013600348	Brentwood, Corporate Pk & Orlando	Med	\$2,000.00
2021400520	San Bonita & Seminary	Med	\$2,000.00
2000000656	Topton	Med	\$2,000.00
2019500213	Glen Ridge & Wellington	Med	\$2,000.00
2024400756	Carondelet & Central	Med	\$2,000.00
2012200548	Carswold & Shirley	Med	\$2,000.00
2019000186	Westwood & Oxford	Med	\$2,000.00
2000000660	Topton	Med	\$2,000.00
2015400463	University & Aberdeen	Med	\$2,000.00
2000000657	Topton	Med	\$2,000.00
2000300234	Cromwell & Glen Ridge	Med	\$2,000.00
2005100633	Pershing & Bemiston	Med	\$2,000.00
2006100223	Glen Ridge & York	Med	\$2,000.00
2006900679	Maryland & Crandon	Med	\$2,000.00
2007200579	Brentwood & Westmoreland	Med	\$2,000.00
2007600696	Brentwood & Forsyth	Med	\$2,000.00
2011500811	Carondelet & Ritz Carlton	Med	\$2,000.00
2019900194	Westwood & York	Med	\$2,000.00
2025900797	Bemiston & Shaw Park	Med	\$2,000.00
2030400362	Hanley & Clayton	Med	\$2,000.00
2024800495	De Mun & San Bonita	Med	\$2,000.00
2029600370	Big Bend & Northmoor	Med	\$2,000.00
2029600371	Big Bend & Northmoor	Med	\$2,000.00
2003300641	Kingsbury & Central	Med	\$2,000.00
2023800836	Forsyth, Jackson & Carondelet	Med	\$2,000.00
2023800837	Forsyth, Jackson & Carondelet	Med	\$2,000.00
2000800066	Walinca & Meramec	Med	\$2,000.00
2004700573	Brentwood & Pershing	Med	\$2,000.00
2006200016	Francis & Roxburgh	Med	\$2,000.00
2009900532	Concordia & Alamo	Med	\$2,000.00
2015000329	Polo & Polo	Med	\$2,000.00
2018700102	Davis & Biltmore	Med	\$2,000.00
2021100293	Big Bend & Tuscany	Med	\$2,000.00
2021300285	Crestwood & Claverach	Med	\$2,000.00
2003200638	Kingsbury & Bemiston	Med	\$2,000.00
2011000736	Forsyth & Bemiston	Med	\$2,000.00
2011000742	Forsyth & Bemiston	Med	\$2,000.00
2017500175	Westwood & Parkdale	Med	\$2,000.00
2019200004	Francis & Whitburn	Med	\$2,000.00



City of Clayton ADA Transition Plan
Curb Ramp Cost Projection Summary



ADA ID	Location/Intersection	Priority	Costs
2009100111	Polo & Middle Polo	Med	\$2,000.00
2021400519	San Bonita & Seminary	Med	\$2,000.00
2027600483	De Mun & Northwood	Med	\$2,000.00
2003900844	Bonhomme & Central	Med	\$2,000.00
2005500711	Maryland & Meramec	Med	\$2,000.00
2005500715	Maryland & Meramec	Med	\$2,000.00
2001500345	Brentwood & Venetian	Med	\$2,000.00
2012200549	Carswold & Shirley	Med	\$2,000.00
2016500128	Wydown & Edgewood	Med	\$400.00
2000900357	Brentwood & Walinca	Med	\$2,000.00
2000900359	Brentwood & Walinca	Med	\$2,000.00
2017100161	Byron & Glen Ridge	Med	\$2,000.00
2017600154	Westwood & Byron	Med	\$2,000.00
2009800528	Concordia & Private	Med	\$2,000.00
2011100670	Parkside & Brighton	Med	\$2,000.00
2015400462	University & Aberdeen	Med	\$2,000.00
2016800426	University & Wydown	Med	\$2,000.00
2023400492	De Mun & Rosebury	Med	\$2,000.00
2004900622	Central & Pershing	Med	\$2,000.00
2004900623	Central & Pershing	Med	\$2,000.00
2007300680	Maryland & Lancaster	Med	\$2,000.00
2019700188	Westwood & Wellington	Med	\$2,000.00
2019700191	Westwood & Wellington	Med	\$2,000.00
2000600605	Brentwood & Kingsbury	Med	\$2,000.00
2004800571	Brentwood & Pershing	Med	\$2,000.00
2005300576	Brentwood & University	Med	\$2,000.00
2017400165	Parkdale & Glen Ridge	Med	\$2,000.00
2017400166	Parkdale & Glen Ridge	Med	\$2,000.00
2017400167	Parkdale & Glen Ridge	Med	\$2,000.00
2017400170	Parkdale & Glen Ridge	Med	\$2,000.00
2018100198	Glen Ridge & Buckingham	Med	\$2,000.00
2031100816	Ritz Carlton & Colorado	Med	\$2,000.00
2000000647	Westmoreland	Med	\$2,000.00
2001000024	Francis & Langton	Med	\$2,000.00
2009000107	Polo & Middle Polo	Med	\$2,000.00
2009000108	Polo & Middle Polo	Med	\$2,000.00
2010200533	Alamo & Private	Med	\$2,000.00
2015300044	Brentwood & Watkins	Med	\$2,000.00
2016700435	Wydown & Shepley	Med	\$2,000.00
2016800422	University & Wydown	Med	\$2,000.00
2016800423	University & Wydown	Med	\$2,000.00
2016800428	University & Wydown	Med	\$2,000.00
2016800429	University & Wydown	Med	\$2,000.00
2017700443	University & Wydown	Med	\$2,000.00
2020100295	Big Bend & Oak Knoll	Med	\$2,000.00
2020700279	Crestwood & Hillvale	Med	\$2,000.00



City of Clayton ADA Transition Plan
Curb Ramp Cost Projection Summary



ADA ID	Location/Intersection	Priority	Costs
2011000740	Forsyth & Bemiston	Med	\$450.00
2003200634	Kingsbury & Bemiston	Med	\$2,000.00
2003200635	Kingsbury & Bemiston	Med	\$2,000.00
2003200637	Kingsbury & Bemiston	Med	\$2,000.00
2007600699	Brentwood & Forsyth	Med	\$2,000.00
2008000700	Maryland & Forsyth	Med	\$2,000.00
2017500176	Westwood & Parkdale	Med	\$2,000.00
2024300759	Bemiston & Carondelet	Med	\$2,000.00
2008200048	Brentwood, Corporate Pk & Daytona	Med	\$2,000.00
2008200050	Brentwood, Corporate Pk & Daytona	Med	\$2,000.00
2021400518	San Bonita & Seminary	Med	\$2,000.00
2021400522	San Bonita & Seminary	Med	\$2,000.00
2021700290	Big Bend & Tuscany	Med	\$2,000.00
2030900821	Jackson & Maryland	Med	\$2,000.00
2005100628	Pershing & Bemiston	Med	\$2,000.00
2005500714	Maryland & Meramec	Med	\$2,000.00
2019500216	Glen Ridge & Wellington	Med	\$2,000.00
2000900355	Brentwood & Walinca	Med	\$2,000.00
2006700672	Gay & Ladue & Maryland	Med	\$2,000.00
2016500132	Wydown & Edgewood	Med	\$2,000.00
2019000184	Westwood & Oxford	Med	\$2,000.00
2008900667	Topton & Mark Twain	Med	\$2,000.00
2016900420	Wydown & Wydown	Med	\$2,000.00
2020700280	Crestwood & Hillvale	Med	\$2,000.00
2023400490	De Mun & Rosebury	Med	\$2,000.00
2025000477	De Mun & Arundel	Med	\$2,000.00
2025000479	De Mun & Arundel	Med	\$2,000.00
2005100631	Pershing & Bemiston	Med	\$2,000.00
2004800569	Brentwood & Pershing	Med	\$2,000.00
2005300577	Brentwood & University	Med	\$2,000.00
2009100110	Polo & Middle Polo	Med	\$2,000.00
2013800130	Edgewood & Walinca	Med	\$2,000.00
2024700496	De Mun & Southwood	Med	\$2,000.00
2003300643	Kingsbury & Central	Med	\$2,000.00
2007500682	Topton & Maryland	Med	\$2,000.00
2007500684	Topton & Maryland	Med	\$2,000.00
2015700148	Wydown & Asbury	Med	\$2,000.00
2017400164	Parkdale & Glen Ridge	Med	\$2,000.00
2017400171	Parkdale & Glen Ridge	Med	\$2,000.00
2018100199	Glen Ridge & Buckingham	Med	\$2,000.00
2018300178	Westwood & Buckingham	Med	\$2,000.00
2018300179	Westwood & Buckingham	Med	\$2,000.00
2019500215	Glen Ridge & Wellington	Med	\$2,000.00
2019500218	Glen Ridge & Wellington	Med	\$2,000.00
2008100342	Brentwood & Davis	Med	\$1,600.00
2000000648	Westmoreland	Med	\$2,000.00



City of Clayton ADA Transition Plan
Curb Ramp Cost Projection Summary



ADA ID	Location/Intersection	Priority	Costs
2012200547	Carswold & Shirley	Med	\$2,000.00
2017700444	University & Wydown	Med	\$2,000.00
2018600453	Arundel & University	Med	\$2,000.00
2025100473	De Mun & Aberdeen	Med	\$2,000.00
2011000737	Forsyth & Bemiston	Med	\$600.00
2000000152	Forest	Med	\$2,000.00
2006700677	Gay & Ladue & Maryland	Med	\$2,000.00
2008000701	Maryland & Forsyth	Med	\$2,000.00
2008000704	Maryland & Forsyth	Med	\$2,000.00
2008000706	Maryland & Forsyth	Med	\$2,000.00
2010800769	Meramec & Forsyth	Med	\$2,000.00
2011000739	Forsyth & Bemiston	Med	\$2,000.00
2011000741	Forsyth & Bemiston	Med	\$2,000.00
2024300757	Bemiston & Carondelet	Med	\$2,000.00
2024300760	Bemiston & Carondelet	Med	\$2,000.00
2025800302	Big Bend & Forest Ridge	Med	\$2,000.00
2025800304	Big Bend & Forest Ridge	Med	\$2,000.00
2008200046	Brentwood, Corporate Pk & Daytona	Med	\$2,000.00
2012400117	Shirley & Polo & Rear	Med	\$2,000.00
2021400523	San Bonita & Seminary	Med	\$2,000.00
2021700291	Big Bend & Tuscany	Med	\$2,000.00
2030900822	Jackson & Maryland	Med	\$2,000.00
2004100778	Bonhomme & Bemiston	Med	\$2,000.00
2005500713	Maryland & Meramec	Med	\$2,000.00
2007800692	Forsyth & Parkside	Med	\$2,000.00
2007800693	Forsyth & Parkside	Med	\$2,000.00
2011400807	Carondelet, Lyle & Colorado	Med	\$2,000.00
2002700616	Brentwood & Clayton Lane	Med	\$2,000.00
2012700538	Hanley & Rear	Med	\$2,000.00
2015300045	Brentwood & Watkins	Med	\$2,000.00
2016500126	Wydown & Edgewood	Med	\$400.00
2000900358	Brentwood & Walinca	Med	\$2,000.00
2006700676	Gay & Ladue & Maryland	Med	\$2,000.00
2015700147	Wydown & Asbury	Med	\$2,000.00
2017100156	Byron & Glen Ridge	Med	\$2,000.00
2017100162	Byron & Glen Ridge	Med	\$2,000.00
2017400169	Parkdale & Glen Ridge	Med	\$2,000.00
2015400457	University & Aberdeen	Med	\$2,000.00
2015400459	University & Aberdeen	Med	\$2,000.00
2018700103	Davis & Biltmore	Med	\$2,000.00
2025000478	De Mun & Arundel	Med	\$2,000.00
2000300235	Cromwell & Glen Ridge	Med	\$2,000.00
2006900678	Maryland & Crandon	Med	\$2,000.00
2011400805	Carondelet, Lyle & Colorado	Med	\$2,000.00
2019900193	Westwood & York	Med	\$2,000.00
2009100113	Polo & Middle Polo	Med	\$2,000.00



**City of Clayton ADA Transition Plan
Curb Ramp Cost Projection Summary**



ADA ID	Location/Intersection	Priority	Costs
2021000238	Audubon & Somerset	Med	\$2,000.00
2024800494	De Mun & San Bonita	Med	\$2,000.00
2025300464	De Mun & Wydown	Med	\$2,000.00
2030700537	Bland & Northmoor	Med	\$2,000.00
2017400168	Parkdale & Glen Ridge	Med	\$2,000.00
2018100203	Glen Ridge & Buckingham	Med	\$2,000.00
2018300180	Westwood & Buckingham	Med	\$2,000.00
2023800833	Forsyth, Jackson & Carondelet	Med	\$2,000.00
2023800835	Forsyth, Jackson & Carondelet	Med	\$2,000.00
2008100343	Brentwood & Davis	Med	\$1,800.00
2000700615	Brentwood & Stratford	Med	\$2,000.00
2000800067	Walinca & Meramec	Med	\$2,000.00
2015800104	Polo & Polo	Med	\$2,000.00
2015800105	Polo & Polo	Med	\$2,000.00
2016700434	Wydown & Shepley	Med	\$2,000.00
2018600451	Arundel & University	Med	\$2,000.00
2018600452	Arundel & University	Med	\$2,000.00
2025100468	De Mun & Aberdeen	Med	\$2,000.00
2025700300	Big Bend & Southmoor	Med	\$2,000.00
2026500474	De Mun & Arundel	Med	\$2,000.00
2026500481	De Mun & Arundel	Med	\$2,000.00
2003200636	Kingsbury & Bemiston	Med	\$2,000.00
2003200639	Kingsbury & Bemiston	Med	\$2,000.00
2011000735	Forsyth & Bemiston	Med	\$2,000.00
2013100367	Big Bend & Shepley	Med	\$2,000.00
2018900206	Oxford & Glen Ridge	Med	\$2,000.00
2018900211	Oxford & Glen Ridge	Med	\$2,000.00
2019200014	Francis & Whitburn	Med	\$2,000.00
2022600005	Brentwood & Whitburn	Med	\$2,000.00
2022600012	Brentwood & Whitburn	Med	\$2,000.00
2025800303	Big Bend & Forest Ridge	Med	\$2,000.00
2025800305	Big Bend & Forest Ridge	Med	\$2,000.00
2008200047	Brentwood, Corporate Pk & Daytona	Med	\$2,000.00
2010700559	Maryland & Lee	Med	\$2,000.00
2023300489	De Mun & Rosebury	Med	\$2,000.00
2027700484	De Mun & Arundel	Med	\$2,000.00
2030900819	Jackson & Maryland	Med	\$2,000.00
2004100779	Bonhomme & Bemiston	Med	\$2,000.00
2011500815	Carondelet & Ritz Carlton	Med	\$2,000.00
2019500214	Glen Ridge & Wellington	Med	\$2,000.00
2020500095	Biltmore & Biltmore	Med	\$2,000.00
2021000237	Audubon & Somerset	Med	\$2,000.00
2006700675	Gay & Ladue & Maryland	Med	\$2,000.00
2029100831	Forsyth & Lyle	Med	\$2,000.00
2029100832	Forsyth & Lyle	Med	\$2,000.00
2012900065	Meramec & Rosiline	Med	\$450.00



**City of Clayton ADA Transition Plan
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ADA ID	Location/Intersection	Priority	Costs
200000663	Topton	Med	\$2,000.00
2014800830	Dartford & Aberdeen	Med	\$2,000.00
2014900441	Wydown & Dartford	Med	\$2,000.00
2015400456	University & Aberdeen	Med	\$2,000.00
2016900421	Wydown & Wydown	Med	\$2,000.00
2021300283	Crestwood & Claverach	Med	\$2,000.00
2023400491	De Mun & Rosebury	Med	\$2,000.00
2025000475	De Mun & Arundel	Med	\$2,000.00
2011200823	Forsyth & Lee	Med	\$450.00
2000300232	Cromwell & Glen Ridge	Med	\$2,000.00
2000300233	Cromwell & Glen Ridge	Med	\$2,000.00
2004900625	Central & Pershing	Med	\$2,000.00
2005100626	Pershing & Bemiston	Med	\$2,000.00
2005100627	Pershing & Bemiston	Med	\$2,000.00
2005100632	Pershing & Bemiston	Med	\$2,000.00
2006100227	Glen Ridge & York	Med	\$2,000.00
2011400806	Carondelet, Lyle & Colorado	Med	\$2,000.00
2011400808	Carondelet, Lyle & Colorado	Med	\$2,000.00
2011400809	Carondelet, Lyle & Colorado	Med	\$2,000.00
2026300817	Ritz Carlton & Carondelet	Med	\$2,000.00
2000600606	Brentwood & Kingsbury	Med	\$2,000.00
2008600252	Clayton & Somerset	Med	\$2,000.00
2025300465	De Mun & Wydown	Med	\$2,000.00
2027600487	De Mun & Northwood	Med	\$2,000.00
2019500212	Glen Ridge & Wellington	Med	\$2,000.00
2023800834	Forsyth, Jackson & Carondelet	Med	\$2,000.00
2031100812	Ritz Carlton & Colorado	Med	\$2,000.00
2000000408	Ellenwood	Med	\$2,000.00
2000700614	Brentwood & Stratford	Med	\$2,000.00
2001000023	Francis & Langton	Med	\$2,000.00
2003500607	Kingsbury & Meramec	Med	\$2,000.00
2017000273	Audubon & Byron	Med	\$2,000.00
2017700445	University & Wydown	Med	\$2,000.00
2017700446	University & Wydown	Med	\$2,000.00
2025100470	De Mun & Aberdeen	Med	\$2,000.00
2025100471	De Mun & Aberdeen	Med	\$2,000.00
2008000703	Maryland & Forsyth	Med	\$650.00
2004300400	Ellenwood & University	Med	\$2,000.00
2018900204	Oxford & Glen Ridge	Med	\$2,000.00
2018900208	Oxford & Glen Ridge	Med	\$2,000.00
2019200033	Francis & Whitburn	Med	\$2,000.00
2022500002	Brentwood & Francis	Med	\$2,000.00
2008200340	Brentwood, Corporate Pk & Daytona	Med	\$600.00
2014800828	Dartford & Aberdeen	Med	\$2,000.00
2020000296	Big Bend & Southmoor	Med	\$2,000.00
2025000476	De Mun & Arundel	Med	\$2,000.00



**City of Clayton ADA Transition Plan
Curb Ramp Cost Projection Summary**



ADA ID	Location/Intersection	Priority	Costs
2027700485	De Mun & Arundel	Med	\$2,000.00
2027800416	Wydown & Wydown Terrace	Med	\$2,000.00
2027800418	Wydown & Wydown Terrace	Med	\$2,000.00
2004100780	Bonhomme & Bemiston	Med	\$2,000.00
2004100781	Bonhomme & Bemiston	Med	\$2,000.00
2019500219	Glen Ridge & Wellington	Med	\$2,000.00
2000000493	De Mun	Med	\$2,000.00
2017200269	Audubon & Parkdale	Med	\$2,000.00
2018200265	Audubon & Buckingham	Med	\$2,000.00
2023200849	Shaw Park & Shaw Park	Med	\$2,000.00
2014800827	Dartford & Aberdeen	Med	\$2,000.00
2014800829	Dartford & Aberdeen	Med	\$2,000.00
2015400458	University & Aberdeen	Med	\$2,000.00
2025000480	De Mun & Arundel	Med	\$2,000.00
2013300405	University & Cecil	Med	\$2,000.00
2019700189	Westwood & Wellington	Med	\$2,000.00
2000200242	Audubon & Cromwell	Med	\$2,000.00
2013800131	Edgewood & Walinca	Med	\$2,000.00
2029600372	Big Bend & Northmoor	Med	\$2,000.00
2007500685	Topton & Maryland	Med	\$2,000.00
2000000482	Northwood	Med	\$2,000.00
2003500609	Kingsbury & Meramec	Med	\$2,000.00
2018400447	Dartford & Arundel	Med	\$2,000.00
2018400450	Dartford & Arundel	Med	\$2,000.00
2025100466	De Mun & Aberdeen	Med	\$2,000.00
2025100469	De Mun & Aberdeen	Med	\$2,000.00
2025100472	De Mun & Aberdeen	Med	\$2,000.00
2019200007	Francis & Whitburn	Low	\$2,000.00
2022500008	Brentwood & Francis	Low	\$2,000.00
2022500013	Brentwood & Francis	Low	\$2,000.00
2000000083	Central	Low	\$2,000.00
2020000297	Big Bend & Southmoor	Low	\$2,000.00
2020300043	Crescent & Oleta	Low	\$2,000.00
2027800417	Wydown & Wydown Terrace	Low	\$2,000.00
2006100226	Glen Ridge & York	Low	\$2,000.00
2001300061	Meramec & Orlando	Low	\$2,000.00
2001300063	Meramec & Orlando	Low	\$2,000.00
2006800588	Forsyth & Westmoreland	Low	\$2,000.00
2018800259	Audubon & Oxford	Low	\$2,000.00
2018800260	Audubon & Oxford	Low	\$2,000.00
2018800262	Audubon & Oxford	Low	\$2,000.00
2019600253	Audubon & Wellington	Low	\$2,000.00
2016400133	Wydown & Glen Ridge	Low	\$400.00
2016400134	Wydown & Glen Ridge	Low	\$400.00
2014200122	Forest & Wydown	Low	\$450.00
2013200364	Big Bend & Brentmoor	Low	\$2,000.00



**City of Clayton ADA Transition Plan
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ADA ID	Location/Intersection	Priority	Costs
2013200365	Big Bend & Brentmoor	Low	\$2,000.00
2014200123	Forest & Wydown	Low	\$2,000.00
2017100158	Byron & Glen Ridge	Low	\$2,000.00
2011100669	Parkside & Brighton	Low	\$2,000.00
2015400461	University & Aberdeen	Low	\$2,000.00
2023300488	De Mun & Rosebury	Low	\$2,000.00
2007200583	Brentwood & Westmoreland	Low	\$400.00
2011500818	Carondelet & Ritz Carlton	Low	\$2,000.00
2013300406	University & Cecil	Low	\$2,000.00
2026300813	Ritz Carlton & Carondelet	Low	\$2,000.00
2016100136	Wydown & Brentmoor	Low	\$2,000.00
2020400038	Oleta & Seminole	Low	\$2,000.00
2027400390	Wallace & Forsyth	Low	\$2,000.00
2003300640	Kingsbury & Central	Low	\$650.00
2015700149	Wydown & Asbury	Low	\$2,000.00
2023800838	Forsyth, Jackson & Carondelet	Low	\$2,000.00
2003500608	Kingsbury & Meramec	Low	\$400.00
2018400448	Dartford & Arundel	Low	\$2,000.00
2020700282	Crestwood & Hillvale	Low	\$2,000.00
2013100366	Big Bend & Shepley	Low	\$2,000.00
2018900205	Oxford & Glen Ridge	Low	\$2,000.00
2018900207	Oxford & Glen Ridge	Low	\$2,000.00
2019200010	Francis & Whitburn	Low	\$2,000.00
2029700368	Big Bend & Private	Low	\$2,000.00
2020300040	Crescent & Oleta	Low	\$2,000.00
2027800419	Wydown & Wydown Terrace	Low	\$2,000.00
2007000613	Meramec & Westmoreland	Low	\$2,000.00
2000000249	Audubon & York	Low	\$2,000.00
2002700617	Brentwood & Clayton Lane	Low	\$2,000.00
2019600255	Audubon & Wellington	Low	\$2,000.00
2019800246	Audubon & York	Low	\$2,000.00
2029200825	Forsyth & Lyle	Low	\$650.00
2005000611	Meramec & Pershing	Low	\$2,000.00
2015700150	Wydown & Asbury	Low	\$2,000.00
2029200826	Forsyth & Lyle	Low	\$2,000.00
2011900544	Edgewood & Shirley	Low	\$2,000.00
2018900210	Oxford & Glen Ridge	Low	\$2,000.00
2000000068	Central	Low	\$2,000.00
2005200592	Forsyth & University	Low	\$2,000.00
2000400604	Kingsbury & Brentwood	Low	\$600.00
2018100202	Glen Ridge & Buckingham	Low	\$2,000.00
2019500217	Glen Ridge & Wellington	Low	\$2,000.00
2023800840	Forsyth, Jackson & Carondelet	Low	\$2,000.00
2000000409	Wydown & Ellenwood	Low	\$2,000.00
2020700281	Crestwood & Hillvale	Low	\$2,000.00
2025700301	Big Bend & Southmoor	Low	\$2,000.00



**City of Clayton ADA Transition Plan
Curb Ramp Cost Projection Summary**



ADA ID	Location/Intersection	Priority	Costs
2018900209	Oxford & Glen Ridge	Low	\$2,000.00
2022500001	Brentwood & Francis	Low	\$2,000.00
2029700369	Big Bend & Private	Low	\$2,000.00
2006800586	Forsyth & Westmoreland	Low	\$200.00
2020500094	Biltmore & Biltmore	Low	\$400.00
2006800587	Forsyth & Westmoreland	Low	\$450.00
2001300062	Meramec & Orlando	Low	\$2,000.00
2019100090	Bemiston & Mohawk	Low	\$2,000.00
2019600254	Audubon & Wellington	Low	\$2,000.00
2006700673	Gay & Ladue & Maryland	Low	\$2,000.00
2006700674	Gay & Ladue & Maryland	Low	\$2,000.00
2002000080	Davis & Central	Low	\$2,000.00
2002300092	Bemiston & Biltmore	Low	\$2,000.00
2014100056	Meramec & Davis	Low	\$2,000.00
2015400460	University & Aberdeen	Low	\$2,000.00
2011200824	Forsyth & Lee	Low	\$250.00
2002100037	Crescent & Seminole	Low	\$450.00
2027400387	Wallace & Forsyth	Low	\$600.00
2027400385	Wallace & Forsyth	Low	\$650.00
2000200245	Audubon & Cromwell	Low	\$2,000.00
2005200591	Forsyth & University	Low	\$2,000.00
2000000413	Wydown	Low	\$2,000.00
2018600454	Arundel & University	Low	\$2,000.00
2018700100	Davis & Biltmore	Low	\$2,000.00
2019200003	Francis & Whitburn	Low	\$2,000.00
2022500006	Brentwood & Francis	Low	\$2,000.00
2000000089	Bemiston & Davis	Low	\$2,000.00
2019400099	Biltmore & Mohawk	Low	\$400.00
2018200264	Audubon & Buckingham	Low	\$2,000.00
2018800261	Audubon & Oxford	Low	\$2,000.00
2005000610	Meramec & Pershing	Low	\$400.00
2002000081	Davis & Central	Low	\$450.00
2002300093	Bemiston & Biltmore	Low	\$450.00
2014100058	Meramec & Davis	Low	\$450.00
2014900440	Wydown & Dartford	Low	\$450.00
2001400059	Meramec & Venetian	Low	\$2,000.00
2011900543	Edgewood & Shirley	Low	\$2,000.00
2000000659	Topton	Low	\$2,000.00
2000500594	Forsyth, Topton & Kingsbury	Low	\$2,000.00
2000500596	Forsyth, Topton & Kingsbury	Low	\$2,000.00
2000500601	Forsyth, Topton & Kingsbury	Low	\$2,000.00
2009500146	Wydown & Ridgemoor	Low	\$200.00
2016800425	University & Wydown	Low	\$400.00
2016800430	University & Wydown	Low	\$400.00
2025100467	De Mun & Aberdeen	Low	\$600.00
2000000407	Ellenwood & Cecil	Low	\$2,000.00



City of Clayton ADA Transition Plan
Curb Ramp Cost Projection Summary



ADA ID	Location/Intersection	Priority	Costs
2000000410	Wydown	Low	\$2,000.00
2000000412	Wydown	Low	\$2,000.00
2018700101	Davis & Biltmore	Low	\$2,000.00
2020300041	Crescent & Oleta	Low	\$400.00
2003100652	Gay & Colonial	Low	\$2,000.00
2020300042	Crescent & Oleta	Low	\$2,000.00
2027700486	De Mun & Arundel	Low	\$2,000.00
2000000072	Central	Low	\$2,000.00
2000000250	Audubon & York	Low	\$2,000.00
2000000251	Audubon & York	Low	\$2,000.00
2017200268	Audubon & Parkdale	Low	\$2,000.00
2015200052	Meramec & Daytona	Low	\$2,000.00
2000500597	Forsyth, Topton & Kingsbury	Low	\$2,000.00
2003600562	Forsyth, Brighton & Pershing	Low	\$2,000.00
2027400386	Wallace & Forsyth	Low	\$200.00
2020500096	Biltmore & Biltmore	Low	\$400.00
2020500097	Biltmore & Biltmore	Low	\$400.00
2000000088	Bemiston & Davis	Low	\$450.00
2002100036	Crescent & Seminole	Low	\$450.00
2016100139	Wydown & Brentmoor	Low	\$450.00
2000000650	Gay & Kingsbury	Low	\$2,000.00
2005200589	Forsyth & University	Low	\$2,000.00
2016100137	Wydown & Brentmoor	Low	\$2,000.00
2027400383	Wallace & Forsyth	Low	\$2,000.00
2000000393	Forsyth	Low	\$2,000.00
2000000394	Forsyth	Low	\$2,000.00
2000000054	Daytona & Watkins	Low	\$2,000.00
2017000272	Audubon & Byron	Low	\$2,000.00
2019400098	Biltmore & Mohawk	Low	\$400.00
2019600257	Audubon & Wellington	Low	\$2,000.00
2014900439	Wydown & Dartford	Low	\$200.00
2014900438	Wydown & Dartford	Low	\$450.00
2014100057	Meramec & Davis	Low	\$2,000.00
2003600565	Forsyth, Brighton & Pershing	Low	\$2,000.00
2000200241	Audubon & Cromwell	Low	\$2,000.00
2023800839	Forsyth, Jackson & Carondelet	Low	\$2,000.00
2000000053	Daytona & Watkins	Low	\$2,000.00
2001800382	Forsyth & Olympian	Low	\$2,000.00
2000000397	Forsyth & Hoyt	Low	\$2,000.00
2000000415	De Mun & Wydown	Low	\$2,000.00
2000000074	Central	Low	\$2,000.00
2015200051	Meramec & Daytona	Low	\$600.00
2000500595	Forsyth, Topton & Kingsbury	Low	\$450.00
2003600563	Forsyth, Brighton & Pershing	Low	\$450.00
2003600564	Forsyth, Brighton & Pershing	Low	\$650.00
2000500598	Forsyth, Topton & Kingsbury	Low	\$2,000.00



City of Clayton ADA Transition Plan
Curb Ramp Cost Projection Summary



ADA ID	Location/Intersection	Priority	Costs
2000500600	Forsyth, Topton & Kingsbury	Low	\$2,000.00
2003600567	Forsyth, Brighton & Pershing	Low	\$2,000.00
2016000142	Wydown & Crestwood	Low	\$200.00
2016200141	Wydown & Audubon	Low	\$300.00
2020400039	Oleta & Seminole	Low	\$400.00
2016100140	Wydown & Brentmoor	Low	\$2,000.00
2000000411	Wydown	Low	\$2,000.00
2001900077	Davis & Central	Low	\$2,000.00
2000000075	Central	Low	\$450.00
2000000073	Central	Low	\$2,000.00
2015900144	Wydown & Brentmoor	Low	\$450.00
2015900145	Wydown & Brentmoor	Low	\$450.00
2003600566	Forsyth, Brighton & Pershing	Low	\$500.00
2000500599	Forsyth, Topton & Kingsbury	Low	\$2,000.00
2016100138	Wydown & Brentmoor	Low	\$2,000.00
2026600621	Forsyth & East	Low	\$2,000.00
2001800381	Forsyth & Olympian	Low	\$2,000.00
2000000392	Forsyth	Low	\$2,000.00
2000000414	De Mun & Wydown	Low	\$2,000.00
2003100651	Gay & Colonial	Low	\$2,000.00
2016200135	Wydown & Audubon	Low	\$400.00
2026600619	Forsyth & East	Low	\$450.00
2000000395	Forsyth & Chaplin	Low	\$2,000.00
2001700620	Forsyth & Stratford	Low	\$2,000.00
2026600618	Forsyth & East	Low	\$2,000.00
2000000398	Forsyth & Hoyt	Low	\$600.00
2000000396	Forsyth & Chaplin	Low	\$2,000.00

Curb Ramp Data Reference Guide

The following list is reference for the headers (1-42) in the ramp data spreadsheet.

1. Ramp ADA ID Number
2. Location
3. Ramp Type
4. Bottom Landing Length (parallel type ramps only)
5. Bottom Landing Width (parallel type ramps only)
6. Bottom Landing Slope (parallel type ramps only)
7. Bottom Landing Cross Slope (parallel type ramps only)
8. Bottom Land to Curb (parallel type ramps only)
9. Ramp Length
10. Ramp Width
11. Ramp Slope
12. Ramp Cross Slope
13. Top Landing Length
14. Top Landing Width
15. Top Landing Slope
16. Top Landing Cross Slope
17. Ramp Length RT (parallel type ramps only)
18. Ramp Width RT (parallel type ramps only)
19. Ramp Slope RT (parallel type ramps only)
20. Ramp Cross Slope RT (parallel type ramps only)
21. Top Landing Length RT (parallel type ramps only)
22. Top Landing Width RT (parallel type ramps only)
23. Top Landing Slope RT (parallel type ramps only)
24. Top Landing Cross Slope RT (parallel type ramps only)
25. Domes Provided
26. Domes Contrast
27. Domes Length
28. Domes Offset
29. Flare Slope LT
30. Flare Slope RT
31. Bottom Landing to Crosswalk
32. Gutter Ponding
33. Gutter Lip
34. Gutter Slope
35. Gutter Cross Slope
36. Activity Score
37. Impedance Score
38. Final Score
39. Activity Ranking
40. Impedance Ranking
41. Final Ranking
42. Costs



City of Clayton ADA Transition Plan Curb Ramp Data Summary



1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22	23	24	25	26	27	28	29	30	31	32	33	34	35	36	37	38	39	40	41	42		
2005300575	Brentwood & University	No Ramp	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	56	100	156	High	High	High	\$2,000
2005300856	Brentwood & University	No Ramp	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	56	100	156	High	High	High	\$2,000
2005300857	Brentwood & University	No Ramp	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	56	100	156	High	High	High	\$2,000
2005300858	Brentwood & University	No Ramp	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	56	100	156	High	High	High	\$2,000
2021800514	Seminary & Alamo	No Ramp	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	56	100	156	High	High	High	\$2,000	
2021800515	Seminary & Alamo	No Ramp	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	56	100	156	High	High	High	\$2,000	
2007900689	Maryland & Brighton	No Ramp	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	55	100	155	High	High	High	\$2,000	
2007100582	Brentwood & Westmoreland	No Ramp	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	53	100	153	High	High	High	\$2,000	
2017600172	Westwood & Byron	No Ramp	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	52	100	152	High	High	High	\$2,000	
2017600173	Westwood & Byron	No Ramp	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	52	100	152	High	High	High	\$2,000	
2010200534	Alamo & Private	No Ramp	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	50	100	150	Med	High	High	\$2,000	
2024500509	Alamo & De Mun	No Ramp	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	50	100	150	Med	High	High	\$2,000	
2024500510	Alamo & De Mun	No Ramp	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	50	100	150	Med	High	High	\$2,000	
2000000153	Forest	No Ramp	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	49	100	149	Med	High	High	\$2,000	
2017500174	Westwood & Parkdale	No Ramp	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	49	100	149	Med	High	High	\$2,000	
2017500177	Westwood & Parkdale	No Ramp	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	49	100	149	Med	High	High	\$2,000	
2010600646	Hanley & Westmoreland	No Ramp	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	47	100	147	Med	High	High	\$2,000	
2022300531	Concordia & Private	No Ramp	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	45	100	145	Med	High	High	\$2,000	
2009800530	Concordia & Private	No Ramp	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	42	100	142	Med	High	High	\$2,000	
2018300181	Westwood & Buckingham	No Ramp	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	40	100	140	Med	High	High	\$2,000	
2014200120	Forest & Wydown	No Ramp	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	37	100	137	Med	High	High	\$2,000	
2014200121	Forest & Wydown	No Ramp	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	37	100	137	Med	High	High	\$2,000	
2014200124	Forest & Wydown	No Ramp	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	37	100	137	Med	High	High	\$2,000	
2014200125	Forest & Wydown	No Ramp	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	37	100	137	Med	High	High	\$2,000	



City of Clayton ADA Transition Plan Curb Ramp Data Summary



1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22	23	24	25	26	27	28	29	30	31	32	33	34	35	36	37	38	39	40	41	42	
202790070	Carondelet & Meramec	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	No	Yes	Yes	No	No	Yes	Yes	Yes	No	Yes	No	Yes	Yes	Yes	No	No	76	55	131	High	Med	High	\$2,000	
2005200593	Forsyth & University	No Ramp	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	31	100	131	Med	High	High	\$2,000		
2005400719	Central & Maryland	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	No	Yes	No	No	No	Yes	Yes	Yes	No	No	No	Yes	No	No	Yes	No	75	55	130	High	Med	High	\$2,000	
2018100196	Glen Ridge & Buckingham	No Ramp	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	30	100	130	Med	High	High	\$2,000		
2018100197	Glen Ridge & Buckingham	No Ramp	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	30	100	130	Med	High	High	\$2,000		
2018100200	Glen Ridge & Buckingham	No Ramp	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	30	100	130	Med	High	High	\$2,000		
2018100201	Glen Ridge & Buckingham	No Ramp	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	30	100	130	Med	High	High	\$2,000		
2000006649	Westmoreland	No Ramp	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	30	100	130	Med	High	High	\$2,000		
2005700726	Maryland & Linden	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	No	Yes	Yes	No	No	No	No	No	No	No	No	N/A	No	Yes	Yes	No	65	65	130	High	Med	High	\$2,000	
2022000286	Clayton & Ridgemoor	No Ramp	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	30	100	130	Med	High	High	\$2,000		
2022000287	Clayton & Ridgemoor	No Ramp	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	30	100	130	Med	High	High	\$2,000		
2007600694	Brentwood & Forsyth	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	No	No	No	No	No	No	Yes	Yes	Yes	No	No	No	N/A	No	Yes	No	69	60	129	High	Med	High	\$2,000	
2012300353	Brentwood & Rosiline	Blended Transition	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	No	No	No	No	No	No	No	No	No	No	No	N/A	No	Yes	Yes	No	64	65	129	High	Med	High	\$2,000	
2023100793	Brentwood & Shaw Park	Parallel	Yes	Yes	No	No	No	No	Yes	No	No	Yes	Yes	No	No	Yes	Yes	No	No	Yes	Yes	No	Yes	Yes	Yes	No	No	N/A	N/A	N/A	No	Yes	No	94	35	129	High	Med	High	\$2,000		
2007700708	Brentwood & Maryland	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	No	Yes	No	No	No	Yes	Yes	Yes	Yes	Yes	No	Yes	Yes	Yes	No	73	55	128	High	Med	High	\$2,000		
2004400552	Hanley & Rear	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	No	No	No	No	No	No	No	No	No	No	No	N/A	No	Yes	No	No	57	70	127	High	Med	High	\$2,000	
2027900772	Carondelet & Meramec	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	No	N/A	N/A	N/A	N/A	No	No	No	No	N/A	Yes	N/A	Yes	Yes	Yes	No	76	50	126	High	Med	High	\$2,000	
2030100796	Shaw Park & Meramec	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	No	No	Yes	No	No	Yes	Yes	Yes	No	Yes	No	N/A	No	Yes	Yes	No	76	50	126	High	Med	High	\$2,000	
2007900691	Maryland & Brighton	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	No	No	No	No	No	No	No	No	No	No	N/A	N/A	N/A	Yes	No	No	55	70	125	High	Med	High	\$2,000	
2023100795	Brentwood & Shaw Park	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	No	No	Yes	Yes	No	Yes	Yes	Yes	No	No	No	N/A	No	Yes	No	94	30	124	High	Low	High	\$2,000		
2012100558	Hanley & Shirley	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	No	No	No	No	No	No	No	No	No	No	Yes	N/A	Yes	Yes	Yes	No	54	70	124	High	Med	High	\$2,000	
2021600500	San Bonita & De Mun	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	No	Yes	Yes	Yes	No	No	No	No	No	No	No	N/A	No	No	No	53	70	123	High	Med	High	\$2,000		
2005600721	Maryland & Bemiston	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	No	No	Yes	No	No	Yes	Yes	Yes	No	Yes	No	N/A	Yes	Yes	Yes	No	72	50	122	High	Med	High	\$2,000	
2004400557	Hanley & Rear	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	No	No	No	No	No	No	No	No	No	No	Yes	No	N/A	Yes	Yes	Yes	No	57	65	122	High	Med	High	\$2,000



City of Clayton ADA Transition Plan Curb Ramp Data Summary



1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22	23	24	25	26	27	28	29	30	31	32	33	34	35	36	37	38	39	40	41	42	
2004500556	Hanley & Country Club	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	No	No	No	No	No	No	No	No	No	No	No	N/A	No	Yes	Yes	No	47	75	122	Med	High	High	\$2,000	
2011600789	Brentwood & Bonhomme	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	No	Yes	Yes	Yes	Yes	No	No	No	No	Yes	N/A	Yes	Yes	No	No	72	50	122	High	Med	High	\$2,000		
2011600792	Brentwood & Bonhomme	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	No	Yes	Yes	Yes	No	Yes	Yes	Yes	Yes	No	No	N/A	Yes	Yes	No	No	72	50	122	High	Med	High	\$2,000	
2004000785	Bonhomme & Meramec	Blended Transition	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	No	No	No	No	No	Yes	Yes	Yes	No	No	No	N/A	Yes	Yes	Yes	No	66	55	121	High	Med	High	\$2,000	
2021800517	Seminary & Alamo	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	No	Yes	Yes	No	Yes	No	No	No	No	No	No	N/A	Yes	Yes	No	No	56	65	121	High	Med	High	\$2,000	
2005700727	Maryland & Linden	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	Yes	No	Yes	Yes	No	No	No	No	No	No	No	No	N/A	Yes	Yes	Yes	No	65	55	120	High	Med	High	\$2,000	
2018400449	Dartford & Arundel	No Ramp	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	20	100	120	Low	High	High	\$2,000	
2018600455	Arundel & University	No Ramp	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	20	100	120	Low	High	High	\$2,000	
2020700275	Crestwood & Hillvale	No Ramp	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	20	100	120	Low	High	High	\$2,000	
2020700276	Crestwood & Hillvale	No Ramp	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	20	100	120	Low	High	High	\$2,000	
2020700277	Crestwood & Hillvale	No Ramp	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	20	100	120	Low	High	High	\$2,000	
2020700278	Crestwood & Hillvale	No Ramp	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	20	100	120	Low	High	High	\$2,000	
2021300284	Crestwood & Claverach	No Ramp	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	20	100	120	Low	High	High	\$2,000	
2025700298	Big Bend & Southmoor	No Ramp	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	20	100	120	Low	High	High	\$2,000	
2025700299	Big Bend & Southmoor	No Ramp	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	20	100	120	Low	High	High	\$2,000	
2029400536	St Rita & Private	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	Yes	No	Yes	Yes	No	No	No	No	No	No	N/A	N/A	N/A	No	Yes	No	No	60	60	120	High	Med	High	\$2,000	
2007600697	Brentwood & Forsyth	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	No	No	Yes	No	No	Yes	Yes	Yes	No	No	Yes	Yes	Yes	Yes	Yes	No	69	50	119	High	Med	High	\$2,000	
2012300354	Brentwood & Rosiline	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	No	No	No	No	No	No	No	No	No	No	N/A	Yes	Yes	Yes	Yes	No	64	55	119	High	Med	High	\$2,000	
2023100794	Brentwood & Shaw Park	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	Yes	Yes	Yes	Yes	Yes	Yes	No	No	No	No	Yes	Yes	Yes	Yes	Yes	94	25	119	High	Low	High	\$2,000		
2000000069	Central	No Ramp	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	19	100	119	Low	High	High	\$2,000		
2000000071	Central	No Ramp	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	19	100	119	Low	High	High	\$2,000		
2012100553	Hanley & Shirley	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	No	No	No	No	No	No	No	No	No	No	Yes	N/A	Yes	Yes	Yes	No	54	65	119	High	Med	High	\$2,000	
2012100554	Hanley & Shirley	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	No	No	No	No	No	No	No	No	No	No	N/A	N/A	N/A	Yes	Yes	Yes	No	54	65	119	High	Med	High	\$2,000
2013600351	Brentwood, Corporate Pk & Orlando	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	No	No	Yes	No	No	Yes	Yes	Yes	Yes	No	No	N/A	No	Yes	No	No	69	50	119	High	Med	High	\$2,000	



**City of Clayton ADA Transition Plan
Curb Ramp Data Summary**



1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22	23	24	25	26	27	28	29	30	31	32	33	34	35	36	37	38	39	40	41	42	
2005500712	Maryland & Meramec	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	No	No	No	No	No	No	Yes	Yes	Yes	No	No	Yes	N/A	Yes	Yes	No	No	63	55	118	High	Med	High	\$2,000
2024600505	San Bonita & De Mun	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	Yes	No	Yes	Yes	No	Yes	No	No	No	No	Yes	No	N/A	No	No	Yes	No	53	65	118	High	Med	High	\$2,000	
2005400717	Central & Maryland	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	Yes	No	Yes	No	Yes	No	Yes	Yes	Yes	No	Yes	No	Yes	No	Yes	No	No	75	42	117	High	Med	High	\$2,000	
2005400718	Central & Maryland	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	No	Yes	Yes	Yes	No	No	Yes	Yes	No	No	Yes	Yes	Yes	Yes	No	No	75	42	117	High	Med	High	\$2,000	
2005600724	Maryland & Bemiston	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	No	Yes	Yes	No	No	Yes	Yes	Yes	No	No	Yes	N/A	No	Yes	Yes	No	72	45	117	High	Med	High	\$2,000	
2005600725	Maryland & Bemiston	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	No	Yes	Yes	No	No	Yes	Yes	Yes	No	No	Yes	N/A	Yes	Yes	Yes	No	72	45	117	High	Med	High	\$2,000	
2016600129	Wydown & Westwood	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	No	Yes	Yes	No	No	Yes	Yes	Yes	Yes	No	No	N/A	Yes	Yes	No	No	62	55	117	High	Med	High	\$2,000	
2004400551	Hanley & Rear	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	No	No	No	No	No	No	No	No	No	No	No	N/A	No	Yes	Yes	Yes	57	60	117	High	Med	High	\$2,000	
2006300032	Sudbury & Roxburgh	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	No	No	No	No	No	No	No	No	No	No	No	N/A	Yes	No	Yes	No	32	85	117	Med	High	High	\$2,000	
2010600645	Hanley & Westmoreland	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	No	No	No	No	No	No	No	No	No	No	No	N/A	No	Yes	Yes	No	47	70	117	Med	Med	High	\$2,000	
2011600790	Brentwood & Bonhomme	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	No	Yes	No	No	No	Yes	Yes	Yes	No	No	No	N/A	No	Yes	Yes	Yes	72	45	117	High	Med	High	\$2,000	
2011600791	Brentwood & Bonhomme	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	No	Yes	Yes	No	No	Yes	Yes	Yes	No	No	Yes	N/A	No	Yes	No	No	72	45	117	High	Med	High	\$2,000	
2000200240	Audubon & Cromwell	No Ramp	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	16	100	116	Low	High	High	\$2,000
2000200243	Audubon & Cromwell	No Ramp	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	16	100	116	Low	High	High	\$2,000
2021000236	Audubon & Somerset	No Ramp	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	16	100	116	Low	High	High	\$2,000	
2021000239	Audubon & Somerset	No Ramp	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	16	100	116	Low	High	High	\$2,000	
2016800433	University & Wydown	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	No	Yes	Yes	No	No	No	No	No	No	No	N/A	N/A	N/A	No	Yes	No	No	40	75	115	Med	High	High	\$2,000
2024500508	Alamo & De Mun	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	Yes	Yes	Yes	No	Yes	No	No	No	No	No	No	N/A	Yes	Yes	No	No	50	65	115	Med	Med	High	\$2,000	
2007600698	Brentwood & Forsyth	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	No	Yes	Yes	No	No	Yes	Yes	Yes	No	Yes	No	Yes	Yes	Yes	No	No	69	45	114	High	Med	High	\$2,000	
2024300761	Bemiston & Carondelet	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	Yes	No	No	No	No	No	Yes	No	Yes	Yes	No	No	Yes	No	No	59	55	114	High	Med	High	\$2,000			
2027000801	Shaw Park & Central	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	No	Yes	Yes	No	No	Yes	Yes	Yes	No	N/A	No	N/A	Yes	Yes	Yes	No	79	35	114	High	Med	High	\$2,000	
2010700560	Maryland & Lee	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	Yes	No	No	No	No	No	No	No	No	No	No	N/A	No	Yes	No	No	44	70	114	Med	Med	High	\$2,000	
2003900783	Bonhomme & Central	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	Yes	No	No	No	No	No	No	Yes	Yes	Yes	Yes	Yes	Yes	N/A	Yes	Yes	No	No	73	40	113	High	Med	High	\$2,000
2007700709	Brentwood & Maryland	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	No	Yes	Yes	No	No	Yes	Yes	Yes	No	No	Yes	Yes	Yes	Yes	Yes	No	73	40	113	High	Med	High	\$2,000	



City of Clayton ADA Transition Plan Curb Ramp Data Summary



1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22	23	24	25	26	27	28	29	30	31	32	33	34	35	36	37	38	39	40	41	42	
2007700710	Brentwood & Maryland	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	No	No	No	No	No	No	Yes	Yes	Yes	No	No	Yes	Yes	Yes	Yes	No	No	73	40	113	High	Med	High	\$2,000
2024400751	Carondelet & Central	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	Yes	Yes	Yes	No	No	Yes	Yes	Yes	No	No	No	N/A	No	Yes	Yes	No	73	40	113	High	Med	High	\$2,000	
2000000248	Audubon & York	No Ramp	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	13	100	113	Low	High	High	\$2,000	
2011800545	Edgewood & Carswold	No Ramp	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	13	100	113	Low	High	High	\$2,000	
2011800546	Edgewood & Carswold	No Ramp	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	13	100	113	Low	High	High	\$2,000	
2017200267	Audubon & Parkdale	No Ramp	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	13	100	113	Low	High	High	\$2,000	
2017200270	Audubon & Parkdale	No Ramp	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	13	100	113	Low	High	High	\$2,000	
2018200263	Audubon & Buckingham	No Ramp	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	13	100	113	Low	High	High	\$2,000	
2018200266	Audubon & Buckingham	No Ramp	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	13	100	113	Low	High	High	\$2,000	
2018800258	Audubon & Oxford	No Ramp	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	13	100	113	Low	High	High	\$2,000	
2019600256	Audubon & Wellington	No Ramp	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	13	100	113	Low	High	High	\$2,000	
2019800244	Audubon & York	No Ramp	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	13	100	113	Low	High	High	\$2,000	
2019800247	Audubon & York	No Ramp	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	13	100	113	Low	High	High	\$2,000	
2024600502	San Bonita & De Mun	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	No	Yes	Yes	No	Yes	No	No	No	No	No	No	N/A	Yes	Yes	Yes	No	53	60	113	High	Med	High	\$2,000	
2024600503	San Bonita & De Mun	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	No	No	No	No	No	No	No	No	No	No	No	Yes	N/A	Yes	Yes	No	53	60	113	High	Med	High	\$2,000	
2000900361	Brentwood & Walinca	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	No	No	No	No	No	No	Yes	Yes	Yes	No	No	No	N/A	No	Yes	Yes	No	57	55	112	High	Med	High	\$2,000
2005600720	Maryland & Bemiston	Parallel	Yes	Yes	No	No	Yes	No	Yes	No	No	Yes	Yes	No	No	Yes	Yes	Yes	No	Yes	Yes	Yes	No	Yes	Yes	Yes	No	N/A	N/A	N/A	Yes	Yes	Yes	No	72	40	112	High	Med	High	\$2,000	
2005600723	Maryland & Bemiston	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	Yes	No	Yes	No	No	No	Yes	Yes	Yes	No	No	Yes	Yes	Yes	Yes	No	Yes	72	40	112	High	Med	High	\$2,000	
2030000847	Corporate Pk & Shaw Park	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	No	No	No	No	No	Yes	Yes	Yes	No	No	No	N/A	No	Yes	No	No	42	70	112	Med	Med	High	\$2,000	
2004500555	Hanley & Country Club	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	No	No	No	No	No	No	No	No	No	No	No	N/A	No	Yes	Yes	No	47	65	112	Med	Med	High	\$2,000	
2011100671	Parkside & Brighton	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	Yes	No	No	No	No	No	No	No	No	No	No	N/A	No	Yes	Yes	No	47	65	112	Med	Med	High	\$2,000	
2004000845	Bonhomme & Meramec	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	No	Yes	Yes	No	Yes	Yes	Yes	Yes	Yes	No	Yes	N/A	No	Yes	No	No	66	45	111	High	Med	High	\$2,000	
2010900747	Central & Forsyth	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	Yes	Yes	No	Yes	Yes	Yes	Yes	Yes	Yes	No	No	No	N/A	No	Yes	No	No	76	35	111	High	Med	High	\$2,000
2021800513	Seminary & Alamo	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	Yes	Yes	Yes	No	Yes	No	No	No	No	No	Yes	No	N/A	Yes	No	No	Yes	56	55	111	High	Med	High	\$2,000



City of Clayton ADA Transition Plan Curb Ramp Data Summary



1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22	23	24	25	26	27	28	29	30	31	32	33	34	35	36	37	38	39	40	41	42	
2007500654	Topton & Maryland	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	Yes	No	Yes	Yes	No	No	No	No	No	No	No	N/A	N/A	No	No	Yes	No	45	65	110	Med	Med	High	\$2,000	
2018300182	Westwood & Buckingham	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	No	Yes	Yes	No	No	Yes	Yes	Yes	Yes	No	No	N/A	No	Yes	No	40	70	110	Med	Med	High	\$2,000		
2017000271	Audubon & Byron	No Ramp	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	10	100	110	Low	High	High	\$2,000		
2017000274	Audubon & Byron	No Ramp	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	10	100	110	Low	High	High	\$2,000		
2007600695	Brentwood & Forsyth	Island Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	Yes	No	No	No	No	No	Yes	Yes	Yes	No	No	No	N/A	Yes	Yes	Yes	Yes	69	40	109	High	Med	High	\$2,000
2018000763	Meramec & Forsyth	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	No	Yes	No	Yes	No	No	Yes	Yes	Yes	No	No	No	N/A	Yes	Yes	Yes	No	69	40	109	High	Med	High	\$2,000
2027000800	Shaw Park & Central	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	Yes	No	Yes	No	No	Yes	Yes	Yes	No	No	No	N/A	No	Yes	Yes	Yes	79	30	109	High	Low	High	\$2,000	
2013600349	Brentwood, Corporate Pk & Orlando	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	Yes	No	No	No	No	Yes	Yes	Yes	No	No	No	N/A	Yes	Yes	Yes	No	69	40	109	High	Med	High	\$2,000	
2019300034	Whitburn & Clayshire	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	No	No	No	No	No	No	No	No	No	No	No	N/A	No	Yes	Yes	No	32	77	109	Med	High	High	\$2,000	
2019000744	Central & Forsyth	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	Yes	No	Yes	No	Yes	No	Yes	Yes	Yes	No	No	Yes	N/A	No	Yes	Yes	Yes	76	32	108	High	Low	High	\$2,000	
2030100804	Shaw Park & Meramec	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	No	No	Yes	Yes	No	Yes	Yes	Yes	Yes	No	No	N/A	Yes	Yes	Yes	No	76	32	108	High	Low	High	\$2,000	
2004000787	Bonhomme & Meramec	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	No	No	No	No	No	Yes	Yes	Yes	No	Yes	Yes	N/A	Yes	Yes	Yes	No	66	40	106	High	Med	High	\$2,000	
2019000743	Central & Forsyth	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	No	Yes	No	Yes	No	Yes	Yes	Yes	No	No	Yes	N/A	Yes	Yes	Yes	Yes	76	30	106	High	Low	High	\$2,000	
2019000746	Central & Forsyth	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	Yes	No	Yes	No	No	No	Yes	Yes	Yes	No	No	Yes	N/A	No	Yes	Yes	Yes	76	30	106	High	Low	High	\$2,000	
2019000749	Central & Forsyth	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	Yes	Yes	No	No	Yes	Yes	Yes	Yes	No	Yes	No	N/A	No	Yes	Yes	Yes	76	30	106	High	Low	High	\$2,000	
2027900771	Carondelet & Meramec	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	No	Yes	Yes	N/A	N/A	No	No	No	No	N/A	No	N/A	Yes	Yes	Yes	No	76	30	106	High	Low	High	\$2,000	
2004700572	Brentwood & Pershing	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	No	Yes	Yes	No	Yes	Yes	Yes	Yes	Yes	No	No	N/A	No	Yes	No	50	55	105	Med	Med	High	\$2,000		
2023100850	Brentwood & Shaw Park	Blended Transition	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	No	No	Yes	Yes	Yes	Yes	94	10	104	High	Low	High	\$200	
2018000766	Meramec & Forsyth	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	Yes	No	Yes	No	No	No	Yes	Yes	Yes	No	No	Yes	N/A	No	Yes	No	Yes	69	35	104	High	Med	High	\$2,000	
2022500009	Brentwood & Francis	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	No	No	No	No	No	No	No	No	No	No	Yes	N/A	No	Yes	No	24	80	104	Low	High	High	\$2,000		
2008200049	Brentwood, Corporate Pk & Daytona	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	No	No	No	No	No	Yes	Yes	Yes	Yes	No	No	N/A	No	Yes	Yes	No	49	55	104	Med	Med	High	\$2,000	
2008700526	San Bonita & St Rita	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	No	Yes	Yes	No	No	No	No	No	No	No	Yes	N/A	Yes	Yes	No	Yes	42	62	104	Med	Med	High	\$2,000	
2013600346	Brentwood, Corporate Pk & Orlando	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	No	Yes	Yes	No	No	Yes	Yes	Yes	Yes	N/A	N/A	N/A	Yes	Yes	Yes	Yes	69	35	104	High	Med	High	\$2,000	
2021400521	San Bonita & Seminary	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	No	Yes	Yes	No	Yes	No	No	No	No	No	No	N/A	Yes	Yes	Yes	No	44	60	104	Med	Med	High	\$2,000	



City of Clayton ADA Transition Plan Curb Ramp Data Summary



1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22	23	24	25	26	27	28	29	30	31	32	33	34	35	36	37	38	39	40	41	42
203090820	Jackson & Maryland	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	Yes	No	No	No	No	No	No	No	No	No	No	Yes	No	No	Yes	No	29	75	104	Med	High	High	\$2,000
200000658	Topton	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	Yes	No	Yes	Yes	No	No	No	No	No	No	No	N/A	N/A	No	Yes	No	31	72	103	Med	Med	High	\$2,000	
2007700707	Brentwood & Maryland	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	No	No	Yes	Yes	No	Yes	Yes	Yes	No	Yes	No	Yes	Yes	Yes	No	73	30	103	High	Low	High	\$2,000	
2024400752	Carondelet & Central	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	Yes	Yes	Yes	No	No	Yes	Yes	Yes	No	No	N/A	No	Yes	No	73	30	103	High	Low	High	\$2,000		
2024400753	Carondelet & Central	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	Yes	No	Yes	No	Yes	No	No	No	No	Yes	Yes	N/A	Yes	Yes	No	73	30	103	High	Low	High	\$2,000	
2007100581	Brentwood & Westmoreland	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	Yes	Yes	Yes	Yes	No	Yes	Yes	Yes	No	No	N/A	No	Yes	No	53	50	103	High	Med	High	\$2,000		
2000900360	Brentwood & Walinca	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	No	No	Yes	No	No	Yes	Yes	Yes	Yes	No	No	N/A	Yes	Yes	Yes	No	57	45	102	High	Med	High	\$2,000
2006300031	Sudbury & Roxburgh	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	No	No	No	No	No	No	No	No	No	No	N/A	No	Yes	Yes	No	32	70	102	Med	Med	High	\$2,000	
2008700525	San Bonita & St Rita	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	Yes	No	No	No	No	No	No	No	No	No	N/A	No	No	Yes	Yes	42	60	102	Med	Med	High	\$2,000	
2008900668	Topton & Mark Twain	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	No	Yes	Yes	No	Yes	Yes	No	Yes	Yes	No	No	N/A	No	No	No	47	55	102	Med	Med	High	\$2,000	
2009800527	Concordia & Private	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	Yes	No	Yes	Yes	No	No	No	No	No	No	No	Yes	N/A	No	Yes	No	42	60	102	Med	Med	High	\$2,000	
2013700114	Polo & Polo	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	No	Yes	Yes	No	No	No	No	No	No	N/A	N/A	N/A	No	Yes	Yes	No	32	70	102	Med	Med	High	\$2,000
2019300035	Whitburn & Clayshire	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	No	No	No	No	No	Yes	No	No	No	No	No	N/A	No	Yes	Yes	Yes	32	70	102	Med	Med	High	\$2,000
2005100629	Pershing & Bemiston	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	No	Yes	Yes	No	Yes	Yes	Yes	Yes	Yes	No	No	N/A	No	Yes	No	36	65	101	Med	Med	High	\$2,000	
2010900745	Central & Forsyth	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	Yes	Yes	Yes	No	Yes	Yes	Yes	Yes	No	Yes	Yes	N/A	Yes	Yes	Yes	No	76	25	101	High	Low	High	\$2,000
2011400810	Carondelet, Lyle & Colorado	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	No	Yes	No	No	No	No	No	No	No	No	N/A	Yes	Yes	Yes	No	36	65	101	Med	Med	High	\$2,000	
2030100802	Shaw Park & Meramec	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	Yes	Yes	Yes	No	No	No	Yes	Yes	Yes	Yes	Yes	N/A	No	Yes	Yes	Yes	76	25	101	High	Low	High	\$2,000
2030100803	Shaw Park & Meramec	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	Yes	No	Yes	Yes	No	Yes	Yes	Yes	Yes	Yes	No	N/A	Yes	Yes	Yes	No	76	25	101	High	Low	High	\$2,000
2004800570	Brentwood & Pershing	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	No	Yes	Yes	No	No	No	No	No	No	No	N/A	No	Yes	No	No	41	60	101	Med	Med	High	\$2,000	
2021800516	Seminary & Alamo	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	Yes	Yes	Yes	No	No	No	No	No	No	No	Yes	N/A	No	Yes	No	56	45	101	High	Med	High	\$2,000	
2024700498	De Mun & Southwood	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	No	No	No	No	No	No	No	No	No	No	No	N/A	No	Yes	Yes	No	36	65	101	Med	Med	High	\$2,000
2005400716	Central & Maryland	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	Yes	No	No	Yes	Yes	Yes	Yes	Yes	No	No	No	Yes	No	Yes	Yes	Yes	75	25	100	High	Low	High	\$2,000
2007500653	Topton & Maryland	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	No	Yes	Yes	No	No	No	No	No	No	N/A	No	N/A	Yes	Yes	Yes	Yes	45	55	100	Med	Med	High	\$2,000
2007500686	Topton & Maryland	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	No	No	No	No	No	No	No	No	No	No	Yes	N/A	No	Yes	Yes	No	45	55	100	Med	Med	High	\$2,000



City of Clayton ADA Transition Plan Curb Ramp Data Summary



1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22	23	24	25	26	27	28	29	30	31	32	33	34	35	36	37	38	39	40	41	42
2007500687	Topton & Maryland	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	No	No	No	No	No	Yes	Yes	Yes	No	No	Yes	N/A	No	Yes	Yes	No	45	55	100	Med	Med	High	\$2,000
2000000662	Topton	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	No	Yes	Yes	No	Yes	No	No	No	No	No	No	N/A	No	Yes	No	No	45	55	100	Med	Med	High	\$2,000
2010200535	Alamo & Private	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	No	Yes	Yes	No	No	No	No	No	No	N/A	N/A	N/A	No	Yes	Yes	No	50	50	100	Med	Med	High	\$2,000
2013000540	Hanley & Walinca	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	No	Yes	Yes	Yes	No	No	No	No	No	No	N/A	Yes	Yes	No	No	33	67	100	Med	Med	High	\$2,000	
2016800432	University & Wydown	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	No	Yes	Yes	No	No	No	No	No	No	No	N/A	No	Yes	No	No	40	60	100	Med	Med	High	\$2,000	
2024500506	Alamo & De Mun	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	Yes	No	Yes	Yes	Yes	No	No	No	No	No	No	N/A	No	Yes	No	No	50	50	100	Med	Med	High	\$2,000	
2024500511	Alamo & De Mun	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	Yes	Yes	Yes	No	Yes	No	No	No	No	No	N/A	Yes	Yes	Yes	No	50	50	100	Med	Med	High	\$2,000	
2004300401	Ellenwood & University	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	Yes	Yes	Yes	No	Yes	No	No	No	No	No	N/A	No	Yes	No	No	34	65	99	Med	Med	High	\$2,000	
2010800765	Meramec & Forsyth	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	Yes	Yes	No	No	Yes	Yes	Yes	Yes	No	No	Yes	N/A	No	Yes	Yes	Yes	69	30	99	High	Low	High	\$2,000
2010800768	Meramec & Forsyth	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	Yes	Yes	No	Yes	Yes	Yes	Yes	Yes	No	No	No	N/A	No	Yes	Yes	No	69	30	99	High	Low	High	\$2,000
2011300777	Brentwood & Carondelet	Parallel	Yes	Yes	Yes	Yes	No	Yes	Yes	No	Yes	Yes	Yes	Yes	No	Yes	Yes	No	Yes	Yes	Yes	No	No	Yes	Yes	Yes	No	N/A	N/A	N/A	No	Yes	No	69	30	99	High	Low	High	\$2,000	
2024300758	Bemiston & Carondelet	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	Yes	Yes	Yes	Yes	Yes	No	No	No	No	Yes	No	N/A	Yes	No	Yes	No	59	40	99	High	Med	High	\$2,000
2008200339	Brentwood, Corporate Pk & Daytona	Island Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	Yes	No	No	No	No	Yes	Yes	Yes	No	No	No	N/A	Yes	Yes	Yes	No	49	50	99	Med	Med	High	\$2,000
2009100112	Polo & Middle Polo	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	No	Yes	Yes	No	No	No	No	No	No	No	No	N/A	No	Yes	No	No	29	70	99	Med	Med	High	\$2,000
2010700561	Maryland & Lee	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	Yes	No	No	No	No	No	No	No	No	No	No	N/A	No	Yes	Yes	No	44	55	99	Med	Med	High	\$2,000
2012500116	Polo & N Polo	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	No	No	No	No	No	No	No	No	No	N/A	N/A	N/A	No	Yes	Yes	Yes	44	55	99	Med	Med	High	\$2,000
2013600347	Brentwood, Corporate Pk & Orlando	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	Yes	No	Yes	No	No	Yes	Yes	Yes	Yes	No	No	Yes	No	Yes	Yes	Yes	69	30	99	High	Low	High	\$2,000
2024400754	Carondelet & Central	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	No	No	No	No	Yes	No	N/A	No	Yes	No	No	73	25	98	High	Low	Med	\$2,000
2024400755	Carondelet & Central	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	Yes	No	Yes	No	Yes	No	No	No	No	Yes	Yes	N/A	Yes	Yes	No	Yes	73	25	98	High	Low	Med	\$2,000
2025900798	Bemiston & Shaw Park	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	No	No	Yes	No	No	Yes	Yes	Yes	No	Yes	No	N/A	No	Yes	Yes	No	56	42	98	High	Med	Med	\$2,000
2007100580	Brentwood & Westmoreland	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	No	Yes	Yes	Yes	No	Yes	Yes	Yes	No	N/A	N/A	N/A	Yes	Yes	No	No	53	45	98	High	Med	Med	\$2,000
2007400665	Topton & Mark Twain	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	No	Yes	Yes	No	Yes	No	No	No	No	N/A	N/A	N/A	Yes	Yes	Yes	No	53	45	98	High	Med	Med	\$2,000
2007400666	Topton & Mark Twain	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	No	Yes	Yes	No	Yes	No	No	No	No	No	No	N/A	Yes	Yes	Yes	No	53	45	98	High	Med	Med	\$2,000
2012700539	Hanley & Rear	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	Yes	Yes	Yes	No	No	No	No	No	No	No	No	N/A	No	Yes	No	No	33	65	98	Med	Med	Med	\$2,000



City of Clayton ADA Transition Plan Curb Ramp Data Summary



1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22	23	24	25	26	27	28	29	30	31	32	33	34	35	36	37	38	39	40	41	42
2021600504	San Bonita & De Mun	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	No	Yes	Yes	Yes	No	No	No	No	No	N/A	N/A	N/A	No	No	Yes	No	53	45	98	High	Med	Med	\$2,000
2024600497	San Bonita & De Mun	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	No	Yes	Yes	No	Yes	No	No	No	No	No	N/A	Yes	Yes	Yes	No	53	45	98	High	Med	Med	\$2,000	
2024600501	San Bonita & De Mun	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	No	Yes	Yes	No	Yes	No	No	No	No	N/A	N/A	N/A	No	Yes	Yes	No	53	45	98	High	Med	Med	\$2,000
2000900356	Brentwood & Walinca	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	No	Yes	Yes	Yes	No	Yes	Yes	Yes	Yes	No	No	Yes	Yes	Yes	No	57	40	97	High	Med	Med	\$2,000	
2030000848	Corporate Pk & Shaw Park	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	No	No	No	No	No	Yes	Yes	Yes	No	No	No	N/A	No	Yes	Yes	No	42	55	97	Med	Med	Med	\$2,000
2000000664	Topton	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	No	Yes	Yes	No	Yes	No	No	No	No	No	N/A	Yes	Yes	Yes	No	42	55	97	Med	Med	Med	\$2,000	
2000000655	Topton	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	No	Yes	Yes	No	No	No	No	No	No	N/A	No	N/A	No	No	Yes	Yes	31	65	96	Med	Med	Med	\$2,000
2004000846	Bonhomme & Meramec	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	Yes	Yes	Yes	No	Yes	Yes	Yes	Yes	Yes	No	Yes	N/A	Yes	Yes	Yes	No	66	30	96	High	Low	Med	\$2,000
2004900624	Central & Pershing	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	No	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	N/A	N/A	N/A	No	Yes	No	No	51	45	96	Med	Med	Med	\$2,000
2005100630	Pershing & Bemiston	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	No	Yes	Yes	No	No	Yes	Yes	Yes	Yes	No	No	N/A	No	Yes	Yes	No	36	60	96	Med	Med	Med	\$2,000
2007300681	Maryland & Lancaster	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	Yes	No	No	No	No	No	No	No	No	No	N/A	No	Yes	Yes	Yes	36	60	96	Med	Med	Med	\$2,000	
2022200289	Clayton & Claytonia	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	Yes	No	Yes	Yes	Yes	No	No	No	No	No	N/A	N/A	N/A	No	No	Yes	No	41	55	96	Med	Med	Med	\$2,000
2027900773	Carondelet & Meramec	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	No	Yes	No	Yes	Yes	No	76	20	96	High	Low	Med	\$2,000
2004800568	Brentwood & Pershing	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	No	Yes	Yes	No	No	Yes	Yes	Yes	No	N/A	N/A	N/A	Yes	Yes	Yes	No	41	55	96	Med	Med	Med	\$2,000
2024700499	De Mun & Southwood	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	No	Yes	Yes	No	No	No	No	No	No	No	N/A	Yes	Yes	Yes	No	36	60	96	Med	Med	Med	\$2,000	
2029600373	Big Bend & Northmoor	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	No	Yes	Yes	No	No	No	No	No	No	N/A	N/A	N/A	Yes	Yes	Yes	No	26	70	96	Low	Med	Med	\$2,000
2003300644	Kingsbury & Central	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	No	Yes	Yes	No	No	Yes	Yes	Yes	Yes	No	No	N/A	Yes	Yes	No	35	60	95	Med	Med	Med	\$2,000	
2007500683	Topton & Maryland	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	No	No	No	No	No	No	Yes	Yes	Yes	No	Yes	No	Yes	Yes	Yes	No	45	50	95	Med	Med	Med	\$2,000
2031100814	Ritz Carlton & Colorado	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	No	No	No	No	No	No	No	No	No	No	No	N/A	Yes	No	Yes	Yes	30	65	95	Med	Med	Med	\$2,000
2000000661	Topton	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	No	Yes	Yes	No	No	No	No	No	No	No	No	N/A	No	Yes	Yes	No	45	50	95	Med	Med	Med	\$2,000
2006200015	Francis & Roxburgh	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	No	No	No	No	No	No	No	No	No	No	N/A	No	Yes	Yes	No	35	60	95	Med	Med	Med	\$2,000	
2014700330	Hanley & Polo	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	Yes	Yes	No	No	No	No	No	No	No	No	Yes	No	N/A	Yes	Yes	Yes	No	30	65	95	Med	Med	Med	\$2,000
2016800427	University & Wydown	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	Yes	Yes	Yes	Yes	Yes	No	Yes	No	No	No	N/A	N/A	N/A	No	Yes	No	No	40	55	95	Med	Med	Med	\$2,000
2020100294	Big Bend & Oak Knoll	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	No	No	No	No	No	No	No	No	No	No	N/A	No	Yes	Yes	No	30	65	95	Med	Med	Med	\$2,000	



City of Clayton ADA Transition Plan Curb Ramp Data Summary



1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22	23	24	25	26	27	28	29	30	31	32	33	34	35	36	37	38	39	40	41	42	
2019200011	Francis & Whitburn	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	No	No	Yes	No	No	No	No	No	No	Yes	No	N/A	No	Yes	No	No	24	70	94	Low	Med	Med	\$2,000	
2013600350	Brentwood, Corporate Pk & Orlando	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	Yes	No	No	Yes	Yes	Yes	Yes	Yes	No	No	No	N/A	Yes	Yes	Yes	No	69	25	94	High	Low	Med	\$2,000	
2013600352	Brentwood, Corporate Pk & Orlando	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	Yes	No	Yes	Yes	No	Yes	Yes	Yes	No	No	No	N/A	No	Yes	No	Yes	69	25	94	High	Low	Med	\$2,000	
2021400524	San Bonita & Seminary	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	No	Yes	Yes	Yes	No	No	No	No	No	No	N/A	No	Yes	No	Yes	44	50	94	Med	Med	Med	\$2,000		
2003900841	Bonhomme & Central	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	No	Yes	N/A	Yes	Yes	Yes	No	73	20	93	High	Low	Med	\$450	
2003900842	Bonhomme & Central	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	No	Yes	N/A	Yes	Yes	Yes	No	73	20	93	High	Low	Med	\$450	
2003900843	Bonhomme & Central	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	No	Yes	N/A	Yes	Yes	No	Yes	73	20	93	High	Low	Med	\$450	
2003900782	Bonhomme & Central	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	Yes	Yes	Yes	No	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	No	73	20	93	High	Low	Med	\$650		
2007000612	Meramec & Westmoreland	Parallel	Yes	Yes	Yes	Yes	No	Yes	Yes	No	Yes	No	No	No	No	Yes	Yes	Yes	No	No	No	No	No	No	Yes	Yes	Yes	Yes	N/A	N/A	N/A	Yes	Yes	Yes	No	48	45	93	Med	Med	Med	\$2,000
2001500344	Brentwood & Venetian	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	No	Yes	Yes	Yes	No	Yes	Yes	Yes	Yes	N/A	N/A	N/A	No	Yes	Yes	No	63	30	93	High	Low	Med	\$2,000	
2012200550	Carswood & Shirley	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	No	No	Yes	No	No	No	No	No	No	No	No	Yes	N/A	No	Yes	No	23	70	93	Low	Med	Med	\$2,000	
2013000541	Hanley & Walinca	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	No	Yes	Yes	Yes	No	No	No	No	No	No	No	N/A	No	Yes	Yes	No	33	60	93	Med	Med	Med	\$2,000	
2021100292	Big Bend & Tuscany	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	Yes	No	Yes	Yes	No	Yes	No	No	No	No	N/A	N/A	N/A	No	Yes	Yes	No	33	60	93	Med	Med	Med	\$2,000	
2017600155	Westwood & Byron	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	Yes	Yes	Yes	No	Yes	Yes	Yes	Yes	Yes	N/A	N/A	N/A	No	Yes	Yes	Yes	52	40	92	High	Med	Med	\$2,000	
2000000118	Shirley	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	Yes	Yes	Yes	No	Yes	No	No	N/A	N/A	N/A	N/A	No	Yes	No	Yes	47	45	92	Med	Med	Med	\$2,000		
2004000786	Bonhomme & Meramec	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	Yes	No	No	No	No	Yes	Yes	Yes	No	No	Yes	N/A	No	Yes	No	Yes	66	25	91	High	Low	Med	\$2,000	
2004000788	Bonhomme & Meramec	Blended Transition	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	No	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	No	Yes	N/A	Yes	Yes	No	No	66	25	91	High	Low	Med	\$2,000	
2006100222	Glen Ridge & York	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	Yes	Yes	Yes	No	Yes	Yes	Yes	Yes	No	No	No	N/A	No	No	Yes	No	36	55	91	Med	Med	Med	\$2,000	
2007200584	Brentwood & Westmoreland	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	No	Yes	Yes	No	No	Yes	Yes	Yes	Yes	No	No	N/A	No	No	Yes	Yes	51	40	91	Med	Med	Med	\$2,000	
2010900748	Central & Forsyth	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	Yes	Yes	Yes	No	Yes	Yes	Yes	Yes	Yes	No	No	Yes	N/A	Yes	No	Yes	76	15	91	High	Low	Med	\$2,000		
2010900750	Central & Forsyth	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	Yes	Yes	Yes	No	No	Yes	Yes	Yes	No	Yes	Yes	N/A	No	Yes	No	Yes	76	15	91	High	Low	Med	\$2,000	
2005300574	Brentwood & University	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	Yes	Yes	Yes	No	Yes	Yes	Yes	Yes	Yes	N/A	N/A	N/A	No	Yes	Yes	No	56	35	91	High	Med	Med	\$2,000	
2012500115	Polo & N Polo	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	No	Yes	Yes	Yes	No	No	No	No	No	N/A	N/A	N/A	No	Yes	No	No	44	47	91	Med	Med	Med	\$2,000	
2003300642	Kingsbury & Central	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	No	No	Yes	No	No	Yes	Yes	Yes	Yes	No	Yes	Yes	Yes	Yes	No	No	35	55	90	Med	Med	Med	\$2,000	



City of Clayton ADA Transition Plan Curb Ramp Data Summary



1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22	23	24	25	26	27	28	29	30	31	32	33	34	35	36	37	38	39	40	41	42	
2007900690	Maryland & Brighton	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	Yes	Yes	Yes	Yes	No	No	No	No	No	No	No	N/A	No	Yes	Yes	Yes	55	35	90	High	Med	Med	\$2,000	
2000000288	Ridgemoor	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	Yes	Yes	Yes	Yes	Yes	Yes	No	No	No	No	No	N/A	No	No	Yes	No	40	50	90	Med	Med	Med	\$2,000	
2009000106	Polo & Middle Polo	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	Yes	No	Yes	Yes	No	No	No	No	No	No	No	No	N/A	No	Yes	No	No	30	60	90	Med	Med	Med	\$2,000	
2009000109	Polo & Middle Polo	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	Yes	No	Yes	Yes	No	No	No	No	No	No	No	No	N/A	No	Yes	No	No	30	60	90	Med	Med	Med	\$2,000	
2009900529	Concordia & Alamo	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	Yes	No	Yes	Yes	No	No	No	No	No	No	N/A	N/A	N/A	No	Yes	Yes	Yes	45	45	90	Med	Med	Med	\$2,000	
2024500507	Alamo & De Mun	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	Yes	Yes	Yes	Yes	Yes	Yes	No	No	No	No	No	N/A	Yes	Yes	Yes	No	50	40	90	Med	Med	Med	\$2,000	
2024500512	Alamo & De Mun	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	Yes	Yes	Yes	No	No	No	No	No	No	No	No	N/A	No	Yes	No	Yes	50	40	90	Med	Med	Med	\$2,000	
2010800762	Meramec & Forsyth	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	No	N/A	Yes	Yes	Yes	No	69	20	89	High	Low	Med	\$450	
2010800767	Meramec & Forsyth	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	Yes	Yes	Yes	No	No	Yes	Yes	Yes	Yes	No	N/A	N/A	Yes	Yes	Yes	Yes	69	20	89	High	Low	Med	\$600	
2008000702	Maryland & Forsyth	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	No	No	Yes	Yes	No	Yes	Yes	Yes	Yes	Yes	No	N/A	Yes	Yes	Yes	No	64	25	89	High	Low	Med	\$2,000	
2008000705	Maryland & Forsyth	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	No	No	No	No	No	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	64	25	89	High	Low	Med	\$2,000	
2010800764	Meramec & Forsyth	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	Yes	Yes	No	No	No	Yes	Yes	Yes	No	No	Yes	N/A	Yes	Yes	Yes	Yes	69	20	89	High	Low	Med	\$2,000	
2011000738	Forsyth & Bemiston	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	No	Yes	No	Yes	No	Yes	Yes	Yes	Yes	Yes	Yes	Yes	N/A	Yes	Yes	Yes	No	69	20	89	High	Low	Med	\$2,000
2011300774	Brentwood & Carondelet	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	Yes	Yes	Yes	No	Yes	Yes	Yes	Yes	No	No	No	N/A	No	Yes	No	Yes	69	20	89	High	Low	Med	\$2,000	
2011300775	Brentwood & Carondelet	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	Yes	Yes	Yes	No	Yes	Yes	Yes	Yes	No	No	No	N/A	No	Yes	Yes	Yes	69	20	89	High	Low	Med	\$2,000	
2011300776	Brentwood & Carondelet	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	No	Yes	No	Yes	Yes	Yes	Yes	Yes	No	No	Yes	N/A	Yes	Yes	Yes	Yes	69	20	89	High	Low	Med	\$2,000	
2030400363	Hanley & Clayton	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	Yes	Yes	Yes	No	No	No	No	No	No	No	Yes	No	N/A	Yes	Yes	Yes	No	34	55	89	Med	Med	Med	\$2,000
2000000119	Shirley	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	Yes	Yes	Yes	No	No	No	No	No	No	No	N/A	N/A	N/A	No	Yes	Yes	Yes	47	42	89	Med	Med	Med	\$2,000
2008200341	Brentwood, Corporate Pk & Daytona	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	Yes	No	No	No	Yes	Yes	Yes	Yes	No	No	No	N/A	No	Yes	Yes	No	49	40	89	Med	Med	Med	\$2,000	
2013600348	Brentwood, Corporate Pk & Orlando	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	Yes	No	Yes	No	Yes	Yes	Yes	Yes	No	No	No	N/A	No	Yes	Yes	Yes	69	20	89	High	Low	Med	\$2,000	
2021400520	San Bonita & Seminary	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	Yes	Yes	Yes	No	No	No	No	No	No	No	No	N/A	Yes	Yes	Yes	No	44	45	89	Med	Med	Med	\$2,000	
2000000656	Topton	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	Yes	No	Yes	Yes	No	No	No	No	No	No	No	No	N/A	N/A	No	Yes	Yes	No	31	57	88	Med	Med	Med	\$2,000
2019500213	Glen Ridge & Wellington	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	Yes	Yes	Yes	No	Yes	Yes	Yes	Yes	No	No	No	N/A	Yes	No	Yes	No	33	55	88	Med	Med	Med	\$2,000	
2024400756	Carondelet & Central	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	No	Yes	Yes	Yes	Yes	Yes	No	No	No	No	Yes	Yes	N/A	Yes	Yes	No	Yes	73	15	88	High	Low	Med	\$2,000



City of Clayton ADA Transition Plan Curb Ramp Data Summary



1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22	23	24	25	26	27	28	29	30	31	32	33	34	35	36	37	38	39	40	41	42	
2012200548	Carswood & Shirley	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	No	No	No	No	No	No	No	No	No	No	No	Yes	N/A	No	Yes	No	No	23	65	88	Low	Med	Med	\$2,000
2019000186	Westwood & Oxford	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	Yes	Yes	Yes	Yes	No	Yes	Yes	Yes	No	No	No	N/A	Yes	No	Yes	No	37	50	87	Med	Med	Med	\$2,000	
200000660	Topton	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	No	Yes	Yes	No	Yes	No	No	No	No	N/A	N/A	N/A	Yes	Yes	Yes	No	42	45	87	Med	Med	Med	\$2,000	
2015400463	University & Aberdeen	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	No	Yes	Yes	No	No	No	No	No	No	N/A	No	N/A	No	Yes	No	No	22	65	87	Low	Med	Med	\$2,000	
200000657	Topton	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	No	Yes	Yes	No	Yes	No	No	No	No	N/A	No	N/A	Yes	Yes	Yes	Yes	31	55	86	Med	Med	Med	\$2,000	
2000300234	Cromwell & Glen Ridge	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	Yes	Yes	Yes	No	Yes	Yes	Yes	Yes	Yes	N/A	N/A	N/A	No	No	Yes	No	36	50	86	Med	Med	Med	\$2,000	
2005100633	Pershing & Bemiston	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	Yes	Yes	Yes	Yes	No	Yes	Yes	Yes	Yes	No	No	N/A	No	Yes	No	No	36	50	86	Med	Med	Med	\$2,000	
2006100223	Glen Ridge & York	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	No	Yes	Yes	No	Yes	Yes	Yes	Yes	Yes	N/A	N/A	N/A	No	Yes	No	Yes	36	50	86	Med	Med	Med	\$2,000	
2006900679	Maryland & Crandon	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	No	Yes	Yes	Yes	No	No	No	No	No	N/A	N/A	N/A	No	No	No	No	36	50	86	Med	Med	Med	\$2,000	
2007200579	Brentwood & Westmoreland	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	Yes	No	Yes	Yes	Yes	Yes	Yes	Yes	Yes	N/A	N/A	N/A	No	Yes	No	No	51	35	86	Med	Med	Med	\$2,000	
2007600696	Brentwood & Forsyth	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	Yes	No	No	Yes	No	Yes	No	Yes	Yes	No	No	Yes	No	Yes	No	Yes	69	17	86	High	Low	Med	\$2,000	
2011500811	Carondelet & Ritz Carlton	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	No	Yes	No	Yes	Yes	No	No	No	No	No	Yes	N/A	Yes	Yes	Yes	No	36	50	86	Med	Med	Med	\$2,000	
2019900194	Westwood & York	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	No	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	N/A	N/A	N/A	No	No	Yes	No	36	50	86	Med	Med	Med	\$2,000	
2025900797	Bemiston & Shaw Park	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	No	Yes	Yes	No	No	Yes	Yes	No	No	No	N/A	No	Yes	Yes	Yes	Yes	56	30	86	High	Low	Med	\$2,000	
2030400362	Hanley & Clayton	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	Yes	Yes	Yes	Yes	No	No	No	No	No	No	N/A	N/A	N/A	Yes	Yes	Yes	No	34	52	86	Med	Med	Med	\$2,000	
2024800495	De Mun & San Bonita	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	Yes	No	Yes	Yes	No	No	No	No	No	No	No	No	N/A	Yes	Yes	Yes	No	41	45	86	Med	Med	Med	\$2,000	
2029600370	Big Bend & Northmoor	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	Yes	No	No	No	No	No	No	No	No	No	No	N/A	No	Yes	No	Yes	26	60	86	Low	Med	Med	\$2,000	
2029600371	Big Bend & Northmoor	Island Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	No	Yes	Yes	No	No	No	No	No	No	N/A	N/A	N/A	Yes	No	Yes	Yes	26	60	86	Low	Med	Med	\$2,000	
2003300641	Kingsbury & Central	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	No	Yes	Yes	Yes	No	Yes	Yes	Yes	Yes	No	Yes	Yes	Yes	Yes	No	No	35	50	85	Med	Med	Med	\$2,000	
2023800836	Forsyth, Jackson & Carondelet	Island Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	No	No	No	No	No	No	No	No	No	No	No	N/A	No	Yes	Yes	No	25	60	85	Low	Med	Med	\$2,000	
2023800837	Forsyth, Jackson & Carondelet	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	No	No	No	No	No	No	Yes	Yes	Yes	Yes	No	No	N/A	No	Yes	No	No	25	60	85	Low	Med	Med	\$2,000
2008800066	Walincy & Meramec	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	No	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	N/A	N/A	N/A	Yes	Yes	No	No	50	35	85	Med	Med	Med	\$2,000	
2004700573	Brentwood & Pershing	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	No	No	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	No	N/A	No	Yes	No	No	50	35	85	Med	Med	Med	\$2,000	
2006200016	Francis & Roxburgh	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	No	No	Yes	No	No	No	No	No	No	No	No	N/A	No	Yes	Yes	Yes	35	50	85	Med	Med	Med	\$2,000	



City of Clayton ADA Transition Plan Curb Ramp Data Summary



1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22	23	24	25	26	27	28	29	30	31	32	33	34	35	36	37	38	39	40	41	42	
2009900532	Concordia & Alamo	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	No	Yes	Yes	Yes	No	No	No	No	No	N/A	N/A	N/A	No	Yes	Yes	Yes	45	40	85	Med	Med	Med	\$2,000	
2015000329	Polo & Polo	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	Yes	No	No	No	No	No	No	No	No	No	No	N/A	Yes	Yes	Yes	No	30	55	85	Med	Med	Med	\$2,000		
2018700102	Davis & Biltmore	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	No	Yes	Yes	No	No	No	No	No	No	N/A	N/A	N/A	Yes	No	Yes	No	20	65	85	Low	Med	Med	\$2,000	
2021100293	Big Bend & Tuscany	Parallel	No	Yes	No	No	Yes	No	No	Yes	No	No	No	No	No	Yes	Yes	No	No	No	Yes	No	No	No	No	No	N/A	N/A	N/A	No	Yes	Yes	No	33	52	85	Med	Med	Med	\$2,000		
2021300285	Crestwood & Claverach	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	No	Yes	Yes	No	Yes	No	No	No	No	No	N/A	No	No	No	No	20	65	85	Low	Med	Med	\$2,000		
2003200638	Kingsbury & Bemiston	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	No	Yes	Yes	No	No	Yes	Yes	Yes	Yes	No	No	N/A	Yes	Yes	No	No	39	45	84	Med	Med	Med	\$2,000	
2011000736	Forsyth & Bemiston	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	No	Yes	Yes	Yes	Yes	Yes	Yes	Yes	No	Yes	Yes	N/A	No	Yes	Yes	No	69	15	84	High	Low	Med	\$2,000	
2011000742	Forsyth & Bemiston	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	Yes	Yes	Yes	Yes	Yes	No	Yes	Yes	Yes	No	No	N/A	No	Yes	Yes	Yes	69	15	84	High	Low	Med	\$2,000	
2017500175	Westwood & Parkdale	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	Yes	Yes	Yes	No	Yes	Yes	Yes	Yes	Yes	N/A	N/A	N/A	No	Yes	Yes	Yes	49	35	84	Med	Med	Med	\$2,000	
2019200004	Francis & Whitburn	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	No	No	Yes	No	No	No	No	No	No	Yes	No	N/A	No	Yes	No	No	24	60	84	Low	Med	Med	\$2,000	
2009100111	Polo & Middle Polo	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	No	Yes	Yes	No	Yes	No	No	No	No	N/A	N/A	N/A	Yes	Yes	Yes	No	29	55	84	Med	Med	Med	\$2,000	
2021400519	San Bonita & Seminary	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	No	Yes	Yes	No	Yes	No	No	No	No	No	N/A	No	Yes	Yes	Yes	44	40	84	Med	Med	Med	\$2,000		
2027600483	De Mun & Northwood	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	No	Yes	Yes	No	Yes	No	No	No	No	No	N/A	No	Yes	Yes	No	29	55	84	Med	Med	Med	\$2,000		
2003900844	Bonhomme & Central	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	No	Yes	N/A	No	Yes	Yes	Yes	73	10	83	High	Low	Med	\$2,000	
2005500711	Maryland & Meramec	Parallel	Yes	Yes	Yes	Yes	No	Yes	Yes	No	No	Yes	Yes	No	Yes	Yes	Yes	No	Yes	Yes	No	No	Yes	Yes	Yes	Yes	N/A	N/A	N/A	No	Yes	Yes	Yes	63	20	83	High	Low	Med	\$2,000		
2005500715	Maryland & Meramec	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	No	Yes	Yes	Yes	Yes	Yes	Yes	Yes	No	No	Yes	Yes	No	Yes	Yes	No	63	20	83	High	Low	Med	\$2,000	
2001500345	Brentwood & Venetian	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	No	Yes	Yes	Yes	No	Yes	Yes	Yes	Yes	N/A	N/A	N/A	Yes	Yes	Yes	Yes	63	20	83	High	Low	Med	\$2,000	
2012200549	Carswood & Shirley	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	Yes	No	No	No	No	No	No	No	No	No	N/A	N/A	N/A	No	Yes	Yes	No	23	60	83	Low	Med	Med	\$2,000
2016500128	Wydown & Edgewood	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	No	No	N/A	Yes	Yes	No	Yes	62	20	82	High	Low	Med	\$400
2000900357	Brentwood & Walinca	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	Yes	Yes	Yes	No	Yes	Yes	Yes	Yes	Yes	Yes	No	N/A	No	Yes	Yes	Yes	57	25	82	High	Low	Med	\$2,000	
2000900359	Brentwood & Walinca	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	No	Yes	Yes	Yes	No	Yes	Yes	Yes	Yes	No	Yes	N/A	Yes	Yes	Yes	No	57	25	82	High	Low	Med	\$2,000	
2017100161	Byron & Glen Ridge	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	No	Yes	No	Yes	No	Yes	Yes	Yes	Yes	No	No	N/A	Yes	Yes	Yes	No	37	45	82	Med	Med	Med	\$2,000	
2017600154	Westwood & Byron	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	Yes	Yes	Yes	Yes	Yes	Yes	No	Yes	Yes	Yes	No	No	N/A	Yes	Yes	Yes	No	52	30	82	High	Low	Med	\$2,000	
2009800528	Concordia & Private	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	No	Yes	Yes	No	No	No	No	No	No	N/A	N/A	N/A	No	Yes	Yes	Yes	42	40	82	Med	Med	Med	\$2,000	



City of Clayton ADA Transition Plan Curb Ramp Data Summary



1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22	23	24	25	26	27	28	29	30	31	32	33	34	35	36	37	38	39	40	41	42	
2011100670	Parkside & Brighton	Parallel	Yes	Yes	No	No	No	Yes	Yes	No	No	Yes	Yes	No	No	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	No	N/A	N/A	Yes	No	Yes	Yes	No	47	35	82	Med	Med	Med	\$2,000	
2015400462	University & Aberdeen	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	No	Yes	Yes	No	No	No	No	No	No	No	N/A	N/A	No	Yes	Yes	No	22	60	82	Low	Med	Med	\$2,000	
2016800426	University & Wydown	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	No	Yes	Yes	Yes	No	No	No	No	No	No	N/A	N/A	No	Yes	No	Yes	40	42	82	Med	Med	Med	\$2,000	
2023400492	De Mun & Rosebury	Parallel	Yes	No	No	Yes	Yes	No	Yes	Yes	No	Yes	Yes	No	No	Yes	Yes	Yes	No	Yes	Yes	No	No	No	No	No	No	N/A	N/A	N/A	Yes	No	No	Yes	32	50	82	Med	Med	Med	\$2,000	
2004900622	Central & Pershing	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	Yes	No	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	N/A	N/A	N/A	Yes	Yes	No	No	51	30	81	Med	Low	Med	\$2,000
2004900623	Central & Pershing	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	No	No	No	N/A	No	Yes	No	Yes	51	30	81	Med	Low	Med	\$2,000
2007300680	Maryland & Lancaster	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	No	Yes	Yes	Yes	No	No	No	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	No	36	45	81	Med	Med	Med	\$2,000	
2019700188	Westwood & Wellington	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	Yes	Yes	Yes	No	No	Yes	No	Yes	Yes	No	No	N/A	No	Yes	Yes	No	36	45	81	Med	Med	Med	\$2,000	
2019700191	Westwood & Wellington	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	Yes	Yes	Yes	No	Yes	Yes	Yes	Yes	No	No	No	N/A	No	Yes	Yes	No	36	45	81	Med	Med	Med	\$2,000	
2000600605	Brentwood & Kingsbury	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	No	Yes	Yes	No	No	Yes	Yes	Yes	Yes	Yes	No	No	N/A	Yes	Yes	Yes	No	41	40	81	Med	Med	Med	\$2,000
2004800571	Brentwood & Pershing	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	Yes	Yes	Yes	Yes	Yes	No	No	No	No	N/A	N/A	N/A	Yes	Yes	Yes	No	41	40	81	Med	Med	Med	\$2,000	
2005300576	Brentwood & University	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	Yes	Yes	Yes	No	Yes	Yes	Yes	Yes	Yes	No	No	N/A	No	Yes	Yes	Yes	56	25	81	High	Low	Med	\$2,000	
2017400165	Parkdale & Glen Ridge	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	Yes	Yes	Yes	Yes	Yes	No	Yes	Yes	Yes	Yes	N/A	N/A	N/A	Yes	Yes	Yes	No	40	40	80	Med	Med	Med	\$2,000
2017400166	Parkdale & Glen Ridge	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	No	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	No	No	N/A	Yes	Yes	Yes	No	40	40	80	Med	Med	Med	\$2,000
2017400167	Parkdale & Glen Ridge	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	Yes	Yes	Yes	No	Yes	Yes	Yes	Yes	No	N/A	N/A	N/A	No	Yes	Yes	No	40	40	80	Med	Med	Med	\$2,000	
2017400170	Parkdale & Glen Ridge	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	No	Yes	Yes	Yes	No	Yes	Yes	Yes	Yes	N/A	N/A	N/A	No	Yes	Yes	No	40	40	80	Med	Med	Med	\$2,000	
2018100198	Glen Ridge & Buckingham	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	Yes	Yes	Yes	No	No	Yes	Yes	Yes	Yes	N/A	N/A	N/A	No	Yes	Yes	No	30	50	80	Med	Med	Med	\$2,000	
2031100816	Ritz Carlton & Colorado	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	No	No	No	No	No	No	No	No	No	No	No	No	N/A	No	Yes	Yes	Yes	30	50	80	Med	Med	Med	\$2,000
2000000647	Westmoreland	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	Yes	No	No	No	No	No	No	No	No	No	No	N/A	No	Yes	Yes	Yes	30	50	80	Med	Med	Med	\$2,000	
2001000024	Francis & Langton	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	No	Yes	Yes	No	Yes	No	No	No	No	No	N/A	N/A	N/A	Yes	Yes	Yes	No	20	60	80	Low	Med	Med	\$2,000
2009000107	Polo & Middle Polo	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	Yes	Yes	Yes	No	Yes	No	No	No	No	N/A	N/A	N/A	No	Yes	Yes	No	30	50	80	Med	Med	Med	\$2,000	
2009000108	Polo & Middle Polo	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	No	No	Yes	No	Yes	No	No	No	No	N/A	N/A	N/A	Yes	Yes	Yes	No	30	50	80	Med	Med	Med	\$2,000	
2010200533	Alamo & Private	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	Yes	No	Yes	Yes	Yes	Yes	No	No	No	No	No	No	N/A	No	Yes	Yes	Yes	50	30	80	Med	Low	Med	\$2,000	
2015300044	Brentwood & Watkins	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	No	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	No	N/A	No	Yes	Yes	No	53	27	80	High	Low	Med	\$2,000	



City of Clayton ADA Transition Plan
Curb Ramp Data Summary



1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22	23	24	25	26	27	28	29	30	31	32	33	34	35	36	37	38	39	40	41	42	
2016700435	Wydown & Shepley	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	No	Yes	Yes	No	Yes	No	No	No	No	No	No	N/A	Yes	Yes	Yes	Yes	30	50	80	Med	Med	Med	\$2,000	
2016800422	University & Wydown	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	No	Yes	Yes	No	No	No	No	No	No	No	N/A	No	Yes	No	Yes	40	40	80	Med	Med	Med	\$2,000		
2016800423	University & Wydown	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	Yes	Yes	Yes	No	No	No	No	No	No	Yes	Yes	N/A	Yes	Yes	Yes	No	40	40	80	Med	Med	Med	\$2,000	
2016800428	University & Wydown	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	No	Yes	Yes	No	No	No	No	No	No	No	N/A	No	Yes	Yes	Yes	40	40	80	Med	Med	Med	\$2,000		
2016800429	University & Wydown	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	Yes	Yes	Yes	No	Yes	No	No	No	No	No	N/A	No	Yes	Yes	Yes	40	40	80	Med	Med	Med	\$2,000		
2017700443	University & Wydown	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	No	Yes	Yes	Yes	Yes	No	No	N/A	N/A	N/A	N/A	N/A	No	Yes	No	No	30	50	80	Med	Med	Med	\$2,000	
2020100295	Big Bend & Oak Knoll	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	No	Yes	Yes	No	No	No	No	No	No	N/A	N/A	N/A	No	Yes	Yes	Yes	30	50	80	Med	Med	Med	\$2,000	
2020700279	Crestwood & Hillvale	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	No	Yes	Yes	No	Yes	No	No	No	No	N/A	N/A	N/A	Yes	No	Yes	No	20	60	80	Low	Med	Med	\$2,000	
2011000740	Forsyth & Bemiston	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	69	10	79	High	Low	Med	\$450	
2003200634	Kingsbury & Bemiston	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	No	Yes	Yes	No	No	Yes	Yes	Yes	Yes	No	Yes	Yes	Yes	Yes	No	No	39	40	79	Med	Med	Med	\$2,000	
2003200635	Kingsbury & Bemiston	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	No	Yes	Yes	No	No	Yes	Yes	Yes	Yes	N/A	N/A	Yes	Yes	Yes	No	No	39	40	79	Med	Med	Med	\$2,000	
2003200637	Kingsbury & Bemiston	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	Yes	No	Yes	Yes	No	Yes	Yes	Yes	Yes	Yes	No	No	N/A	No	Yes	No	No	39	40	79	Med	Med	Med	\$2,000	
2007600699	Brentwood & Forsyth	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	No	Yes	Yes	Yes	No	Yes	Yes	Yes	No	Yes	No	69	10	79	High	Low	Med	\$2,000	
2008000700	Maryland & Forsyth	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	No	No	N/A	Yes	Yes	Yes	Yes	64	15	79	High	Low	Med	\$2,000	
2017500176	Westwood & Parkdale	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	Yes	Yes	Yes	No	No	Yes	Yes	Yes	Yes	N/A	No	N/A	No	Yes	Yes	Yes	49	30	79	Med	Low	Med	\$2,000	
2024300759	Bemiston & Carondelet	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	No	Yes	No	Yes	No	Yes	Yes	Yes	No	Yes	Yes	Yes	No	No	No	59	20	79	High	Low	Med	\$2,000		
2008200048	Brentwood, Corporate Pk & Daytona	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	Yes	No	Yes	No	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	N/A	No	Yes	No	49	30	79	Med	Low	Med	\$2,000	
2008200050	Brentwood, Corporate Pk & Daytona	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	No	No	Yes	No	Yes	Yes	Yes	Yes	Yes	Yes	No	N/A	No	Yes	Yes	Yes	49	30	79	Med	Low	Med	\$2,000	
2021400518	San Bonita & Seminary	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	Yes	Yes	Yes	No	No	No	No	No	No	No	N/A	N/A	N/A	No	Yes	Yes	Yes	44	35	79	Med	Med	Med	\$2,000
2021400522	San Bonita & Seminary	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	Yes	No	Yes	Yes	Yes	No	No	No	No	No	No	No	N/A	No	Yes	Yes	Yes	44	35	79	Med	Med	Med	\$2,000	
2021700290	Big Bend & Tuscany	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	No	Yes	Yes	Yes	No	No	No	No	No	No	No	N/A	Yes	Yes	Yes	No	39	40	79	Med	Med	Med	\$2,000	
2030900821	Jackson & Maryland	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	No	Yes	Yes	No	No	No	No	No	No	No	N/A	N/A	N/A	No	Yes	Yes	Yes	29	50	79	Med	Med	Med	\$2,000
2005100628	Pershing & Bemiston	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	No	Yes	Yes	Yes	No	Yes	Yes	Yes	Yes	No	No	N/A	Yes	Yes	No	No	36	42	78	Med	Med	Med	\$2,000	
2005500714	Maryland & Meramec	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	No	No	Yes	Yes	No	No	63	15	78	High	Low	Med	\$2,000		



City of Clayton ADA Transition Plan Curb Ramp Data Summary



1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22	23	24	25	26	27	28	29	30	31	32	33	34	35	36	37	38	39	40	41	42	
2019500216	Glen Ridge & Wellington	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	Yes	Yes	Yes	No	Yes	Yes	Yes	Yes	No	N/A	N/A	N/A	No	Yes	Yes	No	33	45	78	Med	Med	Med	\$2,000	
2000900355	Brentwood & Walinca	Parallel	Yes	Yes	No	Yes	Yes	Yes	Yes	Yes	No	No	Yes	No	No	Yes	Yes	Yes	No	Yes	Yes	Yes	Yes	Yes	Yes	Yes	No	N/A	N/A	N/A	No	Yes	Yes	Yes	57	20	77	High	Low	Med	\$2,000	
2006700672	Gay & Ladue & Maryland	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	No	Yes	Yes	No	Yes	Yes	Yes	Yes	No	No	No	Yes	No	Yes	Yes	No	32	45	77	Med	Med	Med	\$2,000	
2016500132	Wydown & Edgewood	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	No	Yes	Yes	Yes	No	Yes	Yes	Yes	No	N/A	No	N/A	No	Yes	No	Yes	62	15	77	High	Low	Med	\$2,000	
2019000184	Westwood & Oxford	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	No	Yes	Yes	No	No	Yes	Yes	Yes	Yes	No	No	N/A	No	Yes	Yes	Yes	37	40	77	Med	Med	Med	\$2,000	
2008900667	Topton & Mark Twain	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	Yes	Yes	Yes	No	Yes	Yes	No	Yes	Yes	No	No	N/A	No	Yes	Yes	No	47	30	77	Med	Low	Med	\$2,000	
2016900420	Wydown & Wydown	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	No	Yes	Yes	No	Yes	No	No	No	No	No	Yes	N/A	N/A	No	Yes	No	No	37	40	77	Med	Med	Med	\$2,000
2020700280	Crestwood & Hillvale	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	Yes	Yes	Yes	No	Yes	No	No	No	No	No	No	N/A	No	Yes	No	No	20	57	77	Low	Med	Med	\$2,000	
2023400490	De Mun & Rosebury	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	Yes	Yes	No	No	No	No	No	No	No	No	No	Yes	N/A	Yes	No	No	Yes	32	45	77	Med	Med	Med	\$2,000	
2025000477	De Mun & Arundel	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	No	Yes	Yes	Yes	Yes	Yes	No	No	No	No	N/A	No	N/A	No	Yes	No	No	27	50	77	Med	Med	Med	\$2,000
2025000479	De Mun & Arundel	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	No	Yes	Yes	Yes	No	No	No	No	No	N/A	No	N/A	Yes	Yes	No	No	27	50	77	Med	Med	Med	\$2,000	
2005100631	Pershing & Bemiston	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	Yes	No	Yes	Yes	No	No	Yes	Yes	Yes	Yes	No	No	N/A	No	Yes	No	Yes	36	40	76	Med	Med	Med	\$2,000	
2004800569	Brentwood & Pershing	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	No	Yes	Yes	No	No	Yes	Yes	Yes	Yes	No	No	N/A	No	Yes	No	No	41	35	76	Med	Med	Med	\$2,000	
2005300577	Brentwood & University	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	Yes	Yes	Yes	No	Yes	Yes	Yes	Yes	Yes	N/A	N/A	N/A	No	Yes	Yes	Yes	56	20	76	High	Low	Med	\$2,000	
2009100110	Polo & Middle Polo	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	No	Yes	Yes	Yes	No	No	No	No	No	No	No	N/A	Yes	Yes	No	No	29	47	76	Med	Med	Med	\$2,000	
2013800130	Edgewood & Walinca	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	No	N/A	No	Yes	No	Yes	46	30	76	Med	Low	Med	\$2,000	
2024700496	De Mun & Southwood	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	Yes	No	No	No	No	No	No	No	No	Yes	Yes	N/A	No	Yes	No	Yes	36	40	76	Med	Med	Med	\$2,000	
2003300643	Kingsbury & Central	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	No	Yes	Yes	No	No	Yes	Yes	Yes	Yes	No	No	N/A	No	Yes	No	No	35	40	75	Med	Med	Med	\$2,000	
2007500682	Topton & Maryland	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	Yes	Yes	Yes	No	Yes	Yes	Yes	Yes	No	No	No	Yes	Yes	Yes	Yes	No	45	30	75	Med	Low	Med	\$2,000	
2007500684	Topton & Maryland	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	Yes	Yes	Yes	No	Yes	Yes	Yes	Yes	No	No	Yes	N/A	No	Yes	No	No	45	30	75	Med	Low	Med	\$2,000	
2015700148	Wydown & Asbury	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	No	Yes	Yes	No	No	Yes	Yes	Yes	No	No	No	N/A	No	Yes	No	No	20	55	75	Low	Med	Med	\$2,000	
2017400164	Parkdale & Glen Ridge	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	No	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	No	No	N/A	No	Yes	No	Yes	40	35	75	Med	Med	Med	\$2,000	
2017400171	Parkdale & Glen Ridge	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	No	Yes	Yes	Yes	Yes	Yes	Yes	Yes	No	No	No	N/A	No	Yes	Yes	No	40	35	75	Med	Med	Med	\$2,000	
2018100199	Glen Ridge & Buckingham	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	No	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	No	No	N/A	No	Yes	Yes	No	30	45	75	Med	Med	Med	\$2,000	



City of Clayton ADA Transition Plan Curb Ramp Data Summary



1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22	23	24	25	26	27	28	29	30	31	32	33	34	35	36	37	38	39	40	41	42	
2018300178	Westwood & Buckingham	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	Yes	Yes	Yes	No	No	Yes	Yes	Yes	No	No	No	N/A	No	Yes	Yes	Yes	40	35	75	Med	Med	Med	\$2,000	
2018300179	Westwood & Buckingham	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	Yes	Yes	Yes	No	Yes	Yes	Yes	Yes	Yes	N/A	N/A	N/A	No	Yes	Yes	Yes	40	35	75	Med	Med	Med	\$2,000	
2019500215	Glen Ridge & Wellington	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	Yes	No	Yes	Yes	No	Yes	Yes	Yes	Yes	No	No	No	N/A	No	Yes	Yes	No	33	42	75	Med	Med	Med	\$2,000	
2019500218	Glen Ridge & Wellington	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	No	Yes	Yes	No	Yes	Yes	Yes	Yes	Yes	N/A	N/A	N/A	No	Yes	Yes	Yes	33	42	75	Med	Med	Med	\$2,000	
2008100342	Brentwood & Davis	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	No	Yes	Yes	Yes	No	Yes	Yes	Yes	Yes	N/A	N/A	N/A	No	Yes	Yes	No	45	30	75	Med	Low	Med	\$1,600	
2000000648	Westmoreland	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	Yes	Yes	Yes	Yes	No	Yes	No	No	No	No	No	No	N/A	No	Yes	No	Yes	30	45	75	Med	Med	Med	\$2,000	
2012200547	Carswood & Shirley	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	No	No	No	No	No	No	No	No	No	N/A	N/A	N/A	Yes	Yes	Yes	No	23	52	75	Low	Med	Med	\$2,000	
2017700444	University & Wydown	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	No	Yes	Yes	No	No	No	No	No	No	N/A	No	N/A	No	Yes	No	Yes	30	45	75	Med	Med	Med	\$2,000	
2018600453	Arundel & University	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	Yes	Yes	Yes	No	Yes	No	No	No	No	N/A	No	N/A	No	Yes	No	No	20	55	75	Low	Med	Med	\$2,000	
2025100473	De Mun & Aberdeen	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	Yes	Yes	Yes	No	No	No	No	No	No	N/A	No	N/A	No	No	Yes	No	20	55	75	Low	Med	Med	\$2,000	
2011000737	Forsyth & Bemiston	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	No	Yes	Yes	Yes	Yes	Yes	No	N/A	Yes	Yes	Yes	Yes	69	5	74	High	Low	Med	\$600
2000000152	Forest	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	No	No	No	N/A	No	Yes	No	Yes	49	25	74	Med	Low	Med	\$2,000
2006700677	Gay & Ladue & Maryland	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	Yes	No	No	No	No	Yes	Yes	Yes	No	No	Yes	Yes	No	Yes	No	Yes	32	42	74	Med	Med	Med	\$2,000	
2008000701	Maryland & Forsyth	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	Yes	Yes	Yes	Yes	No	Yes	Yes	Yes	Yes	No	No	Yes	Yes	Yes	Yes	Yes	64	10	74	High	Low	Med	\$2,000	
2008000704	Maryland & Forsyth	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	No	Yes	N/A	No	Yes	Yes	Yes	64	10	74	High	Low	Med	\$2,000	
2008000706	Maryland & Forsyth	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	Yes	No	Yes	Yes	Yes	Yes	Yes	Yes	No	Yes	Yes	Yes	Yes	Yes	No	64	10	74	High	Low	Med	\$2,000		
2010800769	Meramec & Forsyth	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	No	Yes	No	N/A	No	Yes	No	Yes	69	5	74	High	Low	Med	\$2,000	
2011000739	Forsyth & Bemiston	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	No	Yes	Yes	Yes	No	Yes	Yes	Yes	Yes	Yes	Yes	Yes	N/A	Yes	Yes	Yes	Yes	69	5	74	High	Low	Med	\$2,000
2011000741	Forsyth & Bemiston	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	Yes	Yes	Yes	Yes	Yes	Yes	No	Yes	No	Yes	Yes	N/A	No	Yes	Yes	Yes	69	5	74	High	Low	Med	\$2,000	
2024300757	Bemiston & Carondelet	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	Yes	Yes	Yes	Yes	Yes	No	No	No	No	No	Yes	Yes	N/A	Yes	Yes	Yes	59	15	74	High	Low	Med	\$2,000	
2024300760	Bemiston & Carondelet	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	No	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	No	No	59	15	74	High	Low	Med	\$2,000	
2025800302	Big Bend & Forest Ridge	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	No	Yes	Yes	No	No	No	No	No	No	No	No	No	N/A	No	Yes	Yes	No	24	50	74	Low	Med	Med	\$2,000
2025800304	Big Bend & Forest Ridge	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	No	Yes	Yes	No	No	No	No	No	No	No	No	No	N/A	No	Yes	Yes	No	24	50	74	Low	Med	Med	\$2,000
2008200046	Brentwood, Corporate Pk & Daytona	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	Yes	Yes	No	No	Yes	Yes	Yes	Yes	No	No	No	N/A	No	Yes	Yes	Yes	49	25	74	Med	Low	Med	\$2,000	



City of Clayton ADA Transition Plan Curb Ramp Data Summary



1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22	23	24	25	26	27	28	29	30	31	32	33	34	35	36	37	38	39	40	41	42	
2012400117	Shirley & Polo & Rear	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	Yes	Yes	Yes	No	Yes	No	No	No	No	No	No	Yes	N/A	No	Yes	Yes	Yes	44	30	74	Med	Low	Med	\$2,000
2021400523	San Bonita & Seminary	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	Yes	Yes	Yes	No	Yes	No	No	No	No	No	No	N/A	No	Yes	Yes	Yes	44	30	74	Med	Low	Med	\$2,000	
2021700291	Big Bend & Tuscany	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	No	Yes	Yes	Yes	No	No	No	No	No	N/A	N/A	N/A	Yes	Yes	Yes	Yes	39	35	74	Med	Med	Med	\$2,000	
2030900822	Jackson & Maryland	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	Yes	Yes	Yes	No	Yes	No	No	No	No	No	N/A	No	Yes	No	No	29	45	74	Med	Med	Med	\$2,000		
2004100778	Bonhomme & Bemiston	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	Yes	Yes	Yes	No	No	Yes	Yes	Yes	No	Yes	Yes	Yes	No	No	53	20	73	High	Low	Med	\$2,000			
2005500713	Maryland & Meramec	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	Yes	Yes	Yes	No	Yes	Yes	Yes	Yes	No	Yes	Yes	N/A	No	Yes	No	Yes	63	10	73	High	Low	Med	\$2,000	
2007800692	Forsyth & Parkside	Parallel	Yes	Yes	Yes	Yes	No	Yes	Yes	Yes	Yes	Yes	Yes	No	Yes	Yes	Yes	Yes	Yes	Yes	No	No	No	Yes	Yes	Yes	Yes	Yes	N/A	N/A	N/A	No	Yes	Yes	Yes	63	10	73	High	Low	Med	\$2,000
2007800693	Forsyth & Parkside	Parallel	Yes	Yes	Yes	Yes	No	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	No	Yes	Yes	Yes	Yes	Yes	Yes	Yes	N/A	N/A	N/A	No	Yes	Yes	Yes	63	10	73	High	Low	Med	\$2,000
2011400807	Carondelet, Lyle & Colorado	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	No	Yes	Yes	No	No	No	No	No	No	No	Yes	No	Yes	No	Yes	36	37	73	Med	Med	Med	\$2,000		
2002700616	Brentwood & Clayton Lane	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	No	No	Yes	No	Yes	No	No	No	No	N/A	N/A	N/A	Yes	Yes	Yes	No	13	60	73	Low	Med	Med	\$2,000	
2012700538	Hanley & Rear	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	No	Yes	No	Yes	No	No	No	No	No	Yes	Yes	N/A	No	Yes	Yes	No	33	40	73	Med	Med	Med	\$2,000	
2015300045	Brentwood & Watkins	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	Yes	Yes	Yes	No	Yes	Yes	Yes	Yes	Yes	No	Yes	N/A	No	Yes	Yes	Yes	53	20	73	High	Low	Med	\$2,000	
2016500126	Wydown & Edgewood	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	N/A	No	N/A	No	Yes	Yes	Yes	62	10	72	High	Low	Med	\$400
2000900358	Brentwood & Walinca	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	Yes	Yes	Yes	Yes	No	Yes	Yes	Yes	Yes	Yes	No	N/A	No	Yes	No	Yes	57	15	72	High	Low	Med	\$2,000	
2006700676	Gay & Ladue & Maryland	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	Yes	No	No	No	No	Yes	Yes	Yes	No	No	Yes	N/A	Yes	Yes	Yes	Yes	32	40	72	Med	Med	Med	\$2,000	
2015700147	Wydown & Asbury	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	No	Yes	Yes	No	No	No	No	No	No	N/A	N/A	N/A	No	Yes	Yes	No	20	52	72	Low	Med	Med	\$2,000	
2017100156	Byron & Glen Ridge	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	No	Yes	Yes	Yes	No	Yes	Yes	Yes	Yes	No	No	N/A	No	Yes	Yes	Yes	37	35	72	Med	Med	Med	\$2,000	
2017100162	Byron & Glen Ridge	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	N/A	N/A	N/A	No	Yes	Yes	No	37	35	72	Med	Med	Med	\$2,000
2017400169	Parkdale & Glen Ridge	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	N/A	N/A	N/A	Yes	Yes	Yes	No	40	32	72	Med	Low	Med	\$2,000
2015400457	University & Aberdeen	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	No	Yes	Yes	No	No	No	No	No	No	No	N/A	No	N/A	No	Yes	No	Yes	22	50	72	Low	Med	Med	\$2,000
2015400459	University & Aberdeen	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	No	Yes	Yes	No	No	No	No	No	No	No	N/A	No	N/A	No	Yes	Yes	No	22	50	72	Low	Med	Med	\$2,000
2018700103	Davis & Biltmore	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	No	Yes	Yes	No	No	No	No	No	No	No	No	Yes	N/A	Yes	Yes	Yes	No	20	52	72	Low	Med	Med	\$2,000
2025000478	De Mun & Arundel	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	Yes	Yes	Yes	No	Yes	No	No	No	No	No	N/A	N/A	No	Yes	Yes	No	27	45	72	Med	Med	Med	\$2,000	
2000300235	Cromwell & Glen Ridge	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	No	Yes	Yes	No	No	Yes	Yes	Yes	No	No	No	N/A	No	Yes	Yes	Yes	36	35	71	Med	Med	Med	\$2,000	



City of Clayton ADA Transition Plan
Curb Ramp Data Summary



1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22	23	24	25	26	27	28	29	30	31	32	33	34	35	36	37	38	39	40	41	42	
2006900678	Maryland & Crandon	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	No	Yes	Yes	Yes	Yes	No	No	No	No	No	No	N/A	No	Yes	Yes	Yes	Yes	36	35	71	Med	Med	Med	\$2,000
2011400805	Carondelet, Lyle & Colorado	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	Yes	Yes	No	No	Yes	No	No	No	No	No	N/A	Yes	Yes	Yes	Yes	Yes	36	35	71	Med	Med	Med	\$2,000	
2019900193	Westwood & York	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	No	No	No	N/A	No	Yes	Yes	No	36	35	71	Med	Med	Med	\$2,000	
2009100113	Polo & Middle Polo	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	No	Yes	Yes	Yes	No	No	No	No	No	N/A	N/A	N/A	No	Yes	Yes	No	29	42	71	Med	Med	Med	\$2,000	
2021000238	Audubon & Somers	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	No	Yes	Yes	No	Yes	Yes	Yes	Yes	No	N/A	N/A	N/A	No	Yes	Yes	No	16	55	71	Low	Med	Med	\$2,000	
2024800494	De Mun & San Bonita	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	No	No	No	No	No	Yes	Yes	N/A	Yes	No	Yes	No	41	30	71	Med	Low	Med	\$2,000
2025300464	De Mun & Wydown	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	Yes	Yes	Yes	Yes	Yes	Yes	No	No	No	No	No	N/A	Yes	Yes	No	No	26	45	71	Low	Med	Med	\$2,000	
2030700537	Bland & Northmoor	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	Yes	Yes	No	No	Yes	No	No	No	No	N/A	N/A	N/A	No	Yes	No	Yes	16	55	71	Low	Med	Med	\$2,000	
2017400168	Parkdale & Glen Ridge	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	No	No	No	N/A	No	Yes	Yes	No	40	30	70	Med	Low	Med	\$2,000
2018100203	Glen Ridge & Buckingham	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	No	N/A	N/A	N/A	No	Yes	Yes	No	30	40	70	Med	Med	Med	\$2,000
2018300180	Westwood & Buckingham	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	Yes	Yes	Yes	Yes	Yes	No	Yes	Yes	Yes	Yes	N/A	N/A	N/A	No	Yes	Yes	Yes	40	30	70	Med	Low	Med	\$2,000
2023800833	Forsyth, Jackson & Carondelet	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	Yes	No	No	No	No	No	Yes	Yes	Yes	No	No	No	N/A	No	Yes	Yes	No	25	45	70	Low	Med	Med	\$2,000
2023800835	Forsyth, Jackson & Carondelet	Island Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	Yes	No	No	No	No	Yes	No	Yes	Yes	No	No	N/A	Yes	Yes	Yes	No	25	45	70	Low	Med	Med	\$2,000	
2008100343	Brentwood & Davis	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	No	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	No	N/A	No	Yes	Yes	No	45	25	70	Med	Low	Med	\$1,800	
2000700615	Brentwood & Stratford	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	No	Yes	Yes	No	Yes	Yes	Yes	Yes	Yes	Yes	No	No	N/A	No	Yes	Yes	No	25	45	70	Low	Med	Med	\$2,000
2000800067	Walnca & Meramec	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	No	No	No	N/A	No	Yes	Yes	No	50	20	70	Med	Low	Med	\$2,000
2015800104	Polo & Polo	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	Yes	Yes	Yes	No	Yes	No	No	No	No	N/A	N/A	N/A	No	Yes	No	Yes	30	40	70	Med	Med	Med	\$2,000	
2015800105	Polo & Polo	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	No	Yes	Yes	Yes	No	No	No	No	No	No	N/A	N/A	N/A	No	Yes	Yes	Yes	30	40	70	Med	Med	Med	\$2,000
2016700434	Wydown & Shepley	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	No	Yes	Yes	No	Yes	No	No	No	No	N/A	N/A	N/A	Yes	Yes	Yes	Yes	30	40	70	Med	Med	Med	\$2,000	
2018600451	Arundel & University	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	Yes	Yes	Yes	No	Yes	No	No	No	No	No	No	N/A	No	Yes	No	Yes	20	50	70	Low	Med	Med	\$2,000	
2018600452	Arundel & University	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	No	Yes	Yes	Yes	No	No	No	No	No	N/A	N/A	Yes	Yes	No	No	20	50	70	Low	Med	Med	\$2,000		
2025100468	De Mun & Aberdeen	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	No	Yes	Yes	No	Yes	No	No	No	No	No	N/A	N/A	Yes	Yes	Yes	No	20	50	70	Low	Med	Med	\$2,000	
2025700300	Big Bend & Southmoor	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	No	Yes	Yes	No	No	No	No	No	No	N/A	No	N/A	No	No	Yes	Yes	20	50	70	Low	Med	Med	\$2,000	
2026500474	De Mun & Arundel	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	No	Yes	Yes	Yes	No	No	No	No	No	No	N/A	N/A	No	Yes	No	No	30	40	70	Med	Med	Med	\$2,000	



City of Clayton ADA Transition Plan

Curb Ramp Data Summary



1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22	23	24	25	26	27	28	29	30	31	32	33	34	35	36	37	38	39	40	41	42
2026500481	De Mun & Arundel	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	Yes	Yes	Yes	Yes	No	No	No	No	No	N/A	No	N/A	No	Yes	Yes	No	30	40	70	Med	Med	Med	\$2,000
2003200636	Kingsbury & Bemiston	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	No	No	N/A	Yes	Yes	Yes	No	39	30	69	Med	Low	Med	\$2,000
2003200639	Kingsbury & Bemiston	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	No	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	No	No	N/A	No	Yes	No	Yes	39	30	69	Med	Low	Med	\$2,000
2011000735	Forsyth & Bemiston	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	Yes	Yes	Yes	Yes	No	Yes	Yes	Yes	No	Yes	Yes	N/A	No	Yes	No	Yes	69	0	69	High	Low	Med	\$2,000
2013100367	Big Bend & Shepley	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	No	No	No	No	No	No	No	No	No	No	No	N/A	No	Yes	Yes	Yes	14	55	69	Low	Med	Med	\$2,000
2018900206	Oxford & Glen Ridge	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	No	Yes	Yes	Yes	Yes	Yes	Yes	Yes	No	N/A	N/A	N/A	No	Yes	Yes	No	29	40	69	Med	Med	Med	\$2,000
2018900211	Oxford & Glen Ridge	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	Yes	No	Yes	No	Yes	Yes	Yes	Yes	No	N/A	N/A	N/A	No	Yes	Yes	No	29	40	69	Med	Med	Med	\$2,000
2019200014	Francis & Whitburn	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	Yes	No	No	No	No	No	No	No	No	No	No	N/A	No	No	Yes	Yes	24	45	69	Low	Med	Med	\$2,000
2022600005	Brentwood & Whitburn	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	No	Yes	Yes	No	No	Yes	Yes	Yes	Yes	N/A	N/A	N/A	No	Yes	Yes	No	24	45	69	Low	Med	Med	\$2,000
2022600012	Brentwood & Whitburn	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	No	Yes	Yes	No	No	Yes	Yes	Yes	Yes	No	No	N/A	No	Yes	Yes	No	24	45	69	Low	Med	Med	\$2,000
2025800303	Big Bend & Forest Ridge	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	No	Yes	Yes	No	No	No	No	No	No	N/A	N/A	N/A	Yes	Yes	Yes	No	24	45	69	Low	Med	Med	\$2,000
2025800305	Big Bend & Forest Ridge	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	No	Yes	Yes	Yes	No	No	No	No	No	No	No	N/A	No	Yes	Yes	No	24	45	69	Low	Med	Med	\$2,000
2008200047	Brentwood, Corporate Pk & Daytona	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	Yes	Yes	Yes	No	Yes	Yes	Yes	Yes	No	No	Yes	N/A	No	Yes	Yes	Yes	49	20	69	Med	Low	Med	\$2,000
2010700559	Maryland & Lee	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	No	Yes	Yes	Yes	Yes	Yes	No	No	No	No	No	N/A	No	Yes	Yes	Yes	44	25	69	Med	Low	Med	\$2,000
2023300489	De Mun & Rosebury	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	Yes	Yes	Yes	No	Yes	No	No	No	No	No	Yes	N/A	Yes	Yes	Yes	Yes	32	37	69	Med	Med	Med	\$2,000
2027700484	De Mun & Arundel	Parallel	Yes	Yes	No	No	Yes	Yes	Yes	Yes	Yes	Yes	Yes	No	No	Yes	Yes	Yes	Yes	Yes	Yes	Yes	No	No	No	No	N/A	N/A	N/A	Yes	No	No	Yes	29	40	69	Med	Med	Med	\$2,000	
2030900819	Jackson & Maryland	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	No	Yes	Yes	No	No	No	No	No	No	No	No	N/A	No	Yes	No	Yes	29	40	69	Med	Med	Med	\$2,000
2004100779	Bonhomme & Bemiston	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	Yes	No	No	Yes	Yes	Yes	Yes	Yes	No	Yes	Yes	Yes	No	Yes	No	No	53	15	68	High	Low	Med	\$2,000
2011500815	Carondelet & Ritz Carlton	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	Yes	Yes	Yes	No	Yes	No	No	No	No	No	No	N/A	No	Yes	Yes	Yes	36	32	68	Med	Low	Med	\$2,000
2019500214	Glen Ridge & Wellington	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	No	N/A	N/A	N/A	No	No	Yes	No	33	35	68	Med	Med	Med	\$2,000
2020500095	Biltmore & Biltmore	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	No	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	N/A	N/A	N/A	No	Yes	No	No	26	42	68	Low	Med	Med	\$2,000
2021000237	Audubon & Somerset	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	Yes	Yes	Yes	No	Yes	Yes	Yes	Yes	Yes	No	No	N/A	Yes	Yes	Yes	No	16	52	68	Low	Med	Med	\$2,000
2006700675	Gay & Ladue & Maryland	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	Yes	No	Yes	No	Yes	No	No	No	No	No	No	N/A	Yes	Yes	Yes	Yes	32	35	67	Med	Med	Med	\$2,000
2029100831	Forsyth & Lyle	Parallel	Yes	Yes	Yes	Yes	No	Yes	Yes	No	Yes	Yes	Yes	No	Yes	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	No	42	25	67	Med	Low	Med	\$2,000



City of Clayton ADA Transition Plan Curb Ramp Data Summary



1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22	23	24	25	26	27	28	29	30	31	32	33	34	35	36	37	38	39	40	41	42	
2029100832	Forsyth & Lytle	Parallel	Yes	Yes	Yes	Yes	No	Yes	Yes	Yes	No	Yes	Yes	No	No	Yes	Yes	Yes	No	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	N/A	N/A	N/A	No	Yes	Yes	No	42	25	67	Med	Low	Med	\$2,000	
2012900065	Meramec & Rosiline	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	N/A	N/A	N/A	No	Yes	No	Yes	47	20	67	Med	Low	Med	\$450	
200000663	Topton	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	No	Yes	Yes	Yes	Yes	Yes	No	No	No	No	No	N/A	No	Yes	Yes	Yes	42	25	67	Med	Low	Med	\$2,000	
2014800830	Dartford & Aberdeen	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	Yes	Yes	Yes	Yes	Yes	Yes	No	No	No	N/A	N/A	N/A	No	Yes	No	No	22	45	67	Low	Med	Med	\$2,000	
2014900441	Wydown & Dartford	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	Yes	Yes	Yes	No	Yes	No	No	No	No	No	Yes	N/A	No	Yes	Yes	Yes	32	35	67	Med	Med	Med	\$2,000	
2015400456	University & Aberdeen	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	No	Yes	Yes	No	No	No	No	No	No	No	N/A	N/A	No	Yes	Yes	Yes	22	45	67	Low	Med	Med	\$2,000	
2016900421	Wydown & Wydown	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	Yes	Yes	Yes	No	Yes	No	No	No	No	N/A	Yes	N/A	Yes	Yes	Yes	No	37	30	67	Med	Low	Med	\$2,000	
2021300283	Crestwood & Claverach	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	Yes	Yes	Yes	No	Yes	No	No	No	No	No	N/A	No	Yes	Yes	No	20	47	67	Low	Med	Med	\$2,000		
2023400491	De Mun & Rosebury	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	Yes	Yes	Yes	No	Yes	Yes	Yes	Yes	No	Yes	No	N/A	Yes	Yes	Yes	No	32	35	67	Med	Med	Med	\$2,000	
2025000475	De Mun & Arundel	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	No	Yes	Yes	No	No	No	No	No	No	N/A	No	N/A	No	Yes	Yes	Yes	27	40	67	Med	Med	Med	\$2,000	
2011200823	Forsyth & Lee	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	No	Yes	N/A	No	No	Yes	No	36	30	66	Med	Low	Med	\$450
2000300232	Cromwell & Glen Ridge	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	Yes	No	Yes	Yes	No	Yes	Yes	Yes	Yes	Yes	No	N/A	N/A	N/A	No	Yes	Yes	Yes	36	30	66	Med	Low	Med	\$2,000
2000300233	Cromwell & Glen Ridge	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	No	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	No	No	N/A	No	Yes	Yes	Yes	36	30	66	Med	Low	Med	\$2,000	
2004900625	Central & Pershing	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	No	No	N/A	Yes	Yes	Yes	Yes	51	15	66	Med	Low	Med	\$2,000
2005100626	Pershing & Bemiston	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	No	No	N/A	Yes	Yes	Yes	No	36	30	66	Med	Low	Med	\$2,000
2005100627	Pershing & Bemiston	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	No	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	No	No	N/A	Yes	Yes	Yes	Yes	36	30	66	Med	Low	Med	\$2,000
2005100632	Pershing & Bemiston	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	No	Yes	Yes	No	Yes	Yes	Yes	Yes	Yes	No	No	N/A	Yes	Yes	Yes	Yes	36	30	66	Med	Low	Med	\$2,000	
2006100227	Glen Ridge & York	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	Yes	Yes	Yes	No	Yes	Yes	Yes	Yes	No	No	No	N/A	No	Yes	Yes	Yes	36	30	66	Med	Low	Med	\$2,000	
2011400806	Carondelet, Lytle & Colorado	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	No	Yes	Yes	Yes	No	No	No	No	No	No	No	Yes	Yes	Yes	No	Yes	36	30	66	Med	Low	Med	\$2,000	
2011400808	Carondelet, Lytle & Colorado	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	Yes	Yes	Yes	No	Yes	Yes	Yes	Yes	No	Yes	No	N/A	Yes	Yes	Yes	No	36	30	66	Med	Low	Med	\$2,000	
2011400809	Carondelet, Lytle & Colorado	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	Yes	Yes	Yes	No	Yes	Yes	Yes	Yes	No	Yes	No	N/A	Yes	Yes	Yes	No	36	30	66	Med	Low	Med	\$2,000	
2026300817	Ritz Carlton & Carondelet	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	Yes	Yes	Yes	No	No	Yes	Yes	Yes	Yes	Yes	No	N/A	Yes	Yes	Yes	No	36	30	66	Med	Low	Med	\$2,000	
2000600606	Brentwood & Kingsbury	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	No	Yes	Yes	No	No	Yes	Yes	Yes	Yes	Yes	N/A	N/A	N/A	No	Yes	Yes	Yes	41	25	66	Med	Low	Med	\$2,000
2008600252	Clayton & Somerset	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	Yes	No	Yes	No	Yes	No	No	No	No	No	No	N/A	N/A	N/A	No	Yes	Yes	Yes	26	40	66	Low	Med	Med	\$2,000



City of Clayton ADA Transition Plan Curb Ramp Data Summary



1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22	23	24	25	26	27	28	29	30	31	32	33	34	35	36	37	38	39	40	41	42		
2025300465	De Mun & Wydown	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	No	Yes	Yes	Yes	Yes	No	No	No	No	No	No	N/A	No	Yes	Yes	No		26	40	66	Low	Med	Med	\$2,000	
2027600487	De Mun & Northwood	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	No	Yes	Yes	No	Yes	No	No	No	No	Yes	Yes	N/A	Yes	Yes	No	Yes		29	37	66	Med	Med	Med	\$2,000	
2019500212	Glen Ridge & Wellington	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	Yes	No	Yes	Yes	Yes	No	Yes	Yes	Yes	No	No	No	N/A	No	Yes	No	Yes		33	32	65	Med	Low	Med	\$2,000	
2023800834	Forsyth, Jackson & Carondelet	Island Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	Yes	No	No	No	No	Yes	Yes	Yes	Yes	Yes	No	N/A	No	Yes	No	No		25	40	65	Low	Med	Med	\$2,000	
2031100812	Ritz Carlton & Colorado	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	Yes	Yes	No	Yes	No	No	No	No	No	Yes	No	N/A	Yes	Yes	Yes	No		30	35	65	Med	Med	Med	\$2,000	
2000000408	Ellenwood	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	Yes	Yes	Yes	No	No	No	No	No	No	No	No	N/A	No	Yes	No	No		10	55	65	Low	Med	Med	\$2,000	
2000700614	Brentwood & Stratford	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	No	Yes	Yes	No	No	Yes	Yes	Yes	Yes	N/A	N/A	N/A	No	Yes	Yes	No		25	40	65	Low	Med	Med	\$2,000	
2001000023	Francis & Langton	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	Yes	Yes	Yes	Yes	No	Yes	No	No	No	No	No	No	N/A	Yes	Yes	No	Yes		20	45	65	Low	Med	Med	\$2,000	
2003500607	Kingsbury & Meramec	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	No	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	No	No	N/A	Yes	Yes	Yes	No		40	25	65	Med	Low	Med	\$2,000
2017000273	Audubon & Byron	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	No	Yes	Yes	No	Yes	Yes	Yes	Yes	Yes	Yes	N/A	N/A	N/A	No	Yes	Yes	No		10	55	65	Low	Med	Med	\$2,000
2017700445	University & Wydown	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	No	Yes	Yes	Yes	No	No	No	No	No	No	N/A	N/A	No	Yes	No	Yes		30	35	65	Med	Med	Med	\$2,000	
2017700446	University & Wydown	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	Yes	Yes	Yes	No	Yes	No	No	No	No	No	N/A	N/A	N/A	No	Yes	Yes	Yes		30	35	65	Med	Med	Med	\$2,000
2025100470	De Mun & Aberdeen	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	No	Yes	Yes	Yes	No	No	No	No	No	No	N/A	N/A	No	Yes	No	Yes		20	45	65	Low	Med	Med	\$2,000	
2025100471	De Mun & Aberdeen	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	No	Yes	Yes	No	Yes	No	No	No	No	N/A	No	N/A	No	Yes	Yes	Yes		20	45	65	Low	Med	Med	\$2,000	
2008000703	Maryland & Forsyth	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	Yes	No	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	N/A	No	Yes	No	Yes		64	0	64	High	Low	Med	\$650	
2004300400	Ellenwood & University	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	Yes	No	Yes	Yes	Yes	No	No	No	No	No	No	No	N/A	No	Yes	No	Yes		34	30	64	Med	Low	Med	\$2,000	
2018900204	Oxford & Glen Ridge	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	No	N/A	N/A	N/A	No	Yes	Yes	No		29	35	64	Med	Med	Med	\$2,000
2018900208	Oxford & Glen Ridge	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	Yes	Yes	No	Yes	Yes	Yes	Yes	Yes	Yes	No	N/A	N/A	N/A	No	Yes	Yes	No		29	35	64	Med	Med	Med	\$2,000
2019200033	Francis & Whitburn	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A		24	40	64	Low	Med	Med	\$2,000	
2022500002	Brentwood & Francis	Parallel	No	No	No	No	Yes	No	Yes	Yes	No	Yes	Yes	No	No	Yes	Yes	No	No	No	No	No	No	No	No	No	No	N/A	N/A	N/A	Yes	Yes	Yes	No		24	40	64	Low	Med	Med	\$2,000	
2008200340	Brentwood, Corporate Pk & Daytona	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	Yes	No	No	Yes	No	Yes	Yes	Yes	Yes	Yes	No	No	N/A	Yes	Yes	Yes	Yes		49	15	64	Med	Low	Med	\$600
2014800828	Dartford & Aberdeen	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	No	Yes	Yes	Yes	No	No	No	No	No	No	N/A	N/A	No	Yes	No	Yes		22	42	64	Low	Med	Med	\$2,000	
2020000296	Big Bend & Southmoor	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	Yes	Yes	Yes	No	No	No	No	No	No	No	N/A	N/A	No	Yes	Yes	No		19	45	64	Low	Med	Med	\$2,000	
2025000476	De Mun & Arundel	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	No	Yes	Yes	Yes	Yes	Yes	No	No	No	No	N/A	N/A	No	Yes	No	Yes		27	37	64	Med	Med	Med	\$2,000	



City of Clayton ADA Transition Plan
Curb Ramp Data Summary



1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22	23	24	25	26	27	28	29	30	31	32	33	34	35	36	37	38	39	40	41	42	
2027700485	De Mun & Arundel	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	Yes	Yes	Yes	Yes	No	No	No	No	No	No	No	No	N/A	N/A	No	Yes	Yes	Yes	29	35	64	Med	Med	Med	\$2,000
2027800416	Wydown & Wydown Terrace	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	Yes	Yes	Yes	No	No	Yes	Yes	Yes	Yes	No	N/A	N/A	No	Yes	No	Yes	29	35	64	Med	Med	Med	\$2,000	
2027800418	Wydown & Wydown Terrace	Island Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	No	Yes	Yes	No	Yes	Yes	Yes	Yes	Yes	N/A	N/A	N/A	No	Yes	No	Yes	29	35	64	Med	Med	Med	\$2,000	
2004100780	Bonhomme & Bemiston	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	Yes	Yes	Yes	No	Yes	Yes	Yes	Yes	No	Yes	Yes	Yes	No	Yes	No	Yes	53	10	63	High	Low	Med	\$2,000	
2004100781	Bonhomme & Bemiston	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	Yes	Yes	Yes	No	Yes	Yes	Yes	Yes	No	Yes	Yes	N/A	Yes	Yes	Yes	No	Yes	53	10	63	High	Low	Med	\$2,000
2019500219	Glen Ridge & Wellington	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	Yes	Yes	Yes	No	No	Yes	Yes	Yes	No	N/A	N/A	N/A	No	Yes	Yes	Yes	33	30	63	Med	Low	Med	\$2,000	
2000000493	De Mun	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	No	Yes	Yes	Yes	Yes	No	No	No	No	Yes	Yes	N/A	Yes	Yes	Yes	No	36	27	63	Med	Low	Med	\$2,000	
2017200269	Audubon & Parkdale	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	N/A	N/A	N/A	Yes	No	Yes	No	13	50	63	Low	Med	Med	\$2,000	
2018200265	Audubon & Buckingham	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	No	Yes	Yes	No	Yes	Yes	Yes	Yes	Yes	N/A	N/A	N/A	No	Yes	Yes	No	13	50	63	Low	Med	Med	\$2,000	
2023200849	Shaw Park & Shaw Park	Parallel	Yes	Yes	No	No	Yes	Yes	Yes	Yes	Yes	Yes	Yes	No	Yes	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	No	N/A	N/A	N/A	No	Yes	Yes	No	42	20	62	Med	Low	Med	\$2,000
2014800827	Dartford & Aberdeen	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	No	Yes	Yes	No	Yes	No	No	No	No	N/A	N/A	N/A	No	Yes	Yes	Yes	22	40	62	Low	Med	Med	\$2,000	
2014800829	Dartford & Aberdeen	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	Yes	Yes	Yes	No	Yes	No	No	No	No	N/A	No	N/A	No	Yes	No	Yes	22	40	62	Low	Med	Med	\$2,000	
2015400458	University & Aberdeen	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	No	Yes	Yes	Yes	No	No	No	No	No	No	N/A	N/A	No	Yes	No	No	22	40	62	Low	Med	Med	\$2,000	
2025000480	De Mun & Arundel	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	No	Yes	Yes	No	No	No	No	No	No	No	N/A	N/A	Yes	Yes	Yes	Yes	27	35	62	Med	Med	Med	\$2,000	
2013300405	University & Cecil	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	Yes	Yes	Yes	No	No	Yes	No	Yes	Yes	N/A	No	N/A	Yes	Yes	Yes	Yes	31	30	61	Med	Low	Med	\$2,000	
2019700189	Westwood & Wellington	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	Yes	Yes	Yes	Yes	No	Yes	Yes	Yes	Yes	Yes	N/A	N/A	N/A	No	Yes	Yes	Yes	36	25	61	Med	Low	Med	\$2,000	
2000200242	Audubon & Cromwell	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	No	Yes	Yes	Yes	No	Yes	Yes	Yes	Yes	Yes	N/A	N/A	N/A	No	Yes	Yes	No	16	45	61	Low	Med	Med	\$2,000	
2013800131	Edgewood & Walinca	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	No	Yes	Yes	Yes	No	Yes	Yes	Yes	No	N/A	N/A	N/A	Yes	Yes	No	Yes	46	15	61	Med	Low	Med	\$2,000	
2029600372	Big Bend & Northmoor	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	No	Yes	Yes	Yes	Yes	No	No	No	No	No	No	No	N/A	Yes	Yes	Yes	Yes	26	35	61	Low	Med	Med	\$2,000		
2007500685	Topton & Maryland	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	No	Yes	N/A	Yes	Yes	No	Yes	45	15	60	Med	Low	Med	\$2,000	
2000000482	Northwood	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	No	No	No	N/A	Yes	Yes	Yes	Yes	35	25	60	Med	Low	Med	\$2,000	
2003500609	Kingsbury & Meramec	Parallel	Yes	Yes	No	Yes	No	Yes	Yes	No	Yes	Yes	Yes	No	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	N/A	N/A	N/A	Yes	Yes	Yes	No	40	20	60	Med	Low	Med	\$2,000	
2018400447	Dartford & Arundel	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	Yes	Yes	Yes	Yes	No	Yes	No	No	No	No	No	N/A	N/A	No	Yes	No	Yes	20	40	60	Low	Med	Med	\$2,000	
2018400450	Dartford & Arundel	Perpendicular	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	Yes	Yes	No	Yes	Yes	Yes	Yes	Yes	Yes	Yes	No	No	No	N/A	N/A	N/A	No	Yes	No	Yes	20	40	60	Low	Med	Med	\$2,000